

**CHEHALEM PARK AND RECREATION DISTRICT  
BOARD OF DIRECTORS REGULAR MEETING  
CHEHALEM ADMINISTRATION OFFICE  
125 S. ELLIOTT ROAD  
NEWBERG, OREGON  
OCTOBER 28, 2021  
6:00 P.M.**

**I. Call To Order**

**II. Roll Call**

**III. Approval of or Additions to the Agenda**

**IV. Approval of Consent Agenda**

- A. Approve Minutes Regular Board Meeting September 23, 2021.
- B. Approval of Bills Payable
- C. Approval of Financials

**V. Public Participation**

- A. Yamhelas – Westsider Trail Presentation
- B. Others not on Agenda

**VI. Action Items/Committee Reports/Board Comments**

- A. No Action Items at this time
- B. Reports and Comments from Board Members

**VII. Old Business**

- A. Updates on Projects and Questions
- B. Pickle Ball Advisory Committee Report

**VIII. From the Superintendent's Desk**

- A. Financial Report and Questions
- B. Superintendent's Report
- C. Staff Reports

**IX. Correspondence**

- A. Citizens' Comments/Evaluations
- B. Miscellaneous Info

**X. Adjournment**

**Next regular Board meeting is December 2, 2021 (If needed).**

**To:** Board of Directors  
**From:** Superintendent  
**Date:** October 25, 2021  
**Re:** Background information for October 28, 2021 Board Meeting  
Number corresponds to Agenda Item

- II. ROLL CALL** – We need 3 present for the meeting. Please call if you cannot attend. PLEASE REMEMBER MEETING AT THE DISTRICT OFFICE. YOU CAN CALL IN FOR MEETING. YOU CAN ATTEND REMOTELY, VIA ZOOM. Kat will send information needed for meeting  
**Please see page 4 for index for page numbers**

- III. APPROVAL OR ADDITIONS TO AGENDA** – If you wish additions please give me a call.

**IV. APPROVAL OF CONSENT AGENDA**

- A. Approval of Board Meeting Minutes – Please see pages (5-10) for Regular Meeting Minutes of September 23, 2021

**RECOMMENDATION:** Approval of Regular Board Meeting Minutes for September 23, 2021

- B. Approval of Bills Payable – See page (11-12). General Fund \$997,984.67. SDC FUND \$16,329.78. LOAN SERVICE FUND \$0.00. POOL BOND \$0.00. FOUNDATION \$35,205.00

**RECOMMENDATION:** APPROVAL OF BILLS PAYABLE.

- C. Approval of Financial – Please see page (13).

**RECOMMENDATION:** Approve Financials as submitted

**V. PUBLIC PARTICIPATION**

- A. Yamhelas Westsider Trail – Presenters: Steve Wick, Please see pages (14-28) Veronica Hines advocates

Please see pages (29-224). Tom Hammer opposition Please see pages (225).

- B. Others not on agenda – We have no other request at this time.

**VI. ACTION ITEMS/COMMITTEE REPORTS/BOARD COMMENTS..**

- A. No Action Items

- B. Reports Comments from Board Members – Given at meeting.

**VII. OLD BUSINESS**

- A. Update on Projects, Operation and Coronavirus Pandemic – Will discuss at meeting. Staff will be present to answer questions.

- B. Pickle Ball Advisory Committee – Please see pages (226-229)

**VIII. FROM THE SUPERINTENDENTS DESK**

- A. Financial Report and Questions. We did not have to borrow for the 2017-18, 2018-19, 2019-20 and 2020-21 budget. It is projected we will not borrow for the 2021-22 budget The current debt is for the golf course, property on the river, fitness center and pool bond. We are allowed about \$92,400,000.00 in debt. As of 6/30/2019 we have \$25,251,238 outstanding long term debt obligations. Revenue is up in SDC fees. We refinanced the loans for the golf course, property and combined them with the loan for the pool and fitness center. This was done to save money. Currently we have two loans and one bond. Please note the general fund in previous years had transferred the money to pay for debt to the Loan Service Fund. We are now paying debt out of the SDC fund.

**GENERAL FUND SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/30/21-22	DIFFERENCE
<b>TOTAL EXPENDITURES</b>	<b>\$ 1,339,353.27</b>	<b>\$ 1,557,122.52</b>	<b>\$ 217,769.25</b>
TOTAL OPERATION EX.	\$ 1,334,853.55	\$ 1,544,506.96	\$ 209,653.41
TOTAL CAP/AQ/DEV/TRS	\$ 4,499.72	\$ 12,615.56	\$ 8,115.84
<b>TOTAL REVENUE</b>	<b>\$ 4,546,939.64</b>	<b>\$ 6,077,042.86</b>	<b>\$ 1,530,103.22</b>
TOTAL TAXES	\$ 76,160.58	\$ 64,344.48	\$ < 11,816.10>
TOTAL FEES & CHARGES	\$ 982,151.46	\$ 1,228,021.26	\$ 245,869.80
TOTAL OTHER REVENUE	\$ 35,552.00	\$ 121,173.75	\$ 85,621.75
BEGINNING BALANCE	\$ 3,453,075.60	\$ 4,663,503.37	\$ 1,210,427.77
<b><u>BALANCE</u></b>	<b><u>\$ 3,207,586.37</u></b>	<b><u>\$ 4,519,920.34</u></b>	<b>\$ 1,312,333.97</b>

**SDC FUND SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/30/21-22	DIFFERENCE
BEGINNING BALANCE	\$ 2,508,611.31	\$ 1,941,451.07	\$ < 567,160.24>
INTEREST	\$ 4,472.21	\$ 2,723.86	\$ < 1,748.35>
CITY OF NEWBERG	\$ 70,338.52	\$ 1,721,049.54	\$ 1,650,711.02
CITY OF DUNDEE	\$ 0.00	\$ 55,655.30	\$ 55,655.30
COUNTY OF YAMHILL	\$ 44,091.09	\$ 41,642.75	\$ < 2,448.34>
TRANSFERRED IN	\$ 0.00	0.00	\$ 0.00
<b>TOTAL REVENUE</b>	<b>\$ 2,627,513.13</b>	<b>\$ 3,762,522.52</b>	<b>\$ 1,135,009.39</b>
<b>TOTAL EXPENDITURE</b>	<b>\$ 322,335.46</b>	<b>\$ 202,994.43</b>	<b>\$ &lt;119,341.03&gt;</b>
<b><u>BALANCE</u></b>	<b><u>\$ 2,305,177.67</u></b>	<b><u>\$ 3,559,528.09</u></b>	<b>\$ 1,254,350.42</b>

Please note the operational cost in the General Fund is up. The operational revenue Fees & Charges are up, SDC Fund is up. Please remember the debt was to come out of SDC's for the 2020-21 budget and the 2021-22 budget.

B. Superintendent Report – To be given at meeting.

C. Staff Reports – Please see pages (230-241). Kat will send out additional staff reports

**IX. CORRESPONDENCE**

A. Citizens Comments/Evaluations – Please see Pages (242-259)

B. Miscellaneous Information – Please see pages (260-277).

**X. ADJOURNMENT.**

**Next Regular Board Meeting December 2, 2021 (If needed)**

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CHEHALEM PARK AND RECREATION DISTRICT  
REGULAR BOARD MEETING  
CPRD Administration Office  
125 S. Elliott Road  
September 23, 2021  
**MINUTES**

I. **Lisa Rogers called the meeting to order 6:00 p.m.**

II. **Roll Call**

**Board members:**

Gayle Bizeau  
Don Loving  
Jim McMaster  
Bart Rierson  
Lisa Rogers

**CPRD Staff:**

Don Clements, Superintendent  
Casey Creighton, Basic Services Supervisor/Park & Facilities Supervisor  
Julie Petersen, Special Services Supervisor/Recreation Supervisor  
Heidi Smith, Administrative Coordinator  
Kayla McElligott, Events Marketing Coordinator  
Richard Cornwell, IT  
*Kellan Sasken, Special Services/Golf Director – not present*  
*Kat Ricker, Public Information Director – not present*

**Public: (5 people present)**

Martin Peters (remotely)  
Hunter Wylie  
Carol Foley (remotely)  
Peggy Kilburg  
Sheryl Greiner – NPC  
David Larson  
Karen Tooley

III. **Approval of additions or changes to agenda –**

Gayle Bizeau: Have our Board meetings recording and posted online.  
*Add as item e*

Casey Creighton: Proposal to provide land use planning for  
*Add as item f*

***Motion: To add these two items to the agenda as items e and f.***

*Moved Don Loving*  
*Second Jim McMaster*

*Passed unanimously*

**IV. Approval of consent agenda**

- a. Approval of minutes of regular Board meeting August 26, 2021
- b. Approval of bills payable
- c. Approval of financials

***Motion: To approve consent agenda.***

*Moved Don Loving*

*Second Bart Rierson*

*Passed unanimously*

**V. Public participation**

They are here just to answer questions about pickleball, not to speak.

**VI. Action items/committee reports/Board comments**

***a. Approval of Resolution (Appointment of Pickle Ball Advisory Committee)***

13 people have applied to be on the Pickle Ball Advisory Committee. Lisa Rogers stated she doesn't want to limit the number of people that join, so she is ok with everyone joining. Don Loving is not in the favor of a large committee and he would suggest they have at least one person who is out of the District. For efficiency, Don Clements agrees with Don Loving about having a limited number for the committee. Julie Petersen says there is almost 100 active Pickle Ball members in the community and can see where a larger committee could work. Julie Petersen would also like to be on the committee.

***Motion: To appoint all 12 members as well as Julie Petersen as the staff member.***

*Moved Bart Rierson*

*Second Gayle Bizeau*

*Passed unanimously*

***b. Request for Proposal Ewing Young Trail Bridge***

Casey Creighton put out an RFP in August and asked companies for proposals in building a bridge. He received three, each with unique features, but believes NV5 will do the best job. He said they won't build it, but they help CPRD get the permits and secure the engineering, and then CPRD can pick the bridge builder and save money. Creighton says it's in CPRD's best interest to accept the proposal from NV5 at \$149,000.

***Motion: To accept the management of NV5.***

*Moved Jim McMaster*

*Second Don Loving*

*Passed unanimously*

***c. Discussion of Sander Estate Park***

Casey Creighton says Jim Jacks (the contract planner for the Sander Estate Park) came up with staff recommendations. He sent it out on the 13th of September, and a meeting was held on the 21st of September. Creighton had some time to look at it and made

CPRD's comments available to the Dundee City Council, and presented the master plan. Dundee City Council approved the master plans but with a lot more conditions than CPRD was expecting. CPRD will have another discussion with the City of Dundee.

*d. Reports and comments from Board members*

**Gayle Bizeau**

**Don Loving**

**Jim McMaster:**

- Some confusion about lap swim hours—asked Julie Petersen to look into it.

**Bart Rierson:**

- Is working with the county on how to acquire the old landfill, and would like an update on that project. Wants to see it be made a priority.
  - Don Clements: He has contacted legal counsel (John Bridges) and doesn't know where it is right now, so he will double-check and let Bart know. If there is an issue, he will have him at the next board meeting.
- Talked about the first trails advisory committee meeting. Marty Peters has an idea for a trail and would like to come in and talk at a board meeting sometime.
- Should the board put on the agenda for support for the westider trail? Says it's time we start having a discussion if that is a project we are interested in and working with the county on. and how we finance it all.
  - Don Clements: The county doesn't want to go forward with the trail, so do we? The first step for the board is if you are opposed to moving forward, now is the time to say it. If you want to move forward, we can start that discussion. Bart wants to make a motion to move forward, Loving doesn't want to because it's not on the agenda. Bart then makes a motion to put the discussion on the agenda for a future meeting. Jim wants more facts about the decision before moving forward.

**Lisa Rogers:**

- Talked about the possibility of how some events and conversations currently happening in the city are affecting or could potentially affect CPRD—and if it is going to affect the operations of CPRD, then it should be something that the Board discusses.
- Says CPRD should have a message on our website saying everyone is welcome. She has been hearing that a lot of people will not be coming to Newberg because of the

events happening at the Newberg School District. Bart Rierson supports that notion and says that the City of Newberg has done this, the Newberg Downtown Coalition has done this and believes we need to make a statement. Gayle says this is reactionary to the School District and asks if there has been any commentary of feeling not welcome in our parks? Lisa says that people have come forward saying they don't want to come here for softball any longer, as well as for other events.

- Casey Creighton says that CPRD has never denied anyone from using any facilities in Newberg. Suggests maybe posting signs at our facilities saying this facility is open and welcome for everyone.
- David Larson (public) asked Lisa how she is interpreting the School Board. Lisa says that what they did has made this community look racist and homophobic.
- Jim McMaster suggests to put in our manuals that we are open for everyone.
- Don Clements says CPRD will put it on the website saying that "All are welcome," as well as putting up signs at the pool, the ballfields, and other facilities.

*e. Recording Board meetings*

Gayle Bizeau wants the board meetings to be recorded and posted for people to watch from here on out. Richard Cornwell and Casey Creighton say it can be done and that CPRD will start doing that.

*f. Proposed land use for heritage campground*

Casey Creighton says it would be in our best interest that we hire a planner. He put out an RFP for no more than \$39,000. He would also like to work towards adopting the CPRD Master Plan as a working document. Jim McMaster asks if this will include everything in the county and Casey confirms, saying for projects now and in the future. Creighton recommended the Board hire Angelo Planning Group.

***Motion: To hire Angelo Planning Group.***

*Moved*                 *Bart Rierson*

*Second*               *Don Loving*

*Passed unanimously*

**VII. Old business/project updates and questions**

- a. Casey Creighton talked about how materials for the aquatic center that they have been waiting on are still far out due to covid. The aerating golf course is done, the course is back open. They are working on maintaining buildings. The Newberg-Dundee Bypass trail is rolling out soon. He is also

having trouble finding staff, saying no one wants to work for \$12.75 an hour. Julie Petersen also said a lot of people are calling and setting up interviews and not showing up.

- b. Julie Petersen had a volleyball program shut down due to covid breakout.

**VIII. From the superintendent's desk**

- a. Financial report – Nothing new.

- b. Superintendent's report –

- i. Potential Park Land: was discussed at the last meeting, nothing new.

- c. Staff reports –

**Julie Petersen:**

- Very busy with soccer, Little Big Kicks, and tackle football. Middle school volleyball is down for 10 days due to COVID outbreak. Flag football starts Saturday, and youth softball is almost over with a very successful season, and baseball is in full swing.
- The Care program is going well.
- The golf course has had a lot of tournaments and has been very busy—SDAO was out there for a tournament. Kellan Sasken was very busy with golf lessons.
- Summer swim lessons are over and were very successful with 1000 kids going through the program. There are some new programs like SUP Yoga, fencing, and babysitting class.
- We have been contacted with some state funding for kids to participate in CPRD classes for free. We created a coupon code and the Newberg School District sent us \$12,000 for this and it took 6 minutes to go through it all.

**Kayla McElligott:**

- The markets have concluded and Erin has done a great job and all of the vendors enjoy working with her. We are looking at a possible new format for the Dundee market next year, as well as helping promote our vendors even further on our website.

**Richard Cornwell:**

- Talked about the process on how he added a lot more technology to create the new board meeting room.

**IX. Correspondence**

- A. Citizen comments/evaluations – N/A  
B. Miscellaneous Info – N/A

- X. Adjournment – Lisa Rogers moved to adjourn at 7:38 p.m.**

**The next regularly scheduled meeting will take place at 6 p.m. on Thursday, October 28, 2021, at the CPRD Administration Office.**

Respectfully Submitted,

Kayla McElligott, Events Marketing Coordinator

**ACCOUNTS PAYABLE AND PAYROLL  
FROM SEPTEMBER 7, 2021  
UP TO OCTOBER 20, 2021**

**ACCOUNTS PAYABLE FOR GERNERAL FUND**

<u>CHECK NUMBERS</u>	<u>AMOUNT</u>	<u>TYPE CHECKS</u>
124865-125110	\$ 556,604.62	ACCOUNTS PAYABLE
124968-124969 125003-125005 124045 were payroll checks		
	\$ 1,633.77	PAYROLL
WIRE TRANSFER PAYROLL	\$ 408,361.96	PAYROLL
2098-2123	\$ 31,384.32	ACH TRANSFERS
<b>GRAND TOTAL</b>	<b><u>\$ 997,984.67</u></b>	

**BREAKOUT**

<b><u>ACCOUNTS PAYABLE</u></b>	<b>\$ 556,604.62</b>
<b><u>PAYROLL</u></b>	<b>\$ 409,995.73</b>
<b><u>WIRE TRANSFER &amp; ACH</u></b>	<b>\$ 31,384.32</b>

**ACCOUNTS PAYABLE FOR SDC FUND**

<u>CHECK NUMBERS</u>	<u>AMOUNT</u>	<u>TO WHOM</u>
1011	\$ 3,273.75	AKS ENGINEERING
1012	\$ 312.50	AKS ENGINEERING
1013	\$ 2,473.00	DEPMNT OF ENVIRONMENTAL
1014	\$ 497.50	SEA
1015	\$ 2,923.28	GREEN WORKS
1016	\$ 6,849.75	GREEN WORKS
<b>GRAND TOTAL</b>	<b><u>\$ 16,329.78</u></b>	

**ACCOUNTS PYABLE FOR LOAN SERVICE FUND**

<u>CHECK NUMBERS</u>	<u>AMOUNT</u>	<u>TO WHOM</u>
NO CHECKS	\$ 0.00	
<b>GRAND TOTAL</b>	<b><u>\$ 0.00</u></b>	

**ACCOUNTS PAYABLE FOR CAPITAL POOL CONSTRUCTION & POOL BOND**

<u>CHECK NUMBERS</u>	<u>AMOUNT</u>	<u>TO WHOM</u>
NO CHECKS	\$ 0.00	
<b>GRAND TOTAL</b>	<b><u>\$ 0.00</u></b>	

**BREAKOUT**

<u>CAPITAL POOL CONST.</u>	\$ 00.00
<u>POOL BOND DEBT</u>	\$ 00.00

**ACCOUNTS PAYABLE FOR FOUNDATION**

<u>CHECK NUMBERS</u>	<u>AMOUNT</u>	<u>TO WHOM</u>
171	\$ 32,963.30	CPRD REIMBURSEMENT
172	\$ 29.60	TECH SOUP US BANK
173	\$ 100.00	LISA THOMAS TVFBL
174	\$ 250.00	CHANCE HOWARD TVFBL
175	\$ 250.00	DIANNE ANDERSON TVFBL
176	\$ 250.00	LAURA VANDEKOP TVFBL
177	\$ 1,332.50	DORMERS FOOTBALL SHIRTS
178	\$ 29.60	TECH SOUP US BANK
<b>GRAND TOTAL</b>	<b><u>\$ 35,205.00</u></b>	



**FINANCIAL OVERVIEW  
GENERAL FUND SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/30/21-22	DIFFERENCE
Total Operational Expense	\$ 1,334,853.55	\$1,544,506.96	\$ 209,653.41
Total Capital Outlay & Transfers	\$ 4,499.72	\$ 12,615.56	\$ 8,115.84
<b>GRAND TOTAL EXPENSES</b>	<b>\$ 1,339,353.27</b>	<b>\$1,557,122.52</b>	<b>\$ 217,769.25</b>
Total Tax Revenue	\$ 76,160.58	\$ 64,344.48	\$< 11,816.10>
Total Fees & Charges Revenue	\$ 982,151.46	\$1,228,021.26	\$ 245,869.80
Total Other Revenue	\$ 35,552.00	\$ 121,173.75	\$ 85,621.75
Beginning Balance	\$ 3,453,075.60	\$4,663,503.37	\$ 1,210,427.77
<b>GRAND TOTAL REVENUE</b>	<b>\$ 4,546,939.64</b>	<b>\$6,077,042.86</b>	<b>\$ 1,530,103.22</b>

**SDC FUND SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/30/21-22	DIFFERENCE
<b>GRAND TOTAL EXPENSES</b>	<b>\$ 322,335.46</b>	<b>\$ 202,994.43</b>	<b>\$ &lt;119,341.03&gt;</b>
TOTAL REVENUE	\$ 118,901.82	\$1,821,071.45	\$ 1,702,169.63
BEGINNING BALANCE	\$2,508,611.31	\$1,941,451.07	\$ <567,160.24>
<b>GRAND TOTAL REVENUE</b>	<b>\$2,627,513.13</b>	<b>\$ 3,762,522.52</b>	<b>\$ 1,135,009.39</b>

**LOAN SERVICE FUND SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/30/20-21	DIFFERENCE
<b>GRAND TOTAL EXPENSES</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>
REVENUE TRANSFERS	\$ 0.00	\$ 0.00	\$ 0.00>
INTREST	\$ 92.14	\$ 0.00	\$ < 92.14>
BEGINNING BALANCE	\$ 33,567.32	\$ 33,702.97	\$ 135.65
<b>GRAND TOTAL REVENUE</b>	<b>\$ 33,659.46</b>	<b>\$ 33,702.97</b>	<b>\$ 43.51</b>

**EQUIPMENT AND MAJOR MAINTENANCE FUND SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/30/21-22	DIFFERENCE
<b>GRAND TOTAL EXPENSES</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>
TOTAL REVENUE	\$ 0.00	\$ 0.00	\$ 0.00
BEGINNING BALANCE	\$ 0.00	\$ 0.00	\$ 0.00
<b>GRAND TOTAL REVENUE</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>

**CAPITAL PROJECT POOL FUND SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/30/21-22	DIFFERENCE
<b>GRAND TOTAL EXPENSES</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>
<b>GRAND TOTAL REVENUE</b>	<b>\$ 247,933.85</b>	<b>\$ 0.00</b>	<b>\$ &lt; 247,933.85&gt;</b>

**BOND LOAN SERVICE SUMMARY**

DESCRIPTION	AS OF 9/30/20-21	AS OF 9/39/21-22	DIFFERENCE
<b>GRAND TOTAL EXPENSES</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>	<b>\$ 0.00</b>
<b>GRAND TOTAL REVENUE</b>	<b>\$ 324,740.37</b>	<b>\$ 143,112.42</b>	<b>\$ &lt;181,627.95&gt;</b>

**Subject:** Fw: Yamhelas Trail & farming  
**Date:** Friday, October 22, 2021 at 3:37:17 PM Pacific Daylight Time  
**From:** Veronica Haley Hinkes  
**To:** Kat Ricker  
**Attachments:** VCTC Santa Paula Branch Line Study - Final.pdf, Untitled.eml, 20200625110708146.pdf, image001.jpg, image005.png, image006.jpg, image002.png

Here is Steve Wick's testimony and materials....

V

----- Forwarded Message -----

**From:** steve wick <stevencarol.wick@gmail.com>  
**To:** "krickter@cprdnewberg.org" <krickter@cprdnewberg.org>  
**Cc:** 'Wayne Wiebke' <shadypondacre@yahoo.com>; 'Veronica Haley Hinkes' <veronica4249@yahoo.com>; 'Bart Rierson' <brieron@board.cprdnewberg.org>  
**Sent:** Thursday, October 21, 2021, 09:40:48 PM PDT  
**Subject:** Yamhelas Trail & farming

Kat, enclosed is my testimony for the upcoming CPRD Board meeting. Please list me as one of the speakers (Zoom only, please) I won't need 5 minutes. I have a simple premise this time. I could "get in the weeds" and discuss every nuance of farming....talk about the sprays that the farmers said that they must use....the ones that have just been outlawed, or rendered almost impossible to use because of new regulations. I could discuss fencing, vagrants, contaminated crops, and all other perceived reasons why farming cannot be accomplished next to an active trail.....but I don't think that would be profitable.

I have one simple premise....if others can build, maintain, and operate trails next to active farms....why can't we?

.....  
.....  
My name is Steve Wick. I am a board member of the Friends of the Yamhelas Westsider Trail, and a member of Oregon Equestrian Trails, Yamhill Valley chapter.

My wife and I own Chehalem Mountain Farms, a small farming operation in Yamhill County. We have been personally growing and harvesting filberts/hazelnuts in Yamhill County since 1992 (over 28 years) on our 60 acre farm. We have also farmed wheat, vetch, oats, hay, Christmas Trees, and ornamental plants, on this farm, or on the farm my wife and I purchased in Washington County in the mid-eighties. My brothers and I recently sold our 160 acre family tree farm, which we co-managed for years.

Last April I submitted testimony to CRPD in support of the Yamhelas Trail. In this testimony I referenced my farming experience and went into detail refuting some of the inaccuracies that anti-trail farmers had submitted to LUBA. They contended that they could not farm next to a trail, and listed many "reasons" ; they would not be able to spray pesticides, trespassers/ litter could contaminate their crops, vagrants/ homeless would flood their properties, etc.

But all these "reasons" have been encountered before on dozens of other trails in the U.S. and all of them have been solved!

**It all boils down to one thing: If other trails can peacefully co-exist with active farms, why can't the Yamhelas Trail?** (see enclosed VCTC Santa Paulina Branch Line Study)

If other trail farmers found solutions to their concerns, why can't we? If other trails shut down to allow farmers to spray their crops adjacent to a trail, why can't we? If other trails manage field access, mitigate trespassing, minimize fire danger with fences, gates, signs, why can't we? If other trails found solutions by bringing everyone together to list problems and concerns, why can't we?

What makes us so special that we can't use all the experiences and solutions that others have done to make our Trail viable?

In April, when I testified before the CPRD board, I stated that I would have no problem farming along the side of the Yamhelas Trail. I stand behind that.

There is absolutely no reason that we cannot come together and design a Trail that will utilize a wonderful asset for future generations!

Below is some of the specific comments that I supplied last April regarding filbert/hazelnut orchards.

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In the Stay granted by LUBA on June 19 2020 they concluded, in one of their arguments, that Ben Van Dyke would suffer "*Irreparable Injury*" because he would be "*hindered or prohibited from spraying the herbicides and pesticides required to protect his crop.*"

(It should be noted that the proposed Yamhelas Westsider trail right-of-way is approximately 60 feet wide, with the trail itself being 12 feet wide. That means that there is a 24 feet buffer on either side of the trail that Ben Van Dyke, and others, say is not sufficient to mitigate spray drift.)

LUBA also stated *"Van Dyke explained that filbert trees should live 75 years or more and that, if he was unable to spray his filbert trees, those trees could die due to Eastern Filbert Blight. Given the long-lived nature of the filbert trees and the potential for protracted crop and yield loss if spraying cannot occur, we conclude that the injury Van Dyke specified is not an injury that could be adequately compensated in money damages."*

They also concluded that Van Dyke could lose his food safety certification if litter and debris from the construction site enter his hazelnut orchard.

These conclusions, and the following assumptions;

A. that Van Dyke must be able to spray his trees for Eastern Filbert Blight; *"If I am unable to spray as required, I risk my trees dying of Eastern Filbert Blight that could result in the loss of all crops on a tree that should last 75 plus years."*

B. that Van Dyke, and others, would be unable to spray during trail construction (or during trail use)

C. that Van Dyke uses the herbicide sprays Gramoxone and Parazone (Paraquat), which state *"do not use around...recreational parks...."*

D. that Van Dyke would lose his food safety certification if litter and debris enter his orchard from trail construction (or trail use)

**...can be totally mitigated with a few simple actions.**

-  
A. Spraying for Eastern Filbert Blight. This is not necessary for Van Dyke's trees. Van Dyke has previously stated that his trees are two years old; *"Our hazelnuts were planted in 2018,*

and our second year trees,...". BUT; All the new filbert varieties are "blight resistant".

OSU wrote the bible on hazelnuts. No one in the world has the experience and knowledge that they possess regarding the entire hazelnut spectrum. They update info yearly for growers. The Pest Management Guide, Pacific Northwest Plant disease, Insect, and Weed management handbooks constitute our bible. We refer to these and their other hazelnut publications and missives yearly for the most up-to-date guidance. Shawn Mehlenbacher has been working on EFB (Eastern Filbert Blight) for over 30 years, and his breeding programs, and the spraying programs developed by Professor Jay Pscheidt, have enabled our industry to survive.

New trees only have to be sprayed the first spring after planting, and only if they are adjacent to a heavily infected orchard. OSU has documented this in their 2020 Pest Management Guide for Hazelnuts. See p17: *"Cultivars such as Jefferson or McDonald, with the single dominant gene for resistance only, need protection the first spring after planting, when located near heavily infected orchards."* <https://catalog.extension.oregonstate.edu/em8328/html>

Per OSU's Pacific NW Handbook; *"Dorris", 'Jefferson', 'McDonald', 'PollyO', 'Santiam', 'Wepster', and 'Yamhill' are highly resistant with the single dominant-resistance gene from 'Gasaway'."*  
<https://pnwhandbooks.org/plantdisease/host-disease/hazelnut-corylus-avellana-eastern-filbert-blight>

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B. Spraying chemicals next to the trail poses no problem, if the trail is shut down when the spraying must be done. This is simple to do. Other trails do this! The farmers and trail owners coordinate operations. Spraying chemicals in filbert orchards usually occurs less than a dozen times during the year, but young orchards could require more, and non-common diseases or pests would increase the number of spray occurrences required.





In 2015 a study was done of many trails (see attached Santa Paula Branch Line Study) That study specifically addressed farming and how the impacts to farming were dealt with on trails throughout farming acreages. Below you will see some excerpts from that study. It should be noted that many of the trails just shut down while the farmer sprays his fields.

**San Pasqual Valley Trail: Ca.** • Specific fencing was designed for the trail, modified from the park department's standard lodgepole fencing. Chicken wire inserts were added approximately one foot from the ground to allow wildlife to pass through;

- Gates allow sections of the trail to be closed;
- Signage installed to alert the trail-users of trail closure for spraying and to stay on the trail;
- Farmers can dictate trail closure (within reason, i.e., preferably not on weekends) for maintenance and crop spraying. This protocol was developed and approved by the County Farm Bureau, County Farm Advisor's Office, and the affected farmers

**Feedback from Involved Parties**

- Shawna Anderson: There have been no reported incidents of theft, vandalism, or liability issues to this date.
- Many farmers who were initially opposed to the trail now support it.
- An agreement was made early after the SDRP JPA listened to the concerns of the farmers and created specific design and management plans to create a mutually beneficial relationship between the trail and agricultural industry.

**Cowel-Purisima Coastal Trail** • Stout fencing

- Large gates to accommodate cattle and equipment passage while trail is closed
- Gates to close trail during spraying and operations

- Information and regulatory signs
- Maintained by volunteer docents
- Farmer has ability to close gates for maintenance

#### **Feedback from Involved Parties**

- Paul Ringgold: The ability to work as a team, such as on a section where bluff erosion was impacting the trail, is key to success.
- POST recently asked whether there were any security issues that would benefit from additional gates and was told that there were none.
- POST hasn't received any negative comments from owner John Giusti, or Giusti's agricultural tenant on the southern half of the property, Bob Marsh.
- John Guisti reported 8/25/14 that the trail project "has not interfered with his operation at all, and he considers it a successful project." The fence is very important.

**Obern Trail, Ca.** • Little to no physical barrier in most places. Oleander hedges and chainlink fence.

- Each farmer decides on fencing – not installed by the County.
- High levels of use create a self-policing scenario.
- The trail is lit throughout, at all hours.

#### **Feedback from Involved Parties**

- Matthew Dobberteen: In over ten years managing trails for Santa Barbara County, I have never received a complaint about the Obern Trail. Our trails that run near agriculture are never the trails we have problems with. The only issue is every few years we may get some graffiti on a retaining wall. "A bike path will make theft harder, not easier, by bringing light, attention, people, eyes to the trail." "If someone wanted to steal from a farm, they'd find a place where no one could see them, not a trail with steady use."
- John Givens: No significant impacts from the trail. Occasionally

homeless pass through and there is minor vandalism, but it has not been serious enough to involve the County or other authorities. Trail users don't cut through the farm property

### Cedar Valley Trail, Ia.

- Fencing with gates;
- Reinforced crossing to accommodate equipment;
- Easement allowing farm equipment access;
- Signage to warn trail users of crossing farm vehicles.

#### **Feedback from Involved Parties**

- Joyce Squires: She and her husband were initially opposed to the trail, with concern about trespassing, but this has not been an issue;
- Generally the trail has been very positive, she and her family use it;

-

West County Trail, Ca. • Farmers put A-frame signs on their property stating when spraying will occur.

- Spraying generally limited to early morning, before most trail users are present.
- Aerial spraying not conducted near the trail.
- Some vineyard owners have built connections between their properties and the trail.
- "No Trespassing" signs have been installed by some vineyard owners.
- The County patrols the trail and regularly talks with neighbors.

#### **Feedback from Involved Parties**

- Kenneth Tam: The County conducted a record of survey and title search, then reached out individually to agricultural land owners and operators who appeared to be using the railroad ROW without the legal right to do so and requested they



provide documentation that they were using the ROW legally. None were able to provide documentation.

There was a blueberry farmer using the railroad ROW to access his crops. The farmer has since opened a stand along the trail to sell blueberries and blueberry ice cream.

The main concerns voiced during the planning stages included the potential for crime and trespass.

Most spraying is done early in the morning. All spraying must be done with as little wind as possible. At the very worst case, the minimum notice that a farmer could provide would be 24 hours, but generally he would be able to schedule spraying a few days prior and/or schedule a tentative window weeks in advance. An example of this would be the spraying that I just did in my filbert orchard the first week of July for Filbert Worm. The orchard had to be monitored for several weeks for filbert moths. When the level got to a certain point, I had only a few days to get my first air blast spray applied. I consulted weather forecasts for rain, and then looked at the wind forecasts, and found that I could spray immediately. (Wind forecasts are extremely accurate, and with aps like "Windy", can be used to forecast flow and speeds, hour by hour, quite a few days ahead of time.) The spray I used was effective for two to three weeks, and then a second application was required if filbert moths were still present.

To make it a win-win for both the farmer and trail users, the farmer can schedule the trail shutdown for only the very short time he needs to spray the edges of his field, adjacent to the trail. Then he can open up the trail, and continue spraying the balance of his fields.

Foliar sprays (sprays that must cover all the leaves of the tree) require the most setback, since they are accomplished thru "air blast " sprays that force the spray up and over the tops of the trees. Examples of these sprays are the pesticides and fungicides that must be sprayed to minimize pests or diseases. Two rows of trees are sprayed at the same time while the spray operator drives down the aisle. The spray operator can also shut down either side of the air blast sprayer, allowing him to spray the outside edge of a row, and not spray neighboring properties. If a farmer wanted to be positively safe, he could simply power blast spray the first 8 rows, on the edge of the trail, while the trail was closed, and then open up the trail. (filbert trees are generally planted in rows 18 to 20 feet wide).



Herbicides used to control weeds, grasses and for eliminating suckers, are applied on the ground, or on the very bottom/base of trees using booms, while driving down the aisle between two rows of trees. Herbicides must be selectively sprayed; the spray must be kept on the ground or the base of the tree. They cannot be allowed to drift, even to the foliage above the weeds or suckers! If they reach the foliage they can damage or kill the tree. The

#### Restrictions

- Do not allow spray to contact green stems (except suckers), fruit or foliage

label on Gramoxone states the following:

[https://www.syngenta-us.com/current-label/gramoxone\\_sl\\_2.0](https://www.syngenta-us.com/current-label/gramoxone_sl_2.0)

It would be quite easy to spray a couple of the outside rows (36 to 40 feet) from the edge of the trail Right of Way, and then open up the trail while he sprayed the balance of his field. And, since the spray operator was adjacent to the trail ROW when he was spraying the outside rows, it would be very easy for him to monitor the ROW to ensure that no one was on it during his spray operation.



Van Dyke stated that he "must be able to spray his orchid for filbertworm, which can destroy an entire crop, from June through September" But spraying equipment cannot enter an orchard after Labor Day, since the nuts start falling to the orchard floor, and anyone driving equipment on the orchard floor could damage the nuts.

*"Blank nuts fall before good nuts. After blanks have fallen and just before good nuts begin to drop (usually at the end of August), it might be desirable to do a final flailing and floating to fill small depressions in the ground."*

<https://catalog.extension.oregonstate.edu/sites/catalog/files/project/pdf/em9079.pdf>

*'Hazelnuts begin to drop to the ground during the month of September. Prior to nut drop, the orchard floor is made level and smooth, and weeds are flail-mowed to facilitate harvest. Harvest generally occurs during October and is usually a two-step operation.'*

<https://catalog.extension.oregonstate.edu/sites/catalog/files/project/pdf/em9223.pdf>

C. Van Dyke also noted that he stocks and sprays the chemicals Gramoxone and Parazone (Paraquat). Paraquat is a very dangerous chemical. It is so dangerous that the EPA has changed the packaging containers to a new "closed system", and mandated Paraquat specific special training, and the training must be re-newed every three years.  
<https://www.epa.gov/pesticide-worker-safety/paraquat-dichloride-training-certified-applicators>

*"Paraquat is the most highly acutely toxic herbicide to be marketed over the last 60 years. Yet it is one of the most widely used herbicides in the world, and in most countries where it is registered, it can be used without restriction. It is used on more than 100 crops in about 100 countries. Gramoxone, manufactured by Syngenta, is the most common trade name for paraquat, but the herbicide is also sold under many different names by many different manufacturers. China is now the world's largest manufacturer of paraquat, producing more than 100,000 tonnes per year. Paraquat has been banned, or use disallowed, in 32 countries. Many international organisations, such as Rainforest Alliance, Fairtrade, Forest Stewardship Council, and food giants like Dole have voluntarily banned it from their production systems. Paraquat is highly acutely toxic and enters the body mainly by swallowing, or through damaged skin, but may also be inhaled. Thousands of deaths have occurred from ingestion (often suicide) or dermal exposure (mainly occupational) to paraquat.*

*Paraquat is corrosive to the skin and once the skin is damaged it is easily absorbed into the body. One farmer died after just 3.5 hours spraying diluted paraquat with a leaking knapsack. Others have died from spilling the concentrate on their skin. Thousands more have suffered severe acute and chronic effects from occupational use. It represents a severe public health problem in many countries despite the fact that paraquat is considered safe by its manufacturers, who believe they have no responsibility for the suicides. Yet experience has shown that where paraquat is banned or restricted deaths from suicides drop dramatically."*

The Gramoxone and Parazone (Paraquat) that Van Dyke has in stock have the old labels that state : "Do not use around home gardens, schools, recreational parks, golf courses or playgrounds."

But the labels on both products were updated by December of 2019, and the nebulous statement "around" was taken off the new labels:

<https://www.syngenta-us.com/herbicides/gramoxone-sl-2.0>

## "DIRECTIONS FOR USE

### RESTRICTED USE PESTICIDE

It is a violation of Federal Law to use this product in a manner inconsistent with its labeling.

Do not apply this product in a way that will contact workers or other persons, either directly or through drift. Only

protected handlers may be in the area during application. For any requirements specific

c to your State or Tribe,

consult the agency responsible for pesticide regulation.

NEVER USE THIS PRODUCT IN RESIDENTIAL OR PUBLIC RECREATIONAL SETTINGS (E.G., HOMES, HOME GARDENS,

SCHOOLS, RECREATIONAL PARKS, GOLF COURSES, AND/OR PLAYGROUNDS"

A check with the manufacturers revealed that the chemical did not change....only the label

changed. (see attached (FW: Gramoxone SL 2.0 email)

Parazone 3SL updated their label in Dec of 2019 to say the same thing.

[https://www3.epa.gov/pesticides/chem\\_search/ppls/005481-00615-20191230.pdf](https://www3.epa.gov/pesticides/chem_search/ppls/005481-00615-20191230.pdf)

But no matter what the new label states, federal law says that you **MUST** follow the instructions on the container that you are using. (again, read Gramoxone SL 2.0 email)

The simple solution is to purchase some of the new label Gramoxone or Parazone and use the new chemical on the edges of his field, then continue to use the old material on the inside. It seems ridiculous to do this, since the chemical in the old label containers, and the chemical in the new label containers, is exactly the same, but the law is the law. And, per Ben Van Dyke, "since the chemical does not have an expiration date" either chemical can be used for future sucker or weed spraying.

Another option is to use one or more of the other less lethal herbicides, (that readily mix with Gramoxone or Parazone) to spray the edges. Spray the edges with Aim(carfentrazone) and/or Rely (glufosinate), or a mix of Aim and Rely, or just spray with Saber (2,4-D), and then re-fill the spray tank with Gramoxone or Parazone (Paraquat) and do the balance of the orchard.

*.. contact herbicides e.g. glyphosate, glufosinate (Rely), and **paraquat (Gramoxone/Parazone)** may be tank mixed with **AIM** (carfentrazone) EC for broader spectrum weed control ...*

<http://www.cdms.net/ldat/ld5L1010.pdf>

*Rely (Glufosinate) or a combination of Rely and Aim are good options for young trees that are fully barked over and robust. Rely lasts a little longer than just Aim by itself and is a good combination for strip spraying as it controls both small grasses and broadleaves. Rely works best when temperatures are warm. Contact only so coverage is important.*

Gramoxone (Paraquat) is an inexpensive option for older trees, especially when you are also wanting to do a strip spray or full floor spray. A restricted product that has human health risks when used inappropriately, this chemistry isn't for everyone. Contact only so coverage is important.

as per the Hazelnut Growers of Oregon; <https://www.hazelnut.com/spring-sucker-control/>

**D. Loss of Food Safety Certification.** Van Dyke stated that "Construction workers have already littered the area with lunch bags and other detritus have found the way to our farm that we have had to clean up. This causes food safety issues. In that regard, we are strictly regulated by the USDA and any litter on our farms, risks losing food safety certifications that are necessary for the marketability of our brand as well as to maintain our certifications."

Mr. Van Dyke shared a document from Cascade Foods of Albany Oregon, titled *"food safety requirements for delivered product to Cascade Foods"*. (dated April 16, 2020, see attached) In this document Cascade Foods stated;

*" Cascade Foods LLC requires that growers adhere to the Good Agricultural Practices manual set by the Hazelnut Industry office"*. (see attached pdf 202006251).

But nowhere in the Good Agricultural Practices manual is there any reference to a farm losing food safety certification because of litter.

<http://oregonhazelnuts.org/wordpress/wp-content/uploads/2016/05/Haz-GAP-8-12-13.pdf>

It DOES say that the orchards must be constantly cleaned and inspected prior to harvest. This is a critical time. Of course, cleanliness during the year is important, but a clean orchard just before the nuts drop to the floor is strategically important.

Throughout the summer the orchard floor is repeatedly cleaned and leveled by use of drags and/or flail mowers. All debris is eliminated as the floor is pummeled by the repeated passes of the flail mower. Clouds of dust are sometimes seen as the floor is cleaned and manicured over and over. This goes on until the nuts start to fall at the end of August. Now the falling nuts are contained on a clean and pristine orchard floor. Prior to harvest in September/October the operator often inspects the orchard for debris, and removes any. The sweeper and harvester cannot function effectively if there is debris on the orchard floor.

The GAP manual that Mr. Van Dyke has agreed to follow states: *"....orchard floors are cleaned throughout the summer to assure a smooth clean orchard floor during harvest. Of utmost importance to all growers is the need to deliver hazelnuts to the dryer or processor as clean as possible. The Hazelnut lends itself to inherent food safety simply because it is encased in a solid hard shell. It falls to the ground mostly free of the husk, which is a vegetative bract rather than a surgery attraction for microbes. Harvest also lends itself to ensuring safe product. In addition, the product delivered to the washer/dryer or the processor is not ready to eat. It will go through a wash process before it is dried. Growers are committed to employing practices to keep the risk of pathogen contamination as low as possible. As the nuts move to processors and packers more safeguards are in place to ensure safe products leave the plants."*

Litter is NOT a game stopper...it is a item that is constantly evaluated and addressed.

Cascade Food goes on to say; *"The following are a few of the recommendations outlined in the manual; "Grower must participate in traceability. Growers receive a delivery ticket for each load taken to a wash/dryer operator or processor. This*

*ticket corresponds to a specific orchard; Should a food borne pathogen be identified that lot cannot be used for food and will be discarded".*

But the statement; *"Should a food borne pathogen be identified that lot cannot be used for food and will be discarded"* is NOT in the Good Agricultural Manual, and is far from the truth.

In 2017 Salmonella was found in Hazelnuts sold by the Schmidt farm of McMinnville. But the food safety people of the Oregon Department of Agriculture were not worried: "Wholesalers have steps in place that kill any Salmonella on the hazelnuts they handle before the nuts reach consumers," said Stephanie Page, the agriculture department's director of food safety and animal services, in the public warning. "To date, we have no indication there were any issues with this part of the process." <https://www.foodsafetynews.com/2017/01/salmonella-outbreak-traced-to-hazelnuts-from-oregon-farm/>

In fact the GAP manual repeatedly states that orchards must be inspected, (H1 pg 6); *A preharvest assessment is made in the orchard. Risks and possible sources of crop contamination are noted and assessed.* and (H7 pg 6) states; *"In case of product contamination....., product is separated and disposed of separately"* Nowhere does it state that a farmers complete crop will be discarded if contamination is found in one area.

One thing that I had not previously discussed was the fact that there is a thriving **organic** filbert industry in Oregon, which makes 25% more \$\$\$ on their nut sales then we do using horrible chemicals. They don't use any of the noxious chemicals! If they can maintain a successful business without noxious chemicals, why will Ben loose thousands of dollars if he does not spray his edges?

<http://organicfarmermag.com/2020/01/growing-practices-at-the-oldest-us-organic-hazelnut-orchard/>

*Taking the organic leap was difficult, but worth it—not only for Jim's conscience and the good of the earth, but also monetarily. He was able to mark up his organic nuts about 25 percent over conventional-farmed nuts.*

*Jim said. But gradually, he struck a balance with nature. In 1997, Meridian Orchards was certified organic*

*In managing the suckers, David said it's easiest to hit them with an organic herbicide when they're less than eight inches tall. Another grower in the crowd said he uses a side cutter on his mower*

*For weed control they tried Supress and then Homeplate, "which is easier to work with," said David about the latter*

*"Some growers using conventional farming methods think it's impossible to grow hazelnuts organically," Mary said*

A trail and farming are not mutually exclusive! A trail is an opportunity for the farmer to display his skills and to educate the public. And the Yamhelas Trail is an opportunity for the Yamhill Valley to provide a safe mode of transpiration for future generations. No one will suffer "irreparable injury" during its construction or future use. Farmers can still spray their fields. Litter/and or unwanted people, etc. can be controlled by fencing and signage. All it takes is for farmers and trail supporters to sit down and mutually work together to develop a solution that enables us to develop a wonderful asset.

Steven, I reiterate, the label in hand is the one to follow, the label in hand is the law, not whatever other versions might be out there – David Lowe

**From:** Lowe David USGR  
**Sent:** Friday, July 17, 2020 6:01 PM  
**To:** steven.carol.wick@gmail.com  
**Subject:** Gramoxone SL 2.0

Steven,

Attached is the new Gramoxone SL 2.0 label version SCP1431A-L1H 1118 and the old version SCP 1431A-L1F 1115 (probably what your farmer is using). Both Safety Data Sheets (SDS) are attached, they are identical, a label change does not require a SDS change. Both SDS list product number as A7813Q, they are the same product just different label, both 2 pounds of paraquat cation per gallon.

Thank you,

David Lowe

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**Yamhelas Westsider Trail information for Chehalem Park and Recreation District Board  
October 28, 2021**

**Submitted by Veronica Hinkes, Friends of the Yamhelas Westsider Trail Board Secretary**

The idea for the Yamhelas Westsider Trail began in 1991. It grew as a grassroots effort from a community seeing a need, and was at least partially prompted by a child being killed while biking on Highway 47. The proponent group has evolved over the years, and in 2012 became the Friends of the Yamhelas Westsider Trail.

The Friends' mission is education and advocacy for a regional network of trails that support the health, safety and connectivity of our communities. We partner with organizations that share that vision and can help make it a reality. We fully support CPRD's consideration of taking on the Yamhelas Westsider Trail project and hope you will continue to explore the possibility. We believe CPRD's experience in developing recreational spaces and community engagement would be a huge benefit to this project, and is an opportunity to start afresh.

The public opinion survey you recently had done shows there is still strong community support for the trail all around the county. At the beginning of the survey 64% of respondents said they support development of the trail. After hearing messages about both the benefits of the trail and concerns that have been raised, 70% of respondents supported it, including majorities in both the CPRD district and the Yamhill-Carlton School district.

People seem especially interested in providing a safe transportation choice between our communities, increased recreational opportunities for health and fitness, and economic benefits to our local communities. Respondents were also clear that they favor an inclusive process that addresses the concerns of adjacent landowners and includes design elements to mitigate impacts.

Beginning in 2012, Yamhill County and the Friends began an extensive public outreach effort. There have been numerous public meetings, events, hearings, and conversations that were held to gain input from the community and adjacent landowners. We have documented those, including where adjacent landowners were specifically invited, and a list is provided in the packet.

We fully support a planning process that includes input from all concerned parties to find solutions and to plan trail features that will keep it safe and respectful to farmers. If there are other and better ways to engage, we are eager to do so.

Many of the concerns raised about the trail have often been raised before other trails were built. But numerous studies show that those issues usually don't materialize.

We've included more information on these studies in your packets.

Some people have been especially concerned about the potential to attract the homeless and increase crime. Trails do not increase crime, in fact, crime rates on trails are often below those in other nearby locations. Law enforcement people note that the use of trails by the law-abiding public has a dampening effect on crime. Using the Springwater Trail in Portland as a comparison is not realistic. It is an urban trail in a city that allows homeless camping. The homeless tend to stay close to where there are services. Rural trails are different.

We talked to managers of the Banks Vernonia Trail, Rickreall-Monmouth path, and the Row River Trail in Oregon, and the Latah Trail in Idaho. They do not experience any significant problems with trespassing, crime, or homeless camps. The Columbia County sheriff had also previously told us there were not issues on the Banks Vernonia Trail. No-camping ordinances and trail patrols, by both park employees and volunteers, keep issues to a minimum.

It is also important to note that the Land Use Board of Appeals has not denied the trail. They actually declined to reverse the conditional use permit decision and said they do NOT find the trail to be illegal. They did issue a series of remands, denying some of the opponents claims and upholding others, and requiring the County to provide more detail on how it would prevent impacts to farm practices.

All these issues and questions could be addressed in a trail Master Plan, which should include extensive outreach to adjacent landowners and communities. There are thousands of miles of trails through agricultural land around the country, showing they can and do successfully co-exist. I would also point to examples like the Napa Valley AgRespect program—it is a partnership of the local Farm Bureau and tourism and recreation groups to educate trail users about farming practices and trail etiquette.

Another question that has come up is maintenance costs. The Friends of the Yamhelas Westsider Trail had committed to the County to raise funds to help with maintenance costs and to help organize volunteer workers, and would be willing to continue that kind of partnership with CPRD. Ken Wright had pledged to cover maintenance costs for the three-mile section between Carlton and Yamhill and is also willing to discuss this with CPRD as well. There are also grants available that can fund maintenance.

The Friends have also provided letters of support and matching funds for grants in the past, and we could discuss this going forward as well.

The Yamhelas Westsider Trail is a project that was started by the community, and community support remains strong.

Again, we hope you will continue your exploration of this popular public project.

## **Yamhelas Westsider Trail Public Outreach**

Public outreach to gather input for the Yamhelas Westsider Trail began in 2012, with monthly meetings of the Yamhelas Westsider Trail Coalition. To date there have been at least 12 large public meetings, workshops and public hearings, and Friends of the Yamhelas Westsider Trail's information booth has been at more than 30 festivals, farmers markets and other community events. Friends of the Yamhelas Westsider Trail also holds a public Annual Meeting every year.

In addition, FYWT board members have met personally with and had email and phone conversations with concerned property owners numerous times to answer questions and address specific concerns.

Public input surveys were done in 2013 as part of a grant process and in 2015 as part of the Concept Plan process.

Details on some of these meetings:

Annual meetings of Friends of the Yamhelas Westsider Trail: every December or January since 2012. These have all been public meetings.

February 19, 2014-Public meeting, Yamhill Carlton HS cafeteria  
Some adjacent landowners in attendance.

April 16, 2015 Open House at McMinnville Community Center. Mailed letter from FYWT President Lyell Gardner to all adjacent landowners using tax lot list from County.

October 7, 2015- Public Forum at YCHS Cafeteria. Mailed postcard to all adjacent landowners and FYWT master mailing list

November 13-14, 2015 Concept Plan Charrette.

November 13<sup>th</sup>—small groups toured portions of right-of-way with architects and National Park Service consultant.

November 14, day: Design Charrette held. Invitations were mailed and emailed to potential stakeholder participants, including some adjacent landowners

November 14, evening: Charrette follow-up Open House. Invitations mailed to entire FYWT mailing list.

Follow-up meetings in Yamhill, Carlton and Gaston, adjacent landowners and businesses invited.

Concept Plan presentation May 24 2017, McMinnville Community Center. Postcard mailed to adjacent landowners (also with info on community dialogue)

June 8, 2017 Community Dialogue with County Commission

Public Hearings:

May 3, 2018, planning commission

May 15, 2018, Board of Commissioners

Both held to consider amendment to county comprehensive land use plan to recognize county purchase of ROW

November-December 2020: Public input map and survey on Master Plan website.

December 2, 2020 and December 10, 2020: Master Plan open houses (virtual) and Project Advisory Committee meeting

December 2020: Master Plan meetings with adjacent landowners

# Editorial

A7 • Wednesday, October 20, 2021

Pamplin  
Media Group

## Trail would be a boon to Yamhill County

### ■ Anti-trail opinion seeks president of pro-trail organization response

A recent op-ed in the Graphic regarding the Yamhill Westsider Trail, a proposed rail-to-trails project, contains misinformation that begs a response.

In the op-ed, the author says the trail is "little known" and implies that its planning process has been covert. The trail is proposed on a publicly owned right-of-way that has been a transportation corridor for 150 years. It has been a part of Yamhill County's

transportation plan since 2015.

Beginning with monthly meetings in 2012, there have been dozens of opportunities for adjacent landowners to participate in the process. These included large public outreach meetings, planning efforts like the Concept Plan Charrette in 2015, public hearings and listening sessions, outreach booths at more than 30 community events, and conversations and meetings between trail supporters and neighboring landowners and communities.

Adjacent landowners were directly invited to participate from the beginning. Some declined to participate, others stopped participating at the request of an attorney hired by trail opponents.

Interestingly, Lee Schrepel, the farmer behind the recent anti-trail opinion piece, told a trail proponent in

2012: "We are fully in support of your rail-to-trails project. I think that what we're planning here is fully compatible with your recreational use."

Opponents claim the trail will hurt farming. Fencing, gates, temporary trail closures, signage and other tools deployed through trail planning and design can protect farms from disruption. This type of planning is most successful when neighboring property owners participate in the planning process and collaborate to find solutions.

The Land Use Board of Appeals has not outlawed the trail. Instead, LUBA denied some opponents' claims and upheld others. They required Yamhill County to provide more detail about how adjacent farming practices will be protected. The county was in the midst of a master planning process to provide these details when the com-

mission's new anti-trail majority stopped all work on the trail.

The opponents have claimed that food safety will be compromised by the trail. The 60-foot wide Yamhill Trail right-of-way would allow a 24-foot buffer on each side of the 12-foot trail. Furthermore, fencing would add an extra layer of protection for food crop safety.

Another concern has been the potential to attract homeless camps. While opponents cite Portland's Springwater Trail, trails through rural areas have not experienced this problem, as they are not close to services that the homeless use. Many studies examining working trails show that a steady population of users actually decreases undesirable behavior on previously undeveloped rights of way.

Opponents claim that the Oregon

Department of Transportation has

ready canceled the project. In fact, ODOT and the county are negotiating a contract amendment and ODOT was satisfied to continue the project as long as the county tent was to open a public bicycle/designed trail. ODOT demands payment only after county commissioners voted to stop all trail work.

Trails are proven assets to communities throughout the country. State and local anecdotal evidence shows that neighboring trail opponents rarely become trail supporters once the trails are built.

If other communities can find successful solutions, surely Yamhill County residents can as well.

Wayne Wiebke is president of Friends of the Yamhill Westsider Trail.

## READERS' LETTERS

### ■ From Page A6

#### Political stereotypes are hurtful

Reading an essay by David Brooks published this week entitled "Here's the Mindset That Is Tearing Us Apart" made me think of Dundee Mayor David Russ's comments published in the Oct. 6 issue of the Graphic. Russ remarked that "Democrats have, and continue to, abuse the Constitution and the citizens of" the U.S. of A. and that, presumably with him as a candidate, "no reasonable thinking human will be able to vote Democrat with a clear conscience."

In critiquing this kind of talking (coming from every political persuasion, I might add), Brooks writes: "Politics is never about argument; it's jamming together a bunch of categories about people who are allegedly rotten to the core."

He concludes by stating: "point of respect."

They were healthy, safe environments in which kids thrived, and wonderful places to work.

My experiences taught me this: school cultures are created by the adults. However implicitly, boards and administrations establish values and set the tone. If they cherry-pick only procedures and rules of their liking, what does this say to the students? If they suggest that disenfranchised groups aren't worthy of a voice, what does this say to the students? Responsibility for recent bullying and acts of racism rests squarely with the adults.

In the best schools, a key best practice in promoting healthy and harmonious learning environments is to develop and support orientation programs for new students and incoming freshmen. These programs define the values and expectations of a school and teach students about the various cultures contained within the school from a viewpoint of respect.

One man laughed — his brothers are gay. Like Twitter and toddlers.

Our group was counterprotesting their presence. They met on North College; we met at the flagpole. They moved to us. One posted a video alleging we harassed them — it was the opposite. We did outnumber them, thankfully.

After taking a photo of one man and being followed, I heatedly debated with them. They claimed the BLM organization is fraudulent, but we weren't there for an organization.

"Black Lives Matter" shows baseline support for BIPOC humans. Pride symbols show support for LGBTQ+ humans.

They said men are the ones who built this great country. I said immigrants built this country for white men who profited. One man (responded) "White immigrants!" Me: "And black slaves who were kidnapped."

When I stated they're also responsible for the capitalism we have, they called me social-

The garden was installed decades ago to symbolize friendship and connection between students in Newberg and students in Wadaya, Japan. The Hiroshima Memorial Tree is displayed to symbolize peace and the growth that can occur despite hardship and tragedy.

The board passed a policy banning political and controversial symbols. The board is the final arbiter of what is banned, so I have requested a formal exemption to the Garden of Friendship and the Hiroshima Memorial Tree.

Their approval is what is important under the policy, not the intent of the symbol.

Some argue that these symbols touch on some of the most controversial topics of the past 100 years: How to reconcile and create friendship with former enemies? What was the strategic and moral value of using atomic weapons? Are we favoring one culture over others that don't get their own garden? With increasing anti-

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# CITY OF GASTON

116 Front Street, P.O. Box 129,  
Gaston, Oregon 97119 Phone (503)985-3340  
[www.cityofgaston.com](http://www.cityofgaston.com)



August 2, 2018

Carrie Martin  
Grants and Special Projects Manager  
Yamhill County  
434 NE Evans Street  
535 NE Fifth Street - mailing  
McMinnville, OR 97128

RE: Yamhelas Westsider Trail

To Whom it May Concern,

The City of Gaston continues to support the development of the Yamhelas Westsider Trail (YWT). The YWT will be connecting the communities of Carlton, Yamhill and Gaston with the vision of one day being anchored in the North by Forest Grove and in the south by McMinnville. The YWT will have a positive economic impact on area business through increased tourism and healthy recreation. The YWT will provide the students of Gaston School District with a safe non-vehicular alternative to Highway 47 for getting to and from school.

We look forward to the development of the Yamhelas Westsider Trail and the improvements to our community that will follow. Please, if there is any we can be of further assistance, let us know.

Sincerely,

Tony Hall  
Mayor  
City of Gaston



---

September 9, 2020

RE: Yamhelas Westsider Trail

To Yamhill County Commissioners

The Carlton City Council and our community continue to support the development of the Yamhelas Westsider Trail. This trail will be a vital connection for the communities of Carlton, Yamhill, and Gaston.

Highway 47 is currently the only suitable connection between our communities and is not safe for biking or walking. There have been many close calls and accidents from the lack of a safe way to walk or bicycle in and between our communities. The trail will give us a much-needed connection for Carlton and Yamhill students, especially between the schools and homes in our school district.

We look forward to the development of the Yamhelas Westsider Trail and the improvements to our community that will follow.

Sincerely

Brian Rake  
Mayor  
City of Carlton



September 5, 2020

To whom it may concern,

The Yamhill County Parks and Recreation Advisory Board believes there are many reasons to create the Yamhelas Westsider Trail.

The Trail aligns with the goals and mission statement described in the County's Parks and Open Space Master Plan. The statement reads, "Yamhill County parks and open spaces are unique natural, cultural, and historic places to be managed, preserved, and restored for people to enjoy outdoor activities and educational opportunities." The Trail will provide safe exercise opportunities for walking, jogging, and biking, and an opportunity for equestrians to enjoy the Yamhill Valley. What better way to enjoy outdoor activities as a family than to utilize a trail system that is safe for them to enjoy together?

Moreover, natural and recreational sites near cities are very important as populations grow. The trail will have close-in easy access for residents of Gaston, Cove Orchard, Yamhill, Carlton, and McMinnville. When the Trail ultimately connects the Wapato National Wildlife Refuge outside of Gaston, there will also be opportunities to experience an array of wildlife inside the refuge.

The Parks Board supports education efforts and preservation of historic and cultural locations in the county, as laid out in the Master Plan. The Yamhelas Westsider Trail will be a place to celebrate local history, through informative signs along the Trail and through presentations on cultural and historical points of interest going back to the times when native indigenous people lived here.

The Parks Board also supports efforts to create safe routes to school throughout the county. Thus, it is important to note that the first section of the Trail between Yamhill and Carlton will connect two schools in the same district. As a priority recommendation in the 2015 Yamhill County Transportation System Plan, and adopted with community support, the Trail will become an alternative route to using Highway 47, encouraging students to actively participate in getting themselves to and from school. This will not only increase the health of the student population but will also allow for more robust after school activities because they will not need to depend on bus transportation scheduling.

Finally, the Trail has wide community support and the Parks Board values home-grown efforts to conserve land and create recreational opportunities that will serve to bring communities together in the county. Among the supporters of the Trail are members of the health, civic and church communities, as well as city governments and community associations.

For these reasons the Yamhill County Parks and Recreation Advisory Board strongly supports the creation of the Yamhelas Westsider Trail.

Sincerely,

James H. Culbert  
Parks and Recreation Advisory Board Chair





To: Yamhill County Board of Commissioners

January 25, 2021

Commissioners: On behalf of the Yamhill Downtown Association, I would like to make you aware of how much the Yamhelas Westsider Trail fits into our plans to revitalize the city of Yamhill.

The YDA has been discussing all the community benefits of this trail; the economic and health benefits are significant for area merchants and residents. Communities that have trailheads along bike and pedestrian trails often see people coming to ride there and spending money when they do. Yamhill County would benefit from a safe recreation and inter-community pedestrian corridor.

Losing the Trail would be a drastic loss for Yamhill and for the Yamhill Downtown Association. Allowing the Master Plan process to proceed is vitally important for our communities.

The Yamhill downtown has potential for increased commerce, but foot traffic in downtown is currently limited. As we work to improve our downtown, the prospects of new business opportunities related to the presence of a community-friendly trail are significant.

But perhaps the best reason to keep this right-of-way protected is that the stretch between Yamhill and Carlton joins two cities that share the same school district. There is currently no place for students, parents, and residents to walk or ride safely between the two cities.

There is so much support for the trail from many people in Yamhill County. The Master Plan should be completed using the grant money which has already been allocated.

Steve Harloff  
Secretary  
Yamhill Downtown Association  
16175 Puddy Gulch  
Yamhill, Oregon 97148

**CASCADE FOODS LLC**

**38471 Groshong Rd NE  
Albany OR 97321  
(541) 924-1477**

April 16, 2020

**Food safety requirements for Ben VanDyke Farms for delivered product to Cascade Foods**

To Whom It May Concern,

Cascade Foods LLC is an Oregon based hazelnut processor with a lengthy history of compliance with all Federal and State food safety regulations. Cascade Foods LLC operates under the Federal Drug Administration regulations and uses a HACCP based quality system that is audited by a third party following the Global Food Safety Initiative Standards. Under these standards, Cascade Foods LLC maintains many pre-requisite food safety programs such as Good Manufacturing Practices, pest and chemical control, sanitation processes, employee training, product traceability and recall programs. Cascade Foods LLC also uses quality control programs to ensure that all products are manufactured to meet established specifications.

Cascade Foods LLC requires that growers adhere to the Good Agricultural Practices manual set forth by the Hazelnut Industry office. The following are a few of the recommendations outlined in the manual:

- Runoff from septic, lagoons, or municipal or commercial sewage treatment facilities are kept out of orchards as prescribed by the Department of Environmental Quality. Crop production areas are observed for the presence or signs of wild or domestic animals during routine work in the orchards. If raw (domestic or wild) manure is applied, it is applied a minimum of nine months prior to harvest. Use of municipal biosolids, whether Class A or B, is applied as prescribed by the Department of Environmental Quality. Untreated sludge is not approved by the DEQ.
- The number, condition and placement of field sanitation units comply with Division 4/J: 437-004 1110 Field Sanitation for Hand Labor Work and the requirements of the Oregon Department of Environmental Quality. Toilet facilities must be available. Field sanitation units are located in a location that minimizes the potential risk for product contamination and are directly accessible for servicing.
- Grower must participate in a traceability. Growers receive a delivery ticket for each load taken to a wash/dryer operator or a processor. This ticket corresponds to a specific orchard. Should a foodborne pathogen be identified that lot cannot be used for food and will be discarded.

Product is free from foreign material. Trash in field and/or in delivered product poses a food safety liability. Trash can carry vectors that can cause foodborne pathogens.

Food safety and quality is the number one priority for Cascade Foods LLC. We must supply our customers with the best quality product, free from foodborne pathogens. At Cascade Foods LLC, we believe this starts at the grower level and continues through the entire processing system.

Sincerely,

A handwritten signature in black ink, appearing to read "Greg Riches", with a stylized flourish at the end.

Greg Riches

CEO

Cascade Foods LLC

# YAMHELAS WESTSIDER TRAIL

*CONCEPT PLAN*  
Winter 2017



FRIENDS OF THE  
YAMHILL WESTSIDER

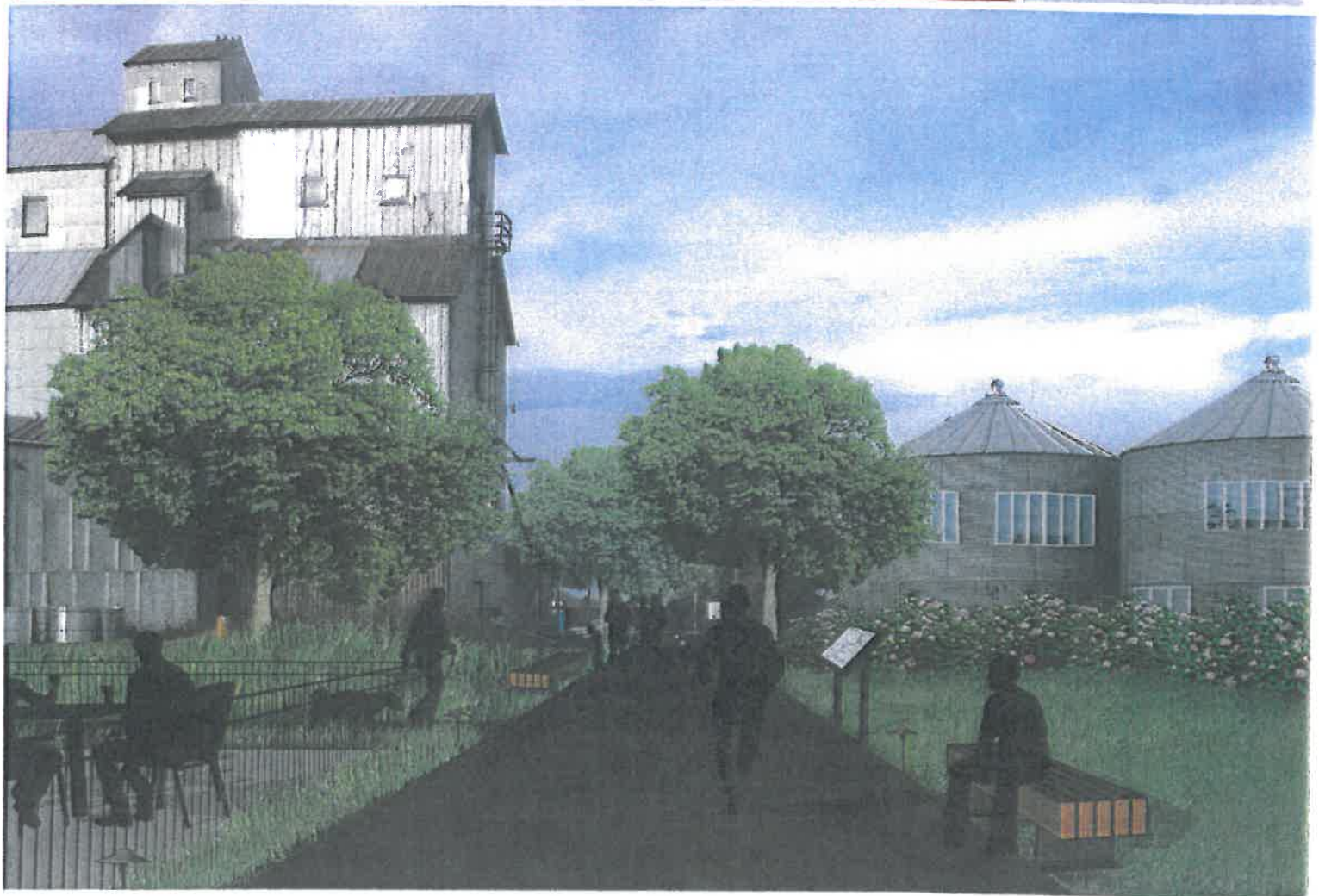
IN  
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technical assistance  
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RIVERS, TRAILS,  
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## DEDICATION

*Energy, vision and patience. These are the requisite qualities needed to see a project through that has the scale of the Yamhelas Westsider Trail.*

*The effort began in 1991 with a coalition led by our current President Wayne Wiebke. This was the first of three separate and subsequent groups that have dedicated countless hours to develop support for the trail.*

*The Yamhelas Westsider Trail members, on the eve of the first purchase of a portion of the trail, wish to dedicate the progress that has been made to Wayne, and two dear friends of the trail that we lost in 2016, Lyell Gardener and Gordon Zimmerman. Lyell, a former Washington County commissioner, was pivotal to the Yamhelas creation and served as President until health prevented his ability to attend meetings. Gordon, a lover of all things railroad like non other, championed the connectivity and vitality that the trail would bring to every community it touched. These three individuals are the embodiment of leadership. All future success for the trail will be built on the foundation they laid. We thank them.*

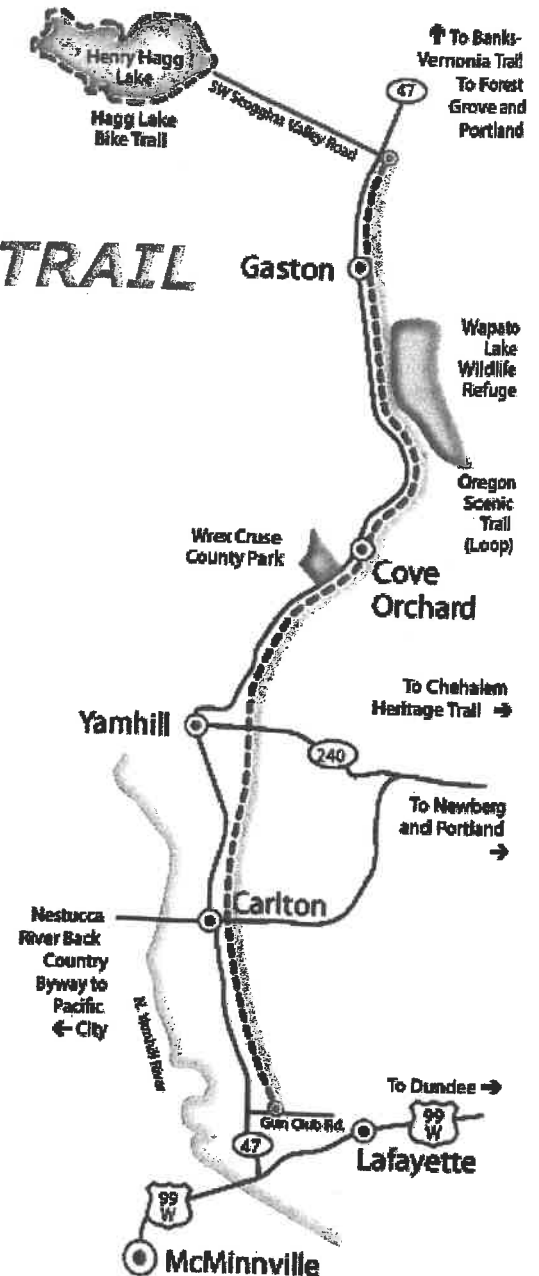
*The Board of Directors  
Yamhelas Westsider Trail*

# YAMHELAS WESTSIDER TRAIL

## INTRODUCTION

The goal of this document is to give a visual and textual overview of the Yamhelas Westsider Trail, a multimodal transportation corridor parallel to HWY 47 in Yamhill and Washington Counties. This will serve as a visual guide for the trail that has been written by key stakeholders including members of the Friends Yamhelas Westsider Trail, Yamhill County elected officials and staff members and National Parks Service staff. Further, this document was informed by the general public that attended public meetings and focus groups on the development of the project.

This document is intended to be a conceptual plan which does not go into detail about exact locations, number of amenities, or contain engineering drawings; as well, no cost analysis or business plan has been developed. It is meant to illustrate what the trail will look like. After reading this plan, one should have an idea of the character of the trail corridor and where design elements such as trailheads and road crossings may be located. Before the trail is built, detailed construction and engineering documents will need to be generated for structures such as bridges and trailheads. It should be noted that this concept plan is not meant to be fixed or static. Instead, it is a 'living' document which can be updated as the effort progresses and the needs arise.





# FREQUENTLY ASKED QUESTIONS

## **Will private property rights be infringed upon?**

No. The majority of the 17 mile railroad line from Gun Club Road to the Henry Hagg Lake junction is owned by the Union Pacific Railroad (UPRR). The railroad abandoned the use of the corridor for active transportation, but maintained ownership of the property. Yamhill County will purchase the corridor from the UPRR, except for a few parcels that are now in private ownership. For two of these, the owners have agreed to allow trail access in various forms, such as by donation to Yamhill County or granting an easement. The boundaries of the entire corridor have been surveyed as part of the requirements for obtaining the Oregon Department of Transportation acquisition funding. The Friends of the Yamhelas Westsider Trail and county staff are working to address any concerns identified by adjacent property owners.

## **Is this rail line being rail-banked?**

No. This is a purchase of the railroad corridor from a willing seller, Union Pacific Railroad (UPRR).

## **Will the development of the trail effect my farming practices?**

Not likely. Many trails just like this one have been developed adjacent to working farms with no adverse effects either to trail users or to the farmers' ability to manage their farms. The concept plan for the Yamhelas Westsider Trail begins to address agriculture issues, and conversations will continue to take place on a case-by-case basis. See below for a discussion on crossing the trail to access a field.

## **What if I must cross the trail to access my property?**

All current easements across the UPRR right-of-way will be honored and maintained as part of the trail design. If you are currently crossing the UPRR right-of-way and don't have an easement/agreement, it is important to know that you have no legal right to your crossing. However, Yamhill County wants to work with you to develop a legally recognized crossing so that you can cross the trail safely for continued access to your property.

## **Will there be an increase in trespass, littering, vandalism, and other illegal activities?**

Actually, the vast majority of trails have experienced a decrease in unwanted activities such as those listed above (Tracy). The reason is that people tend to perform illegal activities in places where there are no reputable citizens frequenting the area. In trail after trail, people have seen the rail corridor transformed from a place that is unkempt and rife with litter to one that becomes an amenity for the area that showcases its natural beauty and heritage. Fencing and/or visual screening will be used where trespassing onto private property is of a particular concern. The trail will be managed by Yamhill County and the Friends of the Yamhelas Westsider Trail and be subject to the rules and regulations approved by the County Commissioners. For more information and examples of other trails, please visit: < [https://safety.fhwa.dot.gov/ped\\_bike/docs/rt\\_safecomm.pdf](https://safety.fhwa.dot.gov/ped_bike/docs/rt_safecomm.pdf) >

## **How will the trail manage and enhance public safety?**

At present, State Highway 47 is not a safe route for pedestrians and bicyclists. The trail will provide a safer transportation corridor along the same general route. This will be especially important between the cities of Carlton and Yamhill, which share the same school facilities. Also, it will be necessary for the trail to cross some paved and unpaved roads, and this situation will be no different than all of the other rail to trails in the country that are being used safely. Appropriate trail design at these crossings, along with the placement of appropriate signs and other markers, will be adequate to insure that motorists and trail users use appropriate caution at these crossings. For more information and examples of other trails, please visit: < [https://safety.fhwa.dot.gov/ped\\_bike/docs/rt\\_safecomm.pdf](https://safety.fhwa.dot.gov/ped_bike/docs/rt_safecomm.pdf) >

## **What about emergency services and fire response?**

A benefit from developing a multi-use recreational trail is that emergency services have better access to the corridor than they ever have had. The trail has a predominantly flat and wide surface so that a police car, fire truck, or ambulance can drive right down the corridor with ease. This allows for faster response times and increased protection for residents along the corridor.



**Who will construct and maintain the trail?**

Options for construction and maintenance will be analyzed in the trail planning process. One of these is the creation of a public/private partnership. Money for construction will most likely come through state, federal and private funding sources (grants). Yamhill County will contribute internal resources to the maintenance of the trail, just like other county properties, and additional maintenance needs will be addressed with funds raised by the Friends of the Yamhelas Westsider Trail, a 501c3 non-profit organization.

**Who is involved in the planning of the project?**

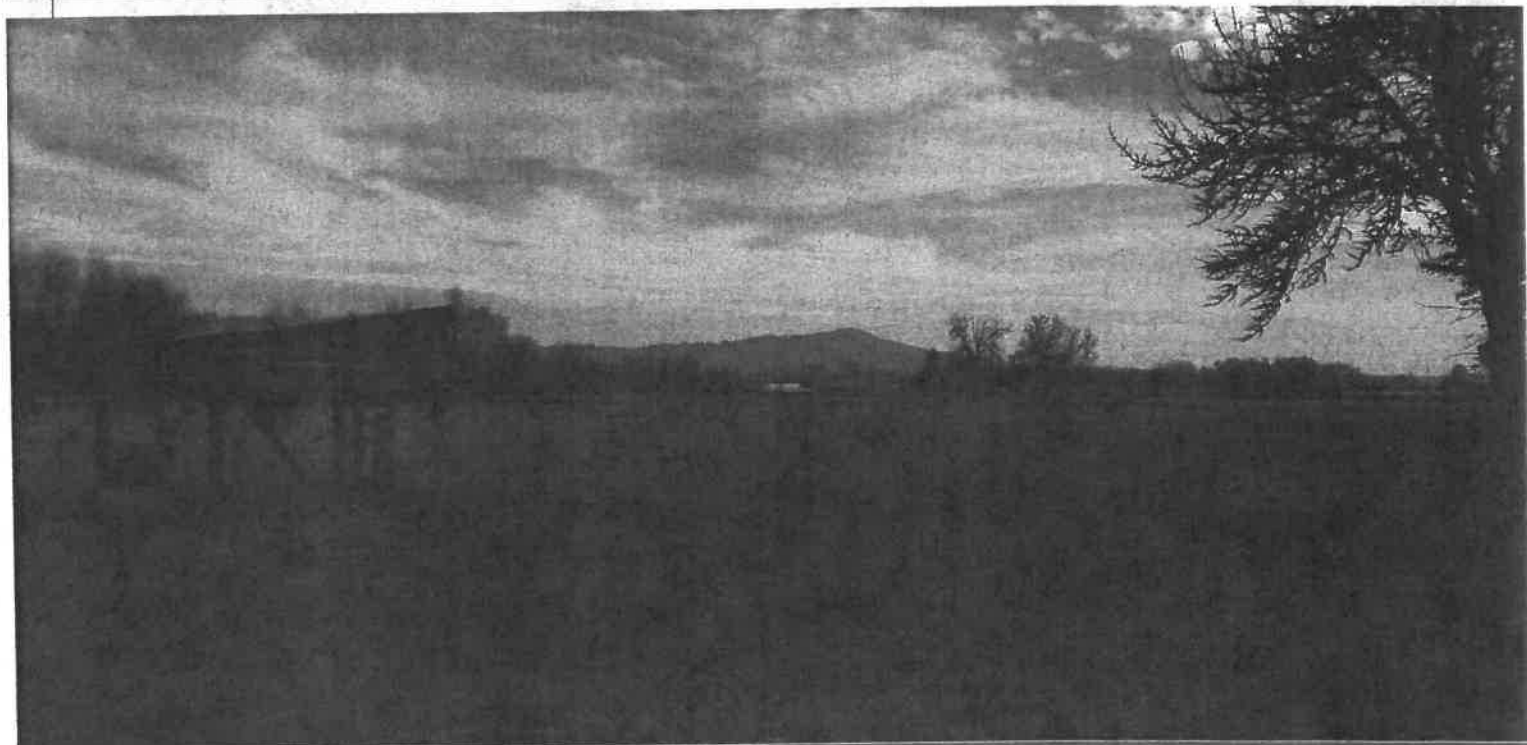
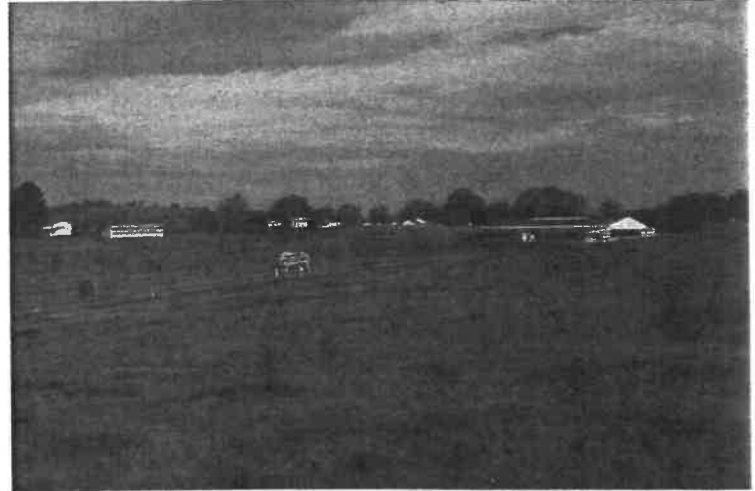
The Friends of the Yamhelas Westsider Trail and Yamhill County are leading the planning and development of the trail. We are actively working with Washington County, City of Carlton, City of Yamhill, City of Gaston, and other stakeholders. The Friends and Yamhill County received a technical assistance grant from the National Park Service's Rivers, Trails and Conservation Assistance Program ([www.nps.gov/rtca](http://www.nps.gov/rtca)) that helped the Friends conduct a public survey and go through an initial public collaborative planning process to better understand the desires and concerns of the community. In November 2015, the Friends and the NPS hosted a group of landscape architects from the American Society of Landscape Architects to work with stakeholders to draw up initial conceptual plans. The Friends have also hosted several public meetings for the purposes of informing the public and taking public input.



# BACKGROUND

The Yamhelas Westsider Trail is a multi-modal transportation corridor located in the heart of Oregon's scenic wine country. Primarily in Yamhill County, the 17 mile trail runs parallel to Highway 47 in the now abandoned Union Pacific Railroad (UPRR) right-of-way. The project aims to convert the right-of-way into an active transportation corridor beginning at Gun Club Road south of the city of Carlton and continuing north to the city of Gaston, eventually ending at Scoggins Creek. It is ultimately envisioned that the trail will connect to McMinnville and potentially to Forest Grove in the future.

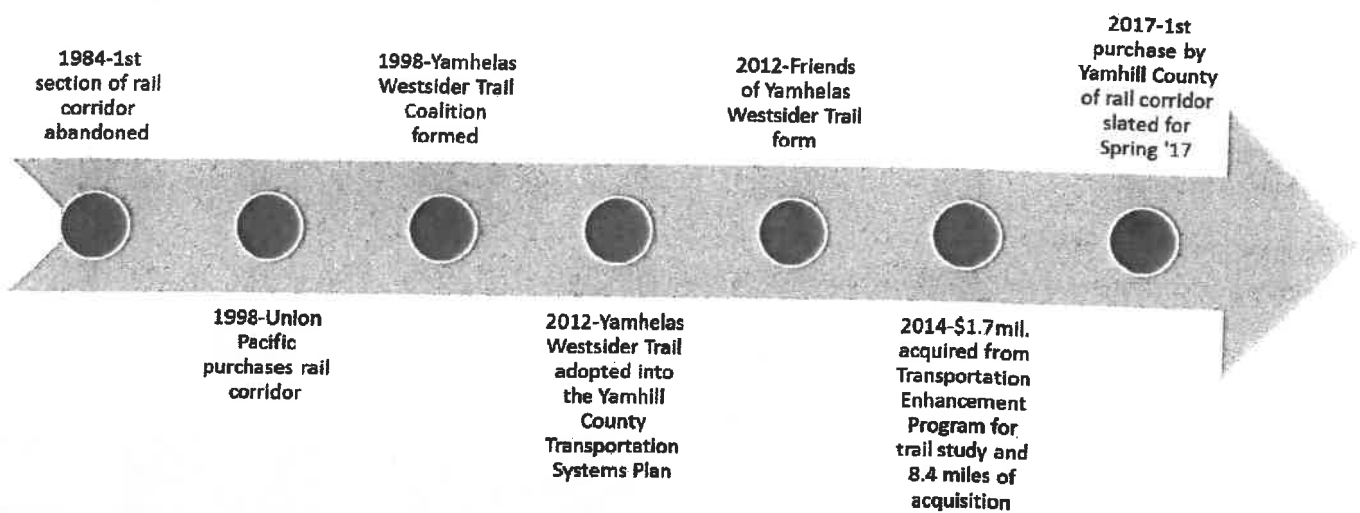
Situated amongst the rolling hills of the Willamette Valley, the Yamhelas Westsider Trail has magnificent views of Oregon's Coastal Range and passes through Century Farms. The towns of Carlton, Yamhill, Cove Orchard and Gaston all lie along the trail route, and access is readily available from the country roads it crosses. Linking the four cities and two counties, as well as several regional and cultural recreation sites, the acquisition of the right-of-way would preserve the corridor in one single public ownership. Furthermore, the trail would provide one continuous corridor for alternative transportation methods, and enhance recreational opportunities within the area, as it seeks to connect with other regional trails and designations.



Actively working with Yamhill County, the Friends of the Yamhelas Westsider Trail have been partnering with various citizens, local businesses, and partner agencies to acquire the abandoned right-of-way since the early 1990's. As the first section of the rail corridor began to become abandoned in 1984, Yamhill County Commissioners briefly considered purchasing a 12 mile section of the right-of-way between Carlton and Yamhill. It wasn't until 1991, when a small group from Carlton and Yamhill contacted Southern Pacific Railroad to begin the process of exploring the conversion of the railroad into a trail. This effort was highly motivated by the death of a student riding his bicycle along Highway 47. However, this effort was halted as Southern Pacific Railroad began to enter negotiations for the sale of the right-of-way to Union Pacific Railroad. After a successful purchase by Union Pacific Railroad, negotiations to acquire the land was resumed in 1998 when another group formed under the name of the Yamhelas Westsider Trail Coalition. However, this effort was quickly put under a moratorium as Union Pacific sought to better understand the rail corridor they had purchased from Southern Pacific Railroad.

The current organization, the Friends of the Yamhelas Westsider Trail, began in the spring of 2012, when an "over the fence" appraisal reduced the Union Pacific price of the right-of-way from over \$9 million dollars to \$2.4 million dollars. This made acquisition of the entire abandoned rail corridor a possibility. The subsequent community, and governmental support, along with grants, has now put the acquisition of the corridor in sight for the spring of 2017.

#### YAMHELAS WESTSIDER TRAIL TIMELINE



# PLANNING PROCESS

## NATIONAL PARK SERVICE PLANNING SERVICES

Concept planning for the Yamhelas Westsider Trail began in the winter of 2015 with an award of a technical assistance grant from the National Park Service's Rivers, Trails and Conservation Assistance Program (NPS-RTCA). NPS worked with Yamhill County and the Friends of the Yamhelas Westsider Trail (the Friends) to host an open house in McMinnville in the spring of 2015. The open house was intended to provide information about the project and to gain input into issues, concerns and opportunities as seen by the public and stakeholders.

## PLANNING TEAM

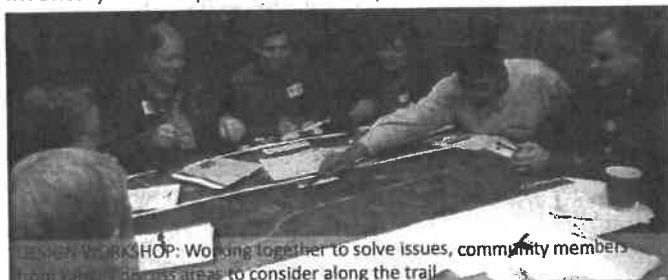
NPS worked with a planning team consisting of Yamhill County and local municipal staff, local county elected officials, and board members and key volunteers from the Friends of the Yamhelas Westsider Trail. The planning team helped to host open houses, review planning documents, and finalize the recommendations within this plan. Many of the planning team members also contributed to writing sections of this plan.

## COMMUNITY SURVEY

An online community-based survey was launched prior to the open house in McMinnville (and available to take on laptops at the open house in April of 2015). This survey continues to be left open throughout the planning process. At writing of this plan, 276 people have taken the survey and provided valuable input used to generate this plan. It should be noted that this survey is not scientific and does not have any statistical validity. In order to conduct a scientific survey, a randomly selected segment of the population is needed. Instead, this survey was designed to allow for everyone who wanted to provide input to do so.

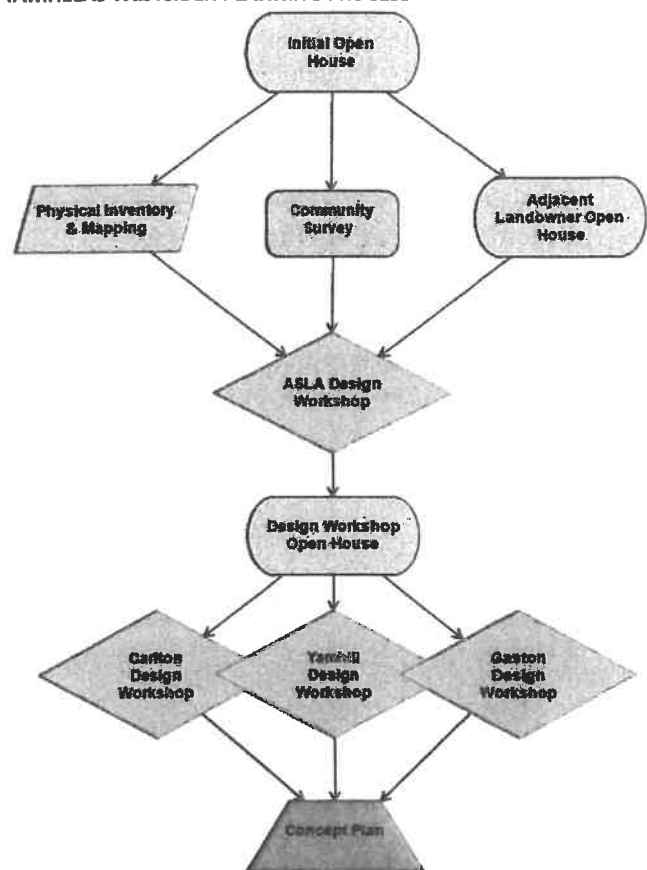
## PHYSICAL INVENTORY

As part of the Transportation Enhancement grant awarded by the Federal Highways Administration (FHA), the firm of OBEC performed an inventory of much of the rail corridor. This inventory included ownership and topography and was completed in the fall of 2015. This data along with information gathered by NPS formed the basis of the physical inventory for this plan and the maps contained within.



DESIGN WORKSHOP: Working together to solve issues, community members from Yamhill discuss areas to consider along the trail.

## YAMHELAS WESTSIDER PLANNING PROCESS



DESIGN WORKSHOP: Community members map out potential opportunities in Carlton.



DESIGN WORKSHOP: Community members work to restore connections to McMinnville.



## ADJACENT LAND-OWNER OPEN HOUSE

A second open house in Yamhill was hosted in October of 2015. It was open to the public but targeted adjacent landowners and business owners along the proposed trail corridor. This open house focused on where the landowners were located and what issues and opportunities they had. Participants were informed that collected information would be used at upcoming design workshops that were held on the 13th and 14th of November 2015.

## DESIGN WORKSHOP

Adjacent landowners, stakeholders, county and municipal leaders and staff worked alongside landscape architects from the Oregon Chapter of the American Society of Landscape Architecture (ASLA Oregon) in an intensive two-day design workshop (also known as a charrette) in mid-November of 2015. NPS and ASLA Oregon collaborated to plan and conduct the design event that took place in Carlton. Participants were divided into seven tables that were paired with at least two professional ASLA landscape architects and one student from the University of Oregon's ASLA chapter. Five of the tables focused on different and distinct geographical sections of the trail corridor. These tables were asked to look at their specific section, assess potential issues, and make recommendations to solve those issues. Each table also focused on recommendations for trailhead placement, bridge construction, trail-road intersections and agriculture crossings, among other design elements. The other two tables tackled topics such as interpretation, trail branding, design guidelines, safety and emergency access, etc.

## DESIGN WORKSHOP OPEN HOUSE

Immediately after the conclusion of the design workshop, participants showcased their work to the public at an open house. The public was invited to weigh in on what elements of the designs they liked and which they disliked. This information helped to give participants of the design workshop and the planning team immediate feedback on their two day efforts.

## STAKEHOLDER WORKSHOPS

After the charrette in November, three additional evening workshops were held to look more closely at the work generated from the November design event and fine-tune recommendations for this plan. These three workshops focused on specific sections of the trail and were hosted in Carlton, Yamhill and Gaston. Those who were invited included charrette participants, local and county staff and officials, adjacent landowners, as well as business owners along the corridor. Results from these stakeholder workshops were brought to the planning team who finalized them and helped generate this concept plan.





# NATURAL RESOURCES

## GEOLOGY AND SOILS

### *Geological Description*

Situated in the northwestern most extent of the Willamette Valley, the Yamhelas Westsider trail corridor lies on relatively flat land with the exception of the gently rolling hills that stretch between the towns of Yamhill and Cove Orchard. With the foothills of the Coastal Mountain Range visible to the west, the Yamhelas Westsider corridor boasts beautiful vistas that make this a highly sought-after scenic area in Oregon.

The Willamette Valley itself is an elongated alluvial plain surrounded by the Coastal Mountain Range to the west and the Cascade Mountains to the east. The Willamette River flows northward through the valley, from Springfield to Portland, for a total distance of 187 miles. The northern boundary of the valley is defined by the confluence of the Willamette and Columbia Rivers; with the Columbia River continuing to head west, eventually draining into the Pacific Ocean (Balster). Roughly 130 miles long, and ranging from 20 to 40 miles wide, the valley is an area of low relief. Elevations within the valley vary, and are as low as sea level near Portland, and as high as 450 feet at the southern end near Eugene (Balster). Within the trail corridor, the elevation north of Cove Orchard drops and wetlands are found in the floodplain of Wapato Creek and the Tualatin River. The northern end of the trail borders the Wapato Lake National Wildlife Refuge near the town of Gaston.

### *Major Historical Geographical Events*

Several large-scale geological events helped shape the Willamette Valley. From 35 million years ago until about 12 million years ago, the Willamette Valley was under water as it was located on the floor of the continental shelf of the Pacific Ocean. During this time, layers of marine sediments slowly accumulated and formed the bedrock or oldest soil. Examples of these soils are: Willakenzie, Bellpine, Chuhulpim, Hazelair, Melbourne and Dupee (Oregon).

The collision of the Pacific and the North American tectonic plates began some 15 million years ago. This force pushed Western Oregon upward creating the Coastal Range Mountains and the volcanic Cascade Range Mountains (Oregon). The result left the Willamette Valley as an ocean floor between these two mountain ranges. Lava from frequent volcanic eruptions along the Cascades rushed down the Columbia Gorge, to the Pacific Ocean, leaving layers of basalt over the marine bedrock. Examples of basalt soils found in the valley include: Jory, Nekia and Saum (Oregon).

A layer of windblown silt, known as Loess, helped create the northeast facing hills west of present day Portland Metropolitan area. This began about one million years ago and lasted until about fifty thousand years before present day. These silts were made up of weathered basalts that originated from the valley floors. An example of Loess found in the Willamette Valley is Laurelwood (Oregon).

During the end of the last Ice Age, in the Pleistocene Epoch Era (between 15,000 and 13,000 years ago), a massive melting of glaciers around the former Lake Missoula in western Montana, created a series of widespread floods now known as the Missoula Floods (or Spokane Floods or Bretz Floods) (Missoula Floods). A series of ice dam ruptures caused waters of Lake Missoula to flood the Columbia River Gorge and Willamette Valley each spring. These floods left behind deep silts and deposits from as far away as Eastern Washington, Idaho, Montana and even present day British Columbia and Alberta, Canada (Campbell). Large ice chunks floated through the valley and when the waters receded massive rocks called glacial erratics were deposited, and the soils that remained created a deep-rich silt layer. This helped form the rich soils now used for a highly productive agriculture region. Examples of the Missoula Flood deposits are: Wapato, Woodburn and Willamette soils (Oregon).

Mountainous country-side near Gaston, Oregon

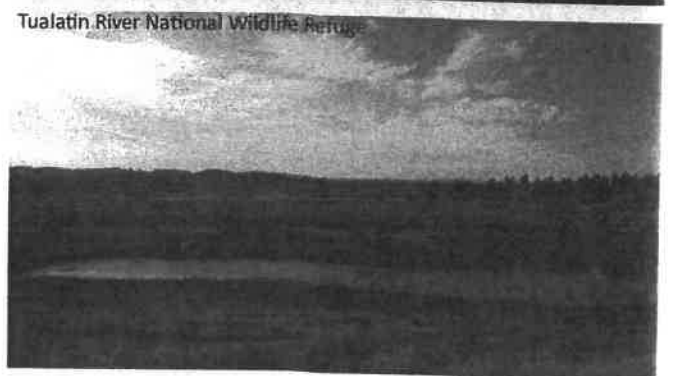
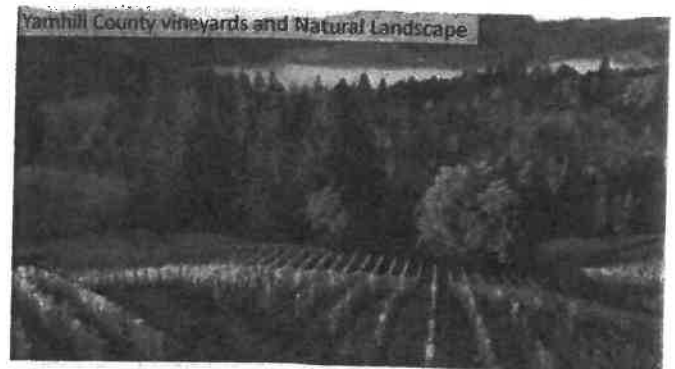
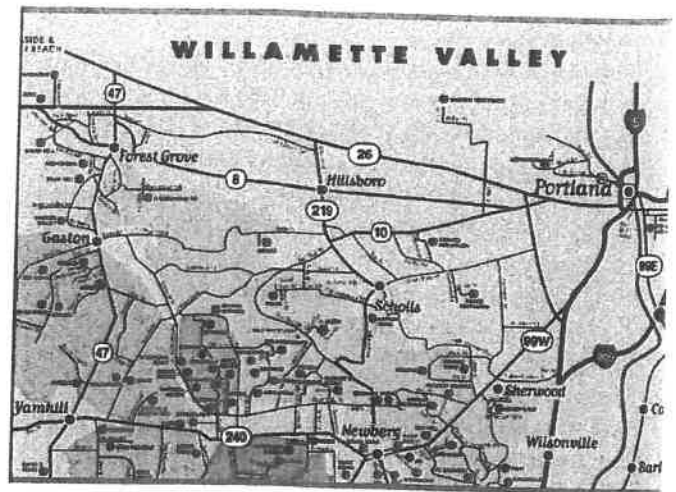
## NATURAL HISTORY

### *Habitat Description*

The Willamette Valley is the fastest growing region of Oregon and contains the three largest cities in the state (Portland, Salem, and Eugene). The region produces half of the state's agriculture in a mere 5,308 square miles ("At a Glance" - Characteristics and Statistics, Willamette Valley Ecoregion, ODF&W, pp. 235). Prior to European settlement, the Willamette Valley was a mixture of oak savannas, grasslands with native grasses and forbs, and wet prairies. Currently, due to widespread development and fire suppression, few native habitats remain. Historically, the Kalapuya Tribe set seasonal fires to improve hunting and foraging. These fires allowed the grasslands, oak savannas and wet prairies to flourish. Without the fires, the native grasses and forbs were diminished, allowing the intrusion of many non-native or invasive species and the degradation of native habitats (Invasive and non-native animals and plants of concern list, Willamette Valley Ecoregion, ODF&W, pp. 239). Imperiled habitats include: oak woodlands, oak savanna, riverine, grassland and wetland (wet prairies) (Oregon Conservation).

Habitats along the Willamette and Tualatin rivers are severely fragmented as well, since the rivers are disconnected from their floodplains. According to the Oregon Department of Fish and Wildlife, these land-use changes, along with private ownership of land (about 96 percent), are causing major challenges to conservation management, as habitat fragmentation remains of particular concern. Today only one-tenth of one percent of the Valley's native grasslands and oak savannas remain (Oregon Conservation).

The Yamhelas Westsider rail corridor runs through a pre-settlement combination of wet prairie, beaver swamp/ riparian stream bottoms and upland prairies (oak savanna, oak woodland and mixed hardwood/conifer forest on north facing hillsides) (Seal). The habitat is influenced by the Tualatin River floodplain, especially along the northern reaches. According to the U.S. Environmental Protection Agency (EPA) and the Commission for Environmental Cooperation (CEC), two Level IV ecoregions are found in the area: the Prairie Terraces and the Valley Foothills. The Terraces of the Willamette River upstream of the Portland/Vancouver Basin characterize the Prairie Terrace ecoregion. Low-gradient, meandering streams and rivers dissect these upland fluvial terraces. Valley foothills are transition zones between the agricultural Willamette Valley and the more heavily forested Cascade and Coastal mountain ranges (Willamette Valley Ecoregions).



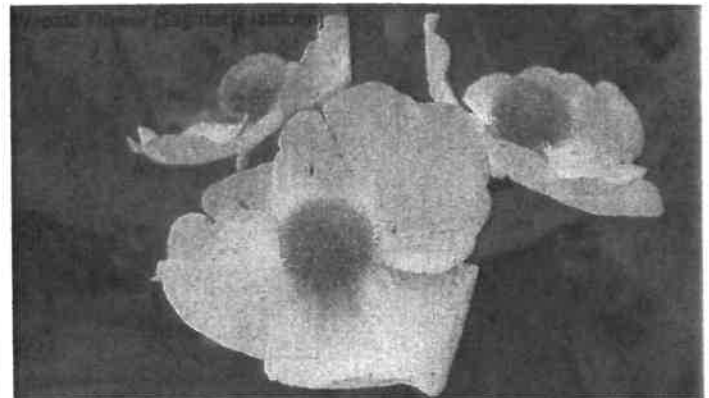
### Wildlife Resources

Notable animal species found in the Willamette Valley are the Townsend's big-eared bat, winter steelhead, Chinook salmon, bald eagle, great blue heron, and peregrine falcon. An abundance of Waterfowl are found in the scattered wetlands. They include some of the following species: Greater White-fronted goose, common merganser, hooded merganser, Common goldeneye, mallard, long-tailed Duck, greater scaup, tufted duck, green-winged teal, northern pintail, northern shoveler, American widgeon, gadwall, wood duck, tundra swan, and trumpeter swan (Oregon Conservation). Oregon's state bird, the western meadowlark (*Sturnella neglecta*), is a critically sensitive species that was once abundant in the Willamette Valley; but rural and urban development and agriculture have degraded the open grassy prairies that this species requires. Notable animals identified as Strategy Species by the Oregon Conservation Strategy (ODF&W) are the western gray squirrel, the northern red-legged frog, northwestern pond turtle, and Fender's blue butterfly (Oregon Conservation).

### Botanical Resources

Common tree species found in riparian forest habitats include: Oregon ash, black cottonwood, western red-cedar, western hemlock, big leaf maple, Douglas fir, and red alder. Upland Prairie habitats contain grass and forb species such as: blue wildrye, California brome, California oatgrass, lupine, and Roemer's fescue. Rare species found in Upland Prairie include Kincaid's lupine and Nelson's checker mallow. The scattered remnants of wet meadow prairie have a great variety of vegetation. Carex, Salix, Scirpus, and Juncus are common genera. Grasses include tufted hairgrass, spikerush, and several sedge species (Oregon Conservation).

Douglas Fir Tree (*Pseudotsuga menziesii*)



Townsend Big Ear Bat (*Corynorhinus townsendii*)



Lupine (*Lupinus*)

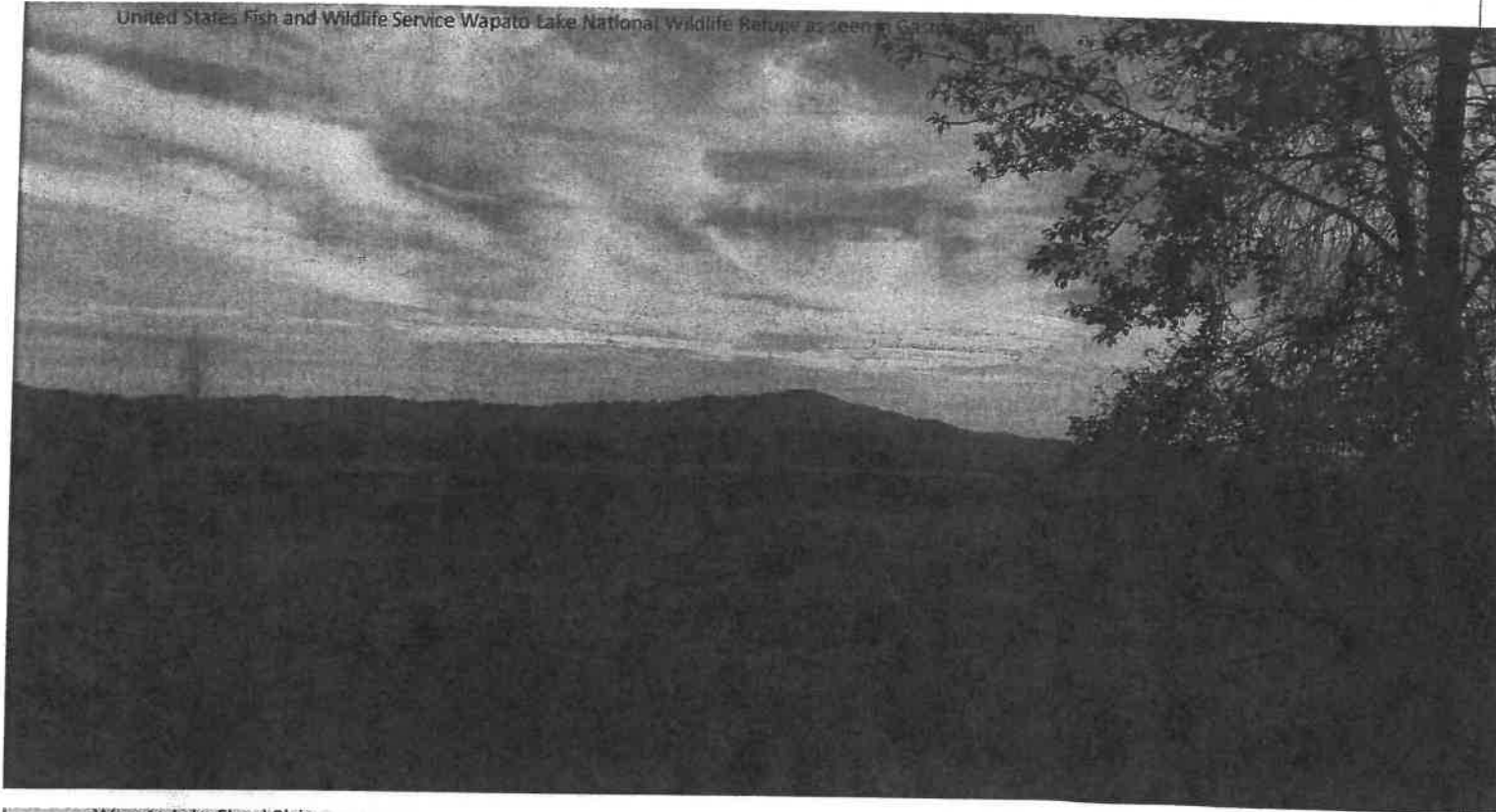




### ***Wapato Lake Floodplain***

The historic Wapato Lake floodplain lies along the northern stretch of the corridor northeast of Cove Orchard, and about a half mile east of Gaston. This area is home to many species of waterfowl and migratory birds who depend on the floodplains for habitat. Historically, the Wapato Lake was nearly 1,500 acres in size and contained shallow water throughout the year. However, due to the construction of dikes, ditches, and drainage tiles the lake was drained to provide for agriculture land as private landowners utilized the Tualatin River for irrigation of these agricultural fields. Furthermore, the Tualatin River no longer seasonally floods in the fall and winter, which in turn continually degrades this historic habitat as the Wapato Basin no longer fills. Recognizing the significance of this flood plain to many different species, the United States Fish and Wildlife Service (USFWS) created the Wapato Lake National Wildlife Refuge in 2013 to preserve this unique region. This newly found wildlife refuge is now federally protected by the USFWS, and their goal is to restore the fragmented habitats by reclaiming the water rights through land acquisition and public/private partnerships (Tualatin). Historically, native plant communities included: scrub-shrub wetland, Oregon ash riparian forest and seasonal herbaceous wetlands. Other historic habitats were upland and wet meadow prairies, as well as mixed hardwood and conifer forests (Tualatin).

United States Fish and Wildlife Service Wapato Lake National Wildlife Refuge as seen in Gaston, Oregon



Wapato Lake Flood Plain

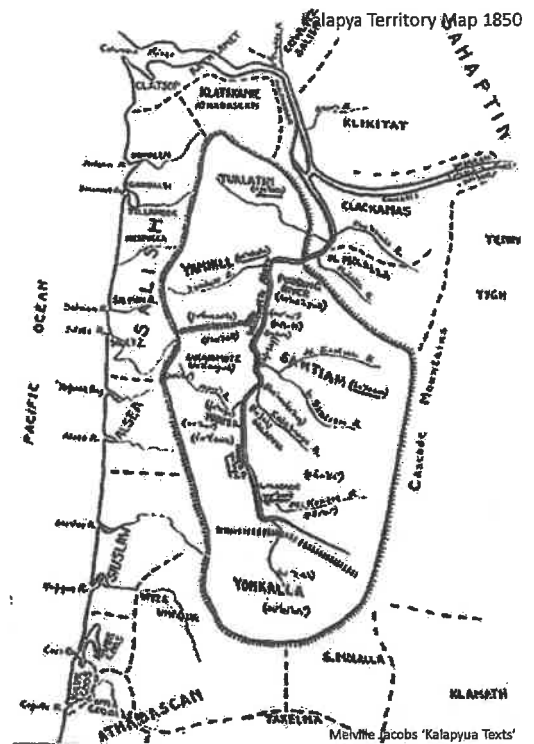


# CULTURAL RESOURCES

## NATIVE AMERICAN

### (PRE EUROPEAN SETTLEMENT)

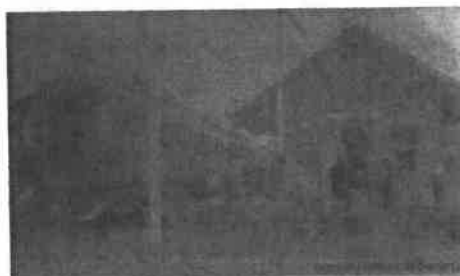
The earliest known indigenous people living in current-day Yamhill County were known as the Yamhelas. They were a band of Northern Kalapuya Tribe, and were closely associated with the neighboring Tualati tribe (sometimes referred to as the Wapato Lake Band) inhabiting current-day Washington County. Both the Yamhelas and the Tualati tribes were closely interconnected both socially and culturally, and shared a definite sense of collective identity for over 8,000 years. Harvesting camas flowers, hunting large game, and catching salmon, they shared many of the same cultural resources. As Europeans began to settle the area and introduce disease to the tribes, drastic depopulation and shrinking territory began to occur as early as the 1780's. By 1851 the tribes began to negotiate with United States Federal Government for transfer of their land, and in 1857 they were forced to migrate to the Grand Ronde Indian Reservation in Oregon's Coastal Range. The area continued to be settled by Europeans as the mass-migration known as the Oregon Trail brought settlers westward. As well, the ever expanding railroad lines throughout the Pacific Northwest brought additional settlers to these newly established territories.



### EUROPEAN SETTLEMENT

European settlement of the Willamette Valley began in conjunction with the removal of Native American Indian tribes from the area in the mid 1800's. Prior to the construction of the first Transcontinental Railroad in 1869, European settlement in the Willamette Valley became widespread following the establishment of the Oregon Trail from 1811 to 1840. This was the main route to Oregon for some 400,000 settlers, particularly through the epoch years from 1846-1869 (Oregon Trail). Fur trapping and trading was common along the western part of the route, as the Hudson's Bay Company (HBC) was the prominent fur company that controlled most of the trading operations in the Pacific Northwest, as well as the western half of the Oregon Trail. This monopoly on trade virtually inhibited settlement within the region. However, the first successful large wagon train to reach Oregon in 1843, led by missionary Marcus Whitman, broke the HBC's stranglehold on settlement within the Willamette Valley. Soon after, thousands of American settlers flooded the region via the Oregon Trail as Yamhill County became known as an agriculture center (Marschner). During "The Great Migration of 1843" an established 700-1,000 settlers arrived within the area (Willamette Valley) with the promise of 640 acres of deeded land to married couples, and 320 acres of deeded land to unmarried individuals who made the trek.

Yamhill County, then known as Yamhill District, was created on July 5, 1843. It was originally over 12,000 square miles until it was reduced in size in 1847. Today it is currently 709 square miles. The city of Lafayette (once referred to as Yamhill Falls) was the first county seat and center of commerce as it was located on the Yamhill River and on the main Indian trail that traversed the Willamette Valley (Oregon Historical). Following the discovery of gold in California in 1848, farming took off as the agricultural products of the fertile Willamette Valley were desired commercially in the California mining towns. By 1845, the 2,000 settlers in the Willamette Valley outnumbered the natives, whose numbers were decimated by disease introduced by Europeans. A survey in 1850 showed that almost 90 percent of all settlement in the Valley was grasslands previously occupied by the Kalapuya tribe (Sinclair). By the 1860s almost all of the tillable farm land in Yamhill County was claimed as homesteads (Willamette Valley).



## THE RAILROAD

As the Willamette Valley and Oregon began to grow, railroads became an integral part of moving people and goods throughout the Pacific Northwest and down to California. Competing companies and rail lines fought to secure control of such movements throughout much of the Willamette Valley. Two such competing companies were the Oregon Central Rail, owned by Joseph P. Gaston, and the Oregon to California railroad owned by Ben Hollady. Gaston started building his railroad along the west side of the Willamette Valley from Beaverton to McMinnville in 1866; and Hollady began soon after, establishing a rail line on the east side of the valley in 1868. This competition between rail lines became known as the Westsider and Eastsider Railroads, due to the geographic location of each rail line in association to the Willamette River. Joseph Gaston eventually sold the Oregon Central Railroad to Ben Hollady in 1870.

Historic Train Depot



The Westsider rail was originally planned by Joseph P. Gaston to move between modern-day Beaverton to St. Joseph. However, it was actually Ben Hollady that completed the line, and expanded it to move from Portland to St. Joseph. The Westside railway was opened from Portland to Hillsboro on December 23, 1871; to Cornelius on January 28, 1872; to Gaston on September 29, 1872 and finally to St. Joseph on November 2, 1872. Stops along the way included Hillsboro, St. Joseph, Patton, Gaston and Carlton. In May of 1887 Ben Hollady sold his Oregon to California Railroad line to Southern Pacific (SP) Railroad Company, which acquired control of the "Westsider" line as part of the acquisition.

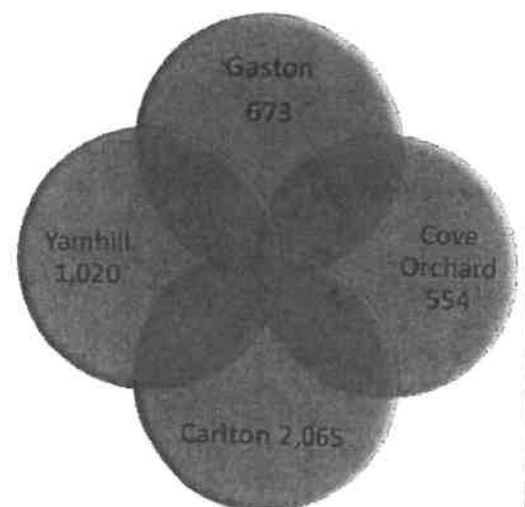
Heavily used rail lines continued to transport goods, services and people throughout most of the early twentieth century. However, as technological advances were made in the fields of electricity more and more pedestrian transportation methods were beginning to shift from traditional rail line transportation to the newly popular electric-trolleys. Anticipating the change, Southern Pacific Rail Company, a rival to Portland-based railroads, began to electrify its lines in Lake Oswego, McMinnville and eventually Corvallis. Before long, a new competitor for commuter traffic appeared in the form of motor busses and the Southern Pacific Rail Company saw a decline in its trolley business. By 1930 all trolley services in the area were discontinued by Southern Pacific Rail Company and the remaining trolley cars were sent to Los Angeles.

The "Westsider", having been owned by Southern Pacific since 1887, continued to support the logging, agriculture, and commercial businesses in the area until 1980, when there was a dramatic decrease in the number of "on-line" industries using the "Westsider". At this time, Southern Pacific began to pull up tracks and in 1998 sold the remaining "Westsider" line to the Union Pacific Railroad Company, which at the time this was written, maintains ownership.

## CURRENT COMMUNITY PROFILE

The current day "Westsider" rail corridor, now known as the Yamelas Westsider Trail (named after the indigenous Yamhelas people, and the development of the rail corridor by Hollady) passes through three cities, one unincorporated area and eventually hopes to connect to the largest city in Yamhill County. The city of Gaston is located at the northern most point of the trail, with a population of 673 (2014). The city's northern section is part of Washington County and the southern boundary falls within Yamhill County. To the south of Gaston is Cove Orchard, an unincorporated area that is partially managed and maintained by Yamhill County. There are approximately 554 people who reside there. Located just over eight miles south of Gaston is the City of Yamhill, a small farming community with a population of 1,020 citizens. The largest local high school, Yamhill-Carlton High School, as well as the Middle School and district offices are all located in Yamhill. Three miles south of Yamhill is the city of Carlton, which prides itself on a strong community and vibrant downtown. With a population of 2,065 Carlton is the center of rural wine tourism and production with over one million visitors a year to its local wineries. Approximately seven miles to the south of Carlton is the City of McMinnville, population 33,131 (2013), the largest city in Yamhill County.

POPULATION OF COMMUNITIES LIVING ALONG THE TRAIL



# REGIONAL TRAILS

## EXISTING AND PLANNED REGIONAL TRAILS

The following are a list of key regional trails (existing or planned) that the Yamhelas Westsider Trail is close to and has the potential to link to:

### ***Forest Grove Trails***

Forest Grove has a number of trails including Highway 47 Trail and B Street Trail. Both of these paved trails intersect on the south end of town on Highway 47 and B Street. B Street Trail is  $\frac{3}{4}$  of a mile long and Highway 47 Trail is 2.31 miles. The distance from Gaston to the southern terminus of these trails is 5.4 miles and only 3.7 miles from Scoggins Creek.

### ***Hagg Lake Trail Loop and Park***

A 13 mile loop around Hagg Lake, which is primarily natural surface with a few sections on road. Haag Lake Park is a popular regional destination and there are plans to connect it to Highway 47 and eventually the northern terminus of the Yamhelas Westsider Trail.

### ***Chehalem Ridge Park & Trails***

At over 1,200 acres, Chehalem Ridge Park is one of Metro's and the region's largest parks. Metro recently began to plan and develop the park including a proposed natural surface trail system. Gaston Road out of Gaston is the access for Chehalem Ridge Park and is approximately 4.8 miles from town.

### ***Banks to Vernonia Trail***

A former railroad corridor turned trail, this 21-mile-long, eight-foot-wide paved trail owned and managed by Oregon Parks and Recreation connects the towns of Banks and Vernonia. For much of the trail, there is a parallel four-foot-wide equestrian path. This trail was the first linear park developed by Oregon's Parks and Recreation Department. This southern-most trailhead is 14.8 miles north of Gaston, in the city of Banks, Oregon.

### ***Crown Zellerbach Trail***

This trail is a 22.4 mile rail trail stretching from Chapman's Landing in Scappoose, Oregon to just north of Vernonia. The trail is actively being updated. Most of the trail is hard-pack and gravel with some sections of asphalt. Although this trail does not connect to the Banks to Vernonia Trail, there are plans to do so.

### ***Salmonberry Trail***

Primarily on old railroad corridors, this proposed paved trail connects the city of Banks to the Vernonia Trail at LL "Stubs" Park. It is planned for the trail to eventually connect the towns of Nehalem and Wheeler on the Oregon Coast, down to Tillamook.

### ***Council Creek Trail***

A 15 mile planned trail that will connect Forest Grove to Hillsboro in the east, and Banks in the north via the Highway 47 and B Street trails.

### ***Metro's Regional Trail System***

1,200 miles of connected trails and greenways in the Portland region was the vision in 1992's for Metropolitan Greenspaces Master Plan. Metro has been actively working to build out an updated version of the 1992 vision. The Yamhelas Westsider Trail and the trails listed above are part of this 1,200 mile vision of a regional trail system.

### ***Wapato Lake National Wildlife Refuge Dike Trails***

The Wapato Lake National Wildlife Refuge (NWR) is a unit of the Tualatin River National Wildlife Refuge Complex. Recently formed in 2008 (date of the first purchase for Wapato NWR), this NWR's western boundary abuts Highway 47, the Yamhelas Westsider rail corridor and Gaston. Currently Wapato NWR is 195 acres in size with the opportunity to increase to 4,310 acres. Due to the relative newness of Wapato NWR, there are no visitor services or trails at the Refuge. But, the United States Fish and Wildlife Service (USFWS) has plans to study these lands for the possible development of a trail system utilizing the old dikes and/or possible other locations. It is envisioned that these USFWS trails on Wapato NWR could use the Gaston trailhead and parking planned for the Yamhelas Westsider Trail.



# Regional trails and greenways



- Existing Pedestrian Trail
- Planned Pedestrian Trail
- Existing Multi-Use Trail
- Planned Multi-Use Trail
- On-street Connection
- Proposed Trail
- Water Trail
- Greenway
- Parks and Natural Areas



1. Banks-Vernonia Trail
2. Beaver Creek Trail
3. Beaver Lake Trail
4. Beaverton Creek Trail
5. Beaverton to Milwaukie Trail
6. Bridgeport to Milwaukie Trail
7. Brimley Creek Trail
8. Butler Butte Trail
9. Canby River Greenway
10. Clackamas River Greenway
11. Columbia Slough Trail
12. Cooper Mountain Trail
13. Coon Creek Trail
14. Crescent Park Trail
15. Crown Zellerbach Trail
16. East Butte Powerline Trail
17. Fanno Creek Trail
18. Gales Creek Trail
19. Gresham-Fairview Trail
20. High Lake Trail
21. Halvick Trail
22. Highway 47 Trail
23. Hillsboro to Banks Trail
24. Hillsdale to Lake Oswego Trail
25. Historic Barlow Road Trail
26. Historic Columbia River Highway
27. I-205 Trail
28. I-405 Trail
29. Ice Age Torquin Trail
30. Keller Creek Trail
31. Marine Drive Trail
32. Mangrove Trail
33. McKay Creek Greenway
34. Mount Scott Trail
35. North Clackamas Greenway
36. Oregon City Loop Trail
37. Oregon Electric Railway Trail
38. Pacific Greenway Trail
39. Peninsula Crossing Trail
40. Red Electric Trail
41. Reservoir Trail
42. Richardson Creek Trail
43. River Terrace Trail
44. Rock Creek Trail
45. Rosemont Trail
46. Salmonberry Corridor Trail
47. Sandy River Greenway
48. Scullens Mountain Trail
49. Springwater Trail
50. Stafford to Canby Trail
51. Stafford Trail
52. Sullivan's Gulch Trail
53. Sunrise Corridor Trail
54. Sunshine Valley Trail
55. Tualatin Valley Trail
56. Tualatin River Greenway
57. Tualatin Valley Trail
58. Tualatin River Greenway
59. Tualatin Valley Trail
60. Tualatin Valley Trail
61. Waterhouse Trail
62. Washburn Trail
63. Willamette River Greenway
64. Willamette River Greenway
65. Yamhill Westside Trail

\* Map shown on map, located in Clackamas County, Oregon.

# BENEFITS

Providing many unbridled opportunities, trails have been an integral part of the American landscape for hundreds of years. These uninterrupted linear corridors have evolved from Native American hunting trails, to trade routes, to established transportation corridors. Today, trails directly benefit local communities in several ways, whether it is by providing recreation and transportation opportunities, or by spurring economic development. Trails have become a culturally important network serving many as a primary link to the outdoors. As numerous communities begin to realize the benefit of establishing trails, it is important to preserve these corridors for future generations.

## RECREATION

One of the most direct benefits of community trails and greenways is the close to home, accessible recreation opportunities they provide. Trails make communities better places to live by preserving and creating open spaces for non-motorized transportation. By providing a unique facility to serve diverse populations, that may otherwise have limited opportunities to access natural areas due to financial or transportation constraints, trails provide affordable exercise and recreational opportunities within local communities.

## TRANSPORTATION

In addition to providing opportunities for recreation, trails and greenways also function as non-motorized transportation corridors for pedestrians and cyclists. Trails and greenways link neighborhoods with shopping and entertainment districts, and provide a pleasant transportation alternative for commuting to work, school, and social activities. Often direct and uninterrupted, trails provide a pedestrian scale environment that allows for safe passage to and from destinations. They also can provide safe alternative transportation methods for children commuting to and from school. A unique opportunity that the proposed Yamhelas Westsider trail provides is the ability to mitigate conflicts between farm equipment and pedestrians on current road-ways. Being a heavily agricultural area, there is potential for farmers moving equipment along country and town roads to come into contact with ever-increasing pedestrian and recreation seekers, such as bicyclists. Removing pedestrians and recreation seekers off the current roads and onto the proposed trail, lessen the potential conflict between these two uses.

## HEALTHY LIVING

Trails and greenways provide a safe, inexpensive avenue for regular exercise for people living in rural, urban and suburban areas. For many, exercise is important for maintaining good health in all stages of life; however, most people do not regularly exercise. Scientific evidence supports that people are more likely to be physically active if they have recreational facilities close to their homes. By providing opportunities for recreation through trails, people of all ages have an accessible and low-or no-cost, attractive and safe place to cycle, walk, skate and jog. By connecting people with places they want or need to go, trails encourage people to incorporate exercise within their daily routines, thus creating a significant impact on public health and wellness.

## TOURISM

Rails to trails projects are a significant draw for many visitors. Local entrepreneurs can take advantage of the influx of people seeking to walk or bike the trail. The opportunity to rent bikes, provide shuttles or even guide trips will be generated with the creation of this trail. Local restaurants and stores are anticipated to benefit from the enhanced tourism visitation due to the creation of the trail. The nearby Banks-Vernonia Rail Trail receives over 250,000 annual visitors. The area's well known and highly productive viticulture paired with the Yamhelas Westsider Trail could be a unique experience and draw for visitors. The trail will link together several wine tasting rooms and vineyards offering the potential for a Wine and Bike Tour.

## ENVIRONMENT

Greenways and trails play an important role in mitigating environmental impacts by preserving vegetated open spaces that act as a natural buffer zones between the built and natural environments. Vegetated areas have been shown to improve overall water quality by preventing pollution run-off into direct water sources. Air quality is also impacted by greenways because they provide a non-motorized transportation route reducing overall carbon emission from traditional vehicular transportation methods. Vegetative areas also help mitigate air pollution by filtering out pollutants from the surrounding air and by naturally increasing oxygen levels. By providing direct access to the outdoors, the value of public open space is enhanced as trails can also function as a hands-on environmental classrooms for people of all ages, including but not limited to nature studies, history interpretation and student field trips.

## SAFE and LIVABLE COMMUNITIES

Trails promote safe and livable communities. The recreation, health, transportation and environmental benefits collectively can contribute to an overall enhanced quality of life in communities. Another variable to consider is the impact of trails and greenways on safety. While many believe that trails promote crime, they can actually reduce illegal activity through regular use and high visibility of users. Trails often provide informal opportunities to meet and interact with neighbors, creating a sense of place and community pride. Communities in which children can safely walk or bike to a park, school, or to a neighbor's home are generally thought to be excellent places to live. **For more information and examples of other trails please visit:** < [https://safety.fhwa.dot.gov/ped\\_bike/docs/rt\\_safecomm.pdf](https://safety.fhwa.dot.gov/ped_bike/docs/rt_safecomm.pdf) >

## HISTORIC PRESERVATION and COMMUNITY IDENTITY

The Yamhelas Westsider Trail can enhance the region's sense of community identity and pride through the interpretation of the area's rich historic and cultural resources including the preservation of this historically significant transportation corridor. Trails and greenways provide a window into our history and culture by connecting people to the past. They often link and provide access to historic features, such as train depots, Alec Carson's resting place, and historic grain silos. The preservation and highlighting of these places lets us learn about and understand the history of the region and the area's communities.

## PROTECTION OF AGRICULTURE EQUIPMENT ON ROADS

The Yamhelas Westsider Trail will reduce foot and bike traffic on Highway 47 and other arterial roads in the area. The winding nature of Highway 47 and nearby roads presents less than optimal conditions for non-motorized traffic within the road corridors. This coupled with the presence of farming equipment legally using the road corridor to transport machinery and access crops could lead to potential traffic accidents that involve farm equipment and bikes/hikers. The creation of the Yamhelas Westsider Trail will greatly reduce this potential by getting bikers and hikers off the road and away from farming equipment.



*Agriculture Equipment on the Road*

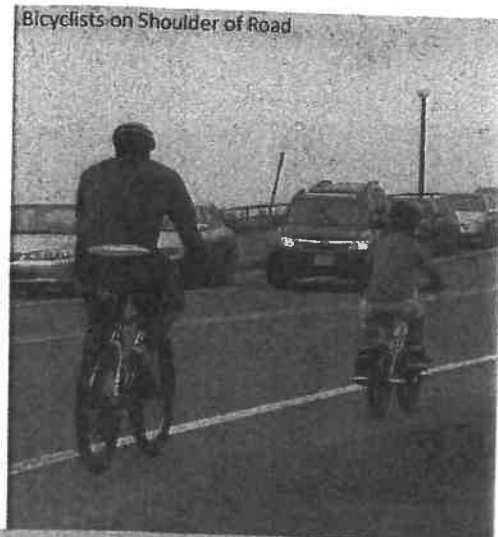
As this picture demonstrates, agriculture equipment can take up most, if not all of the roadway. This can cause conflict with pedestrians trying to use the road shoulder for recreation, transportation or other uses.

### *Trails for Health*

Multimodal trails are beneficial for all ages, skill levels, and transportation methods. Trails promote healthy active lifestyles, social interactions, nature conservation and education, among other things.

### *Bicyclists on Shoulder of Road*

This picture shows that it is dangerous to bike along the shoulder of busy roadways, as there is no buffer and often times the shoulders can be narrow.



# DESIGN RECOMMENDATIONS

As stated earlier in this document, a concept plan is an early look at how the trail will look and feel within the rail corridor. In general, the plan defines where the trail will be located within the corridor. This plan provides the framework for the construction of the trail. The design principles and guidelines detailed in this chapter of the concept plan will support consistent design over the course of the development of this trail, which will be done in phases as monies are available. This chapter also describes the proposed trail amenities that will serve to enhance the user's experience.

## TRAILHEADS

The size, character, and amenities of trailheads will vary from site to site depending on demand, size/geography of site, and funding available for development. Some potential locations for trailheads will provide opportunities for viewpoints, picnicking, or park features in addition to parking and trail access. Trailheads will include parking for ten or more cars. They may include seating to allow trail users to prepare for their trip, bicycle racks for short-term storage, and wayfinding information. In some locations trailheads may also include water fountains and restrooms, picnic tables, art installations, covered bicycle parking, and other user amenities. Trailheads should be managed like small parks and be open from dawn to dusk. Typically, trailheads have easy visibility from nearby roads for law enforcement purposes. These locations are regularly visited for trash removal and routine maintenance.

## SIGNAGE

Signage along the Yamhelas Westsider Trail will serve to welcome trail users, communicate trail regulations and rules, identify locations in the case of an emergency, and aide wayfinding. Kiosks can also be placed at strategic locations giving trail information and also provide historical and cultural interpretation along the trail. Signage is a basic feature at trailheads, access points, and in advance of intersections. The need for signage must be weighed against over-signing the corridor to the determinant of the character and aesthetic of the trail experience.

## PUBLIC ART

Public Art is an excellent opportunity to provide a unique experience along the Yamhelas Westsider Trail and to create a sense of place. Well done artwork can inspire trail use and offer pleasant, provocative, and enriching experiences. It can contribute to the beauty, cultural vitality, and economic development of Yamhill County. Public art can also create a lasting cultural legacy. It provides added value and a community aesthetic that encourages positive community development, potentially increasing public safety. The Yamhelas Westsider Trail's unique route, history, and the character of the landscape and communities it runs along will favor the creation of distinctive cultural and artistic instillations.

Public Art along the trail corridor could:

- Create movement corridors leading to destinations and as destinations themselves;
- Provide opportunities to express the unique personality of each trail section while striving for accessibility and optimal user experience;
- Cultivate public places that are aesthetic, green, interesting, social, cultural, and shared;
- Build a legacy of artwork and aesthetic enrichment; and
- Enhance public awareness of trails and their surrounding environment.

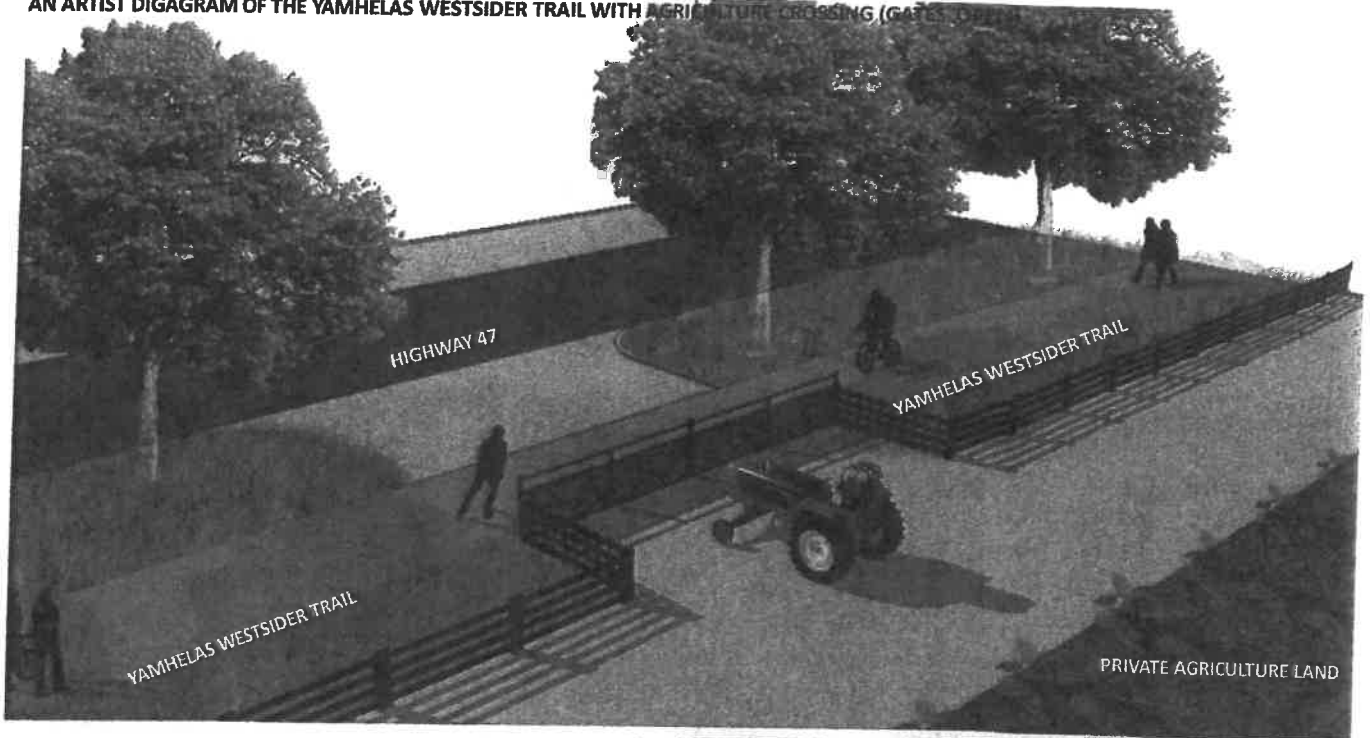




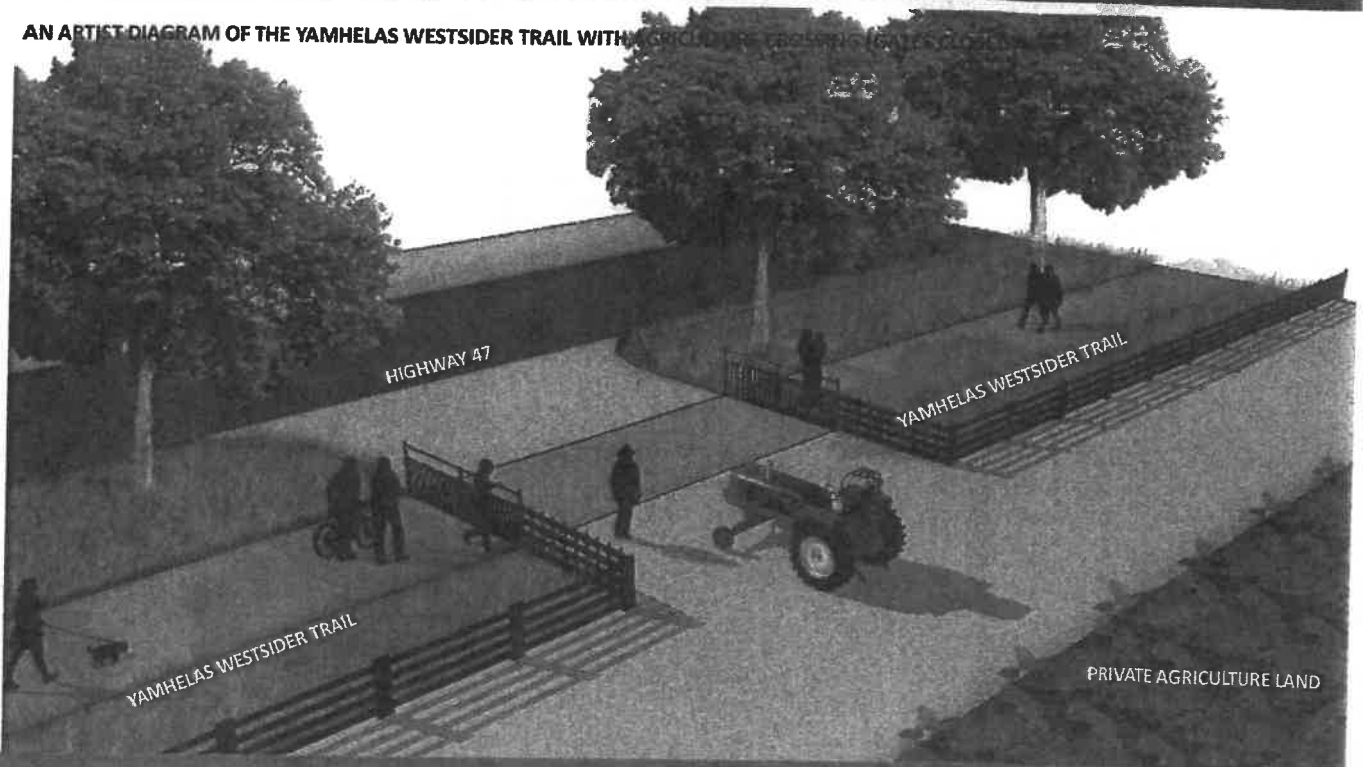
## AGRICULTURE CROSSINGS

For much of the rail corridor, the trail traverses through a rich agrarian landscape. Cattle ranchers, vineyards, crop farms, and orchards are located alongside or close to the trail. Some of the agricultural operators have lands that the railroad bisects. These farmers need to access their operations. Yamhill County intends to work with all farmers (on a one-on-one basis) to maintain access to their lands. In some locations, farm machinery and livestock need to be transported across the trail. At these locations, a set of gates could be installed so that when all gates are closed, the entire trail is blocked in both directions and a 20 foot gated corridor (this can be made wider if needed) through the trail is created for animals and machinery to safely travel and access on the other side of the trail.

AN ARTIST DIGAGRAM OF THE YAMHELAS WESTSIDER TRAIL WITH AGRICULTURE CROSSING (GATES CLOSED)



AN ARTIST DIAGRAM OF THE YAMHELAS WESTSIDER TRAIL WITH AGRICULTURE CROSSING (GATES OPEN)

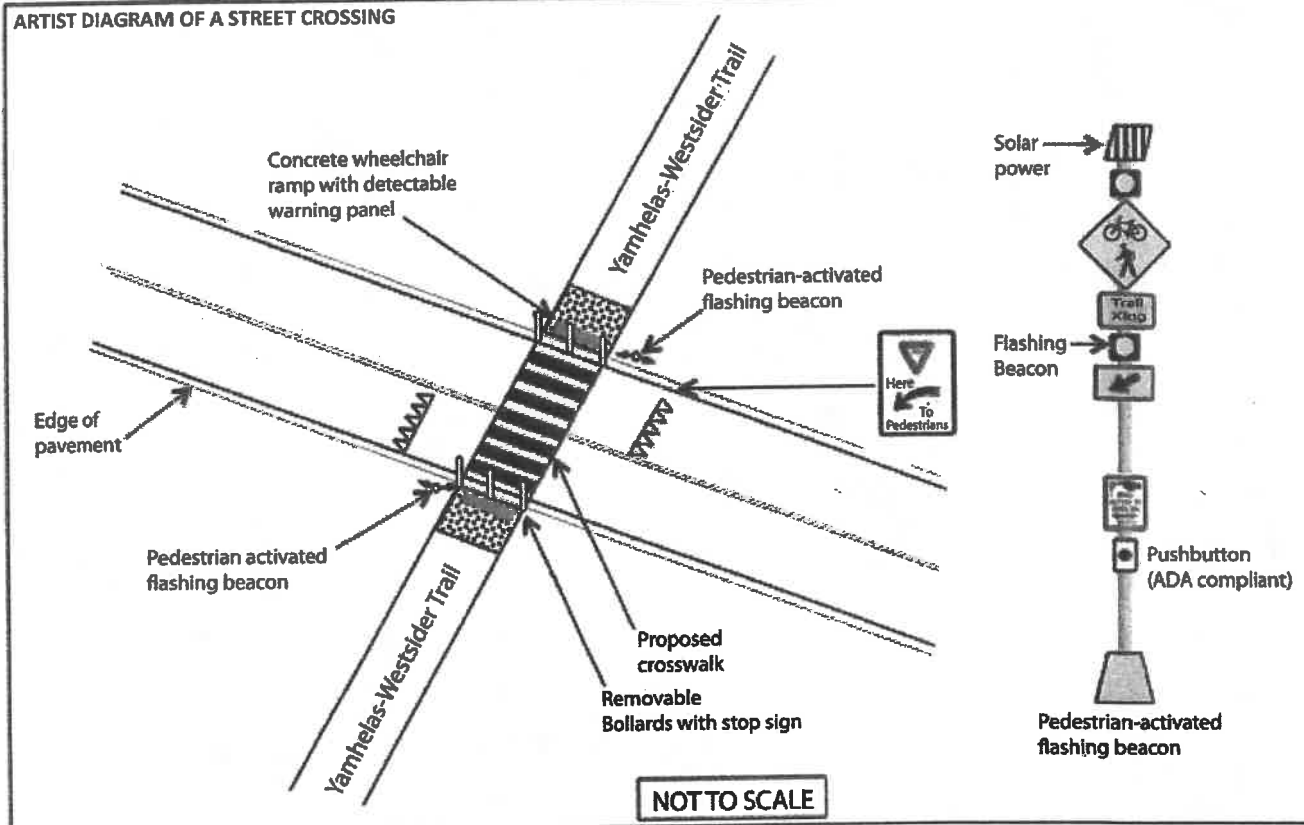


## ROAD CROSSINGS

Road crossings are one of the most critical design elements for a shared use path. The goal of trail design is to make these crossings safe for trail users and motorists. At most intersections, except those with major arterials, crossings will be designed so that the road has a striped walkway crossing the road surface. Signs installed on the road will give motorists advance warning that there is a bike/pedestrian crossing. Trail users will be asked to stop prior to entering the intersection. Prior to the trail intersection, removable bollards will be installed that will help to slow bicycle traffic and prevent motorized traffic from entering the trail corridor.

At high volume/more dangerous crossings, trail users will be asked to push a button to activate a flashing light warning motorists that a pedestrian or bicyclist is crossing the road. It should be noted that some crossings can be complex; therefore, final decisions on the locations and design elements for crossings will be made in the engineering phase.

ARTIST DIAGRAM OF A STREET CROSSING



ARTIST RENDER OF MAIN STREET CROSSING IN CARTON



## LIGHTING

Lighting provides both a safe setting for trail users and allows for the illumination of hazards and obstacles. If done properly, it can also provide a nice aesthetic to the trail corridor. Lights should be considered in the following circumstances:

- Trailheads
- Approaches to bridges and boardwalks
- Intersections with roads (where streetlights do not already exist)
- Changes in tread height or tread surface where a bike tire or foot might slip
- In the Commercial District Corridor (see page 26)

Illumination should be designed to support pedestrian and bicycle safety and security while minimizing glare and obtrusiveness to surrounding neighborhoods. A variety of lighting types may be used on the trail, depending on the function and the location. Taller light fixtures could be used where uniform light is needed for the entire trail surface, or where lighting would enhance safety. Wherever possible, low elevation lighting that is at or near the surface should be considered as long as appropriate. It reduces ambient light to surrounding neighbors.

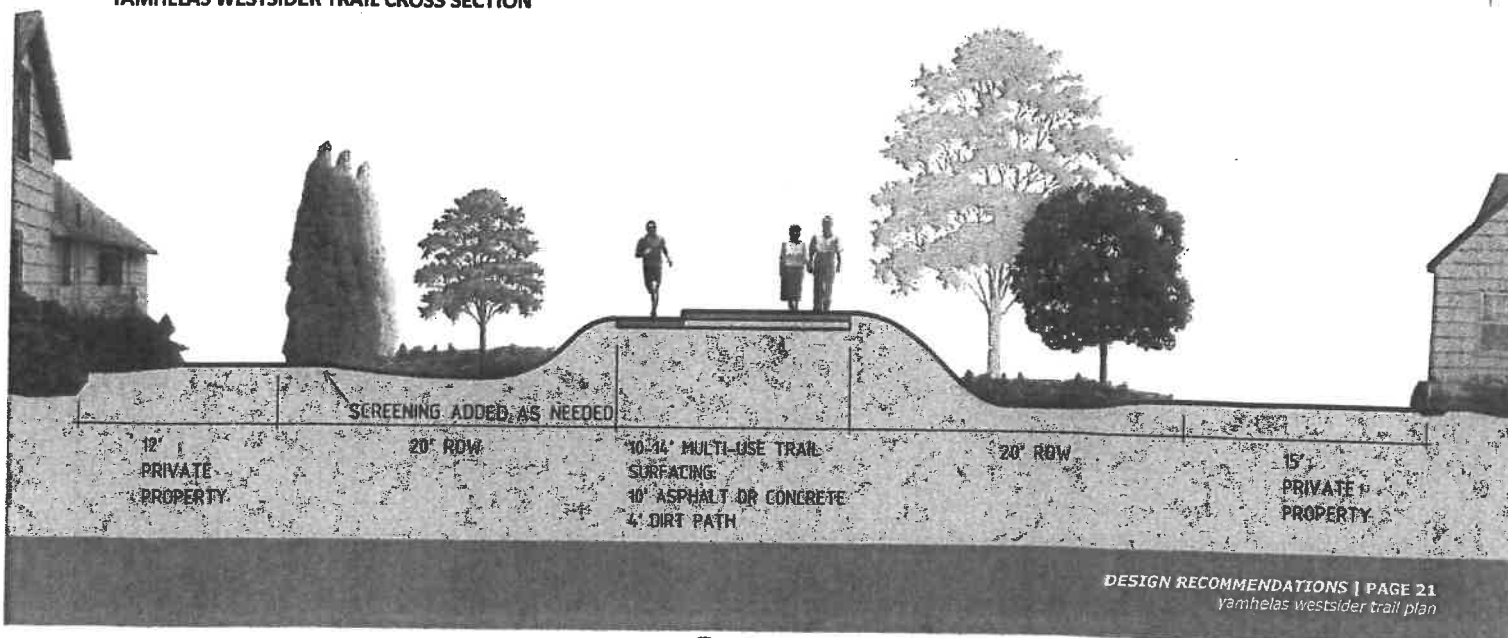
## VEGETATIVE PLANTINGS and SCREENINGS

The Yamhelas Westsider Trail is comprised of open areas with rail bed gravel, unmaintained grassy cover, bushes and some trees that have grown up and into the corridor. Plants, shrubs and trees are a mix of natives and invasive species such as Himalayan blackberry and Scotch Broom. Construction of the trail will only take up a part of the rail corridor. The landscape within the corridor that does not need to be removed or impacted from trail construction should be evaluated to determine if it is consistent with public use, including aesthetics and overall trail design. To create a sustainable, maintainable landscape along the length of the trail, replanting will typically be simple, with a focus on meadow grasses and native trees and shrubs.



To prevent unsafe conditions for the trail, shrubs should typically be planted 5 feet or more from the trail edge, and trees about 15 feet or more from the trail. The area directly adjacent to the trail is expected to be maintained by mowing or trimming. The areas farther from the trail should be preserved in a natural state and should not be maintained once plants are established. Planting strategies for specific areas should be developed based on the character of the surroundings. In some locations taller shrubs may be used as a visual screen between the trail and nearby homes. In other locations, the landscape may be selectively opened up to improve visibility and for security surveillance, which is an important consideration for discouraging vandalism and other unwanted activities.

## YAMHELAS WESTSIDER TRAIL CROSS SECTION



# ***YAMHELAS WESTSIDER TRAIL RECOMMENDATIONS***

***Note: these are conceptual ideas and are not meant to be interpreted as final design plans or a master plan.***

These site specific recommendations are organized from south to north along the rail corridor.

## **McMINNVILLE TO GUN CLUB ROAD *Sheet 1***

At the time of plan production, the rail corridor from Gun Club Road south towards McMinnville continues to be utilized by the railroad and is retained in their ownership. As a result, a different route will need to be developed if the trail is to connect to McMinnville. Alternatively, Yamhill County and partners can wait to see if this section of trail becomes unused and open for acquisition.

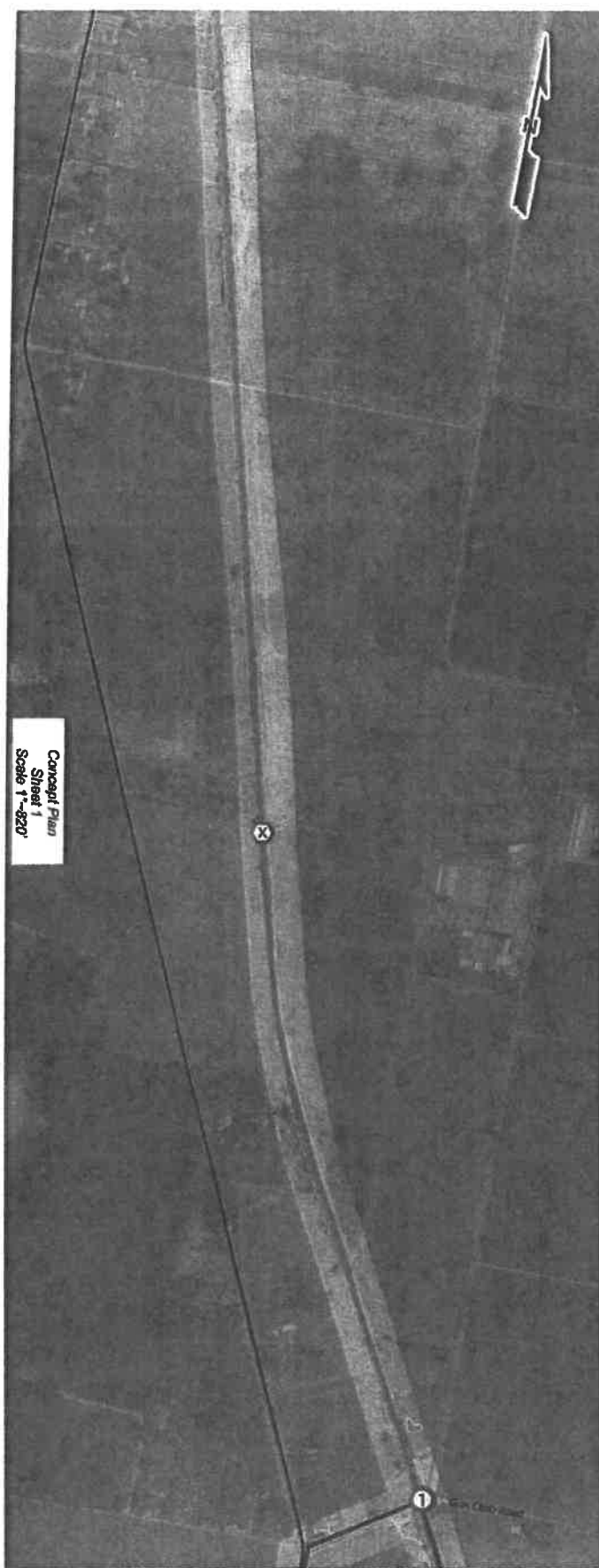
Other alternatives to connect to McMinnville identified in the planning process include routing the trail within the Highway 47 (Tualatin Valley Highway) right-of-way corridor or connecting less travelled roads such as Gun Club Road and Poverty Bend Road with easements through private property (acquired by willing landowners). All of the McMinnville connection alternatives will need to be studied and evaluated for feasibility.

It should be noted that there was early discussion of a trailhead at Gun Club Road. This trailhead is no longer being proposed. It would only serve as a temporary trailhead until a connection could be made to McMinnville and is only about 2.25 miles south of the town of Carlton, where a larger trailhead is recommended.

At the southern end of the proposed trail (immediately North of Gun Club Road) is a residence which lies close to the rail corridor. It is recommended to work with the landowner to install a vegetative screen to provide a visual barrier between the residents and the trail users.

Approximately .8 miles north of Gun Club Road, an agriculture crossing is recommended to allow for easy and safe passage of agricultural equipment. (*Sheet 1- Design Recommendation 1*)








In areas where agriculture traffic must cross the trail, crossing gates could be installed (*see artist example of agriculture crossing on page 19*).



Concept Plan  
Sheet 1  
Scale 1"=820'

## Yamhelas-Westsider Trail Concept Plan

### Sheet 1

-  Proposed trail
-  Property lines
-  Highway 47
-  Proposed trail extension alternative to McMinville
-  Design Recommendations
-  Agriculture Crossing
-  Vegetative screen

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

### Notes

- ① Gun Club Road
- End of railroad abandonment
- Potential future connection to McMinville

## **GUN CLUB ROAD TO CARLTON** *Sheet 2*

Just as the trail enters Carlton (and past the AVA stone monument), a “gateway” to Carlton sign and informational map is recommended (*Sheet 2 – Design Recommendation 1*). It will greet users of the trail and orient them to town. Information regarding eateries, services and other business information should be included.

While the main trailhead and parking area is recommended at the intersections of Pine and Monroe Streets, overflow parking for heavy usage times (such as events) is recommended on the recently acquired school district property near Carlton Elementary School. (*Sheet 2 – Design Recommendation 2*)

Polk Street is anticipated to have heavy use due to the proximity of the school, and upgraded street crossing is recommended for this intersection to provide extra pedestrian safety within the area. This will directly benefit kids on their way to school, parents driving cars to and from picking up their kids, and other users of the trail. In addition to the usual striping, signage and removable bollards, this intersection should have an on-demand flashing light for increased safety and awareness. (*Sheet 2 – Design Recommendation 3*) (*See artist rendering of Polk Street Crossing on page 28*)

## **DOWNTOWN CARLTON** *Sheet 2 and Carlton Detail*

The trail from Polk Street up to Roosevelt Street (Carlton Fire Hall) is recommended to be the Commercial District Corridor and as such will change in the feel and look of the corridor. The Commercial District Corridor (CDC) will have lighting (low elevation – downward as opposed to street lamps), increased benches, may be wider in areas, have different surfacing (concrete, coloring, etc.), and have more landscaping and other aesthetic touches. This is the only section of trail that goes through a town until Gaston. The CDC is represented on Sheet 2 and the Carlton Detail as cross-hatched portion of the trail.

The Carlton Veterinary Hospital has expressed the desire to develop a horse corral (*Sheet 2 – Design Recommendation 4*) at their facility for equestrians to hitch their horse while they go in to downtown to eat and shop.

The historic Madsen Grain Elevator provides an interesting aesthetic and architectural building for trail users to see. An interpretive sign should be installed here to provide more information to trail users. (*Sheet 2 – Design Recommendation 5*) (*See artist rendering of the Madsen Grain Elevator and Silos on page 29*)

East Main Street is another crossing recommended to be upgraded to include on-demand flashing lights for trail users. (*Sheet 2 – Design Recommendation 6*)








Historically, there was a railroad spur line that went out west towards Carlton located north of Monroe Street. This location would be a good spot for a historic interpretive panel on the railroad and the spur line. In addition, the spur line could be delineated in the surface of the trail through a different color or texture. (*Sheet 2-Design Recommendation 7*)

The Carlton Trailhead (and southern-most trailhead recommended until a trailhead can be sited in or near McMinnville) will be located at the corner of Pine and Monroe Street in the existing paved lot immediately to the north of the old train depot station. Restrooms, a water fountain, plus trail and Carlton commercial signage are recommended. Additional overflow parking can be accommodated across the street. (*See Carlton Detail*)



# Yamhelas-Westsider Trail Concept Plan

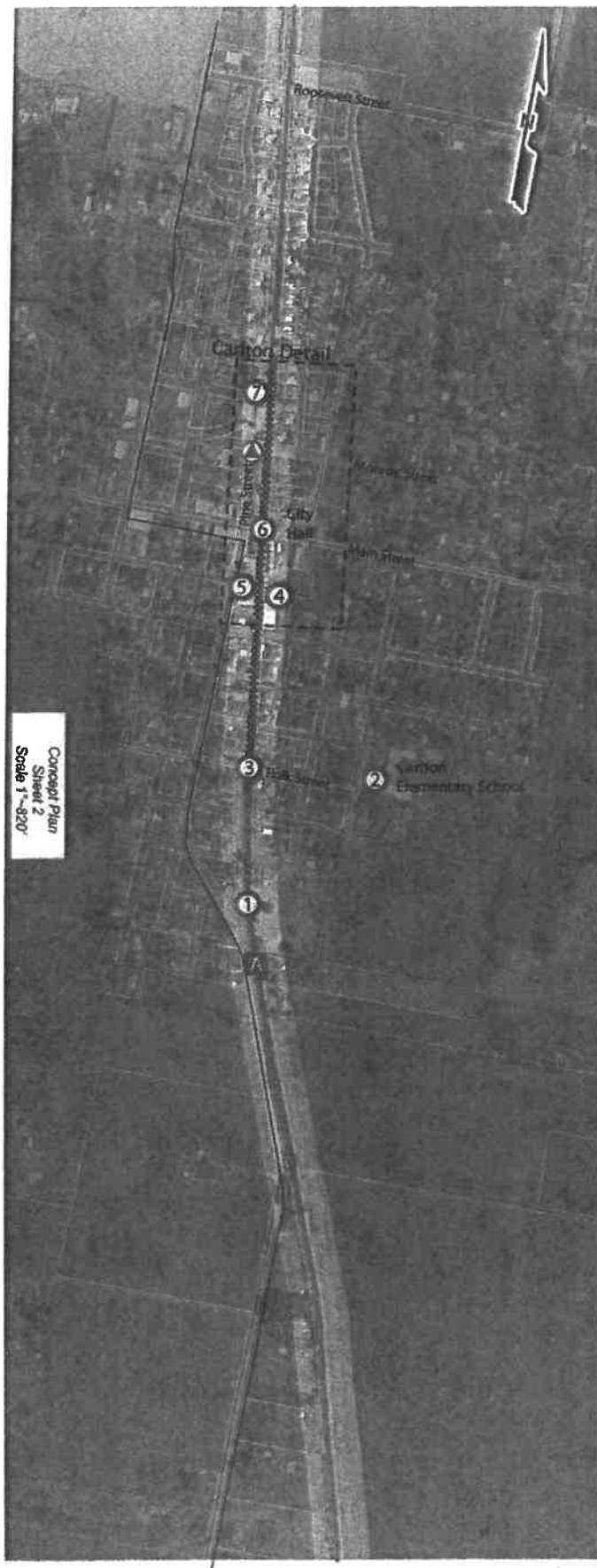
## Sheet 2: Carlton

-  Proposed trail
-  Property lines
-  Highway 47
-  Trailhead, proposed
-  Design Recommendations
-  AVA Stone Monument
-  Enlargement

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

### Notes

-  Carlton Trailhead
  - Paved lot, existing
  - Restrooms, proposed
  - Water fountains, proposed
  - Carlton sign & map
-  Carlton gateway sign & informational map
-  Potential trailhead overflow parking at Carlton Elementary School
-  East Polk Street crossing
  - signage
  - striping
  - pedestrian activated flashing lights
  - removable bollards
-  Horse Corral
-  Interpretive signage
  - Madsen grain elevator
-  East Main Street crossing
  - signage
  - striping
  - pedestrian activated flashing lights
  - removable bollards
-  Interpretive signage
  - historical spur
-  Commercial district corridor
  - Lighting
  - Benches
  - Potential increased width
  - Potential alternate surfacing
  - Increased landscaping
  - Screening, vegetative & constructed



Concept Plan  
Sheet 2  
Scale 1"=820'



***An artist rendering of Polk Street in Carlton.***

The image below depicts a pedestrian activated crossing. Removable bollards are used to help slow down pedestrian traffic, while acknowledging and highlighting to be aware of cross traffic. Striping is used, and flashing lights are activated when a pedestrian wants to cross.



***An artist rendering of the Madsen Grain Elevators and Silos in downtown Carlton.***

The image below illustrates how the Yamhelas Westsider Trail interacts with the preservation of the Madsen Grain Elevators and Silos. The Yamhelas Westsider Trail would like to celebrate the agricultural history of the county by incorporating interpretive elements about the Madsen Grain Elevator and Silos along the trail. In the heart of downtown Carlton, the Yamhelas Westsider provides the opportunity for active recreation, social neighborhood interactions, lounging in the sun, or grabbing a bite to eat.








### **NORTH OF CARLTON** *Sheet 3*

Alexander "Alec" Carson, Kit Carson's relative, is believed to have met his demise close to the rail corridor at a hill known as Alec's Butte. In his time, Alec Carson was a well-known trapper, frontiersman, and gunsmith. He served on the Astor Overland Expedition in 1811 and worked out of Fort Astoria on Astor hunting parties. This spot would make a good interpretive site focusing on the pioneer days of the region. *(Sheet 3 – Design Recommendation 1)*

In areas where agriculture traffic must cross the trail, crossing gates could be installed *(see artist example of agriculture crossing on page 19)*.

# Yamhelas-Westsider Trail Concept Plan

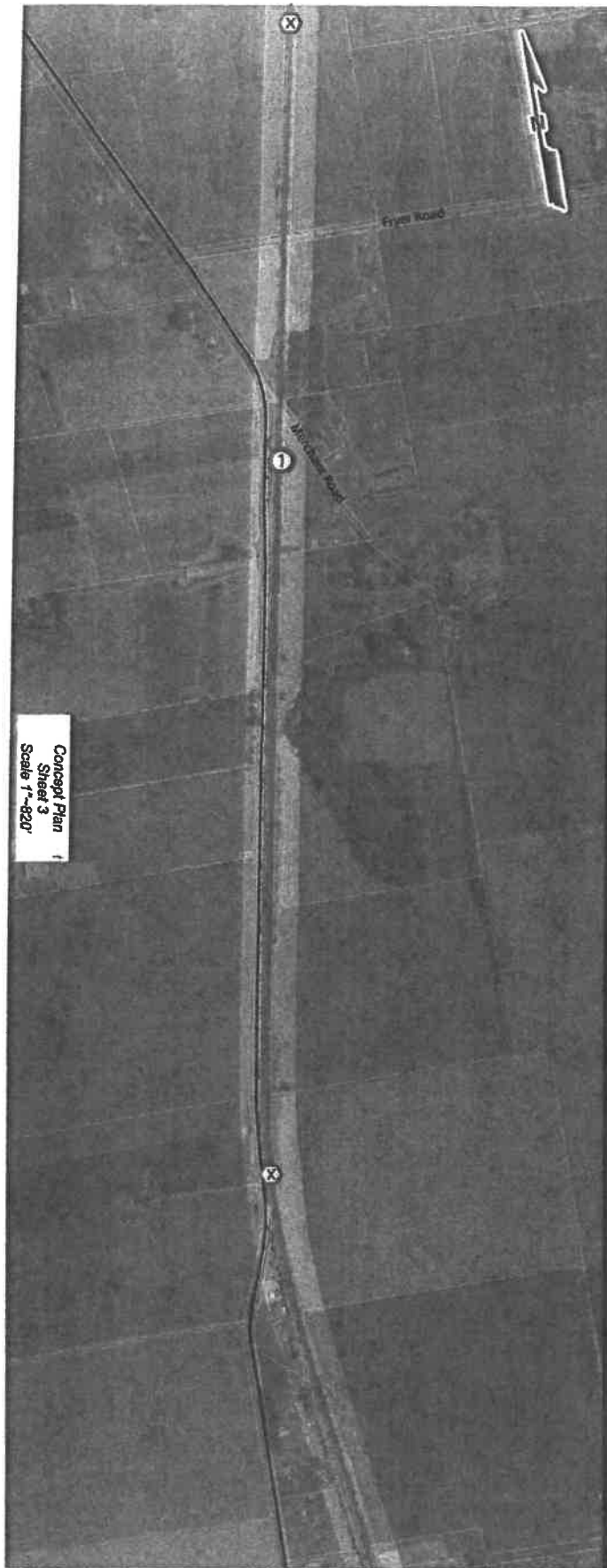
## Sheet 3

-  Proposed trail
-  Property lines
-  Highway 47
-  Design Recommendations
-  Agriculture Crossing

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

## Notes

- ① Historical Interpretive Signage  
-Alec Carson, early frontiersman and  
Kit Carson's relative



Concept Plan  
Sheet 3  
Scale 1"=820'

#### **YAMHILL Sheet 4**

On the way north to Yamhill, there are three bridges that need to be constructed (see Sheet 4). The first (southern) bridge would also make an excellent viewpoint and rest stop. A pull-out on or close to the bridge would allow for a nice view of the surrounding pastoral landscape. *(Sheet 4 – Design Recommendation 1) (See Artist Rendering of a Long Bridge Below)*

As the rail-trail comes close to Highway 240, the trail will need to take a jog to the west and stay as far away from the school district bus depot as possible. *(Sheet 4 – Design Recommendation 2)*

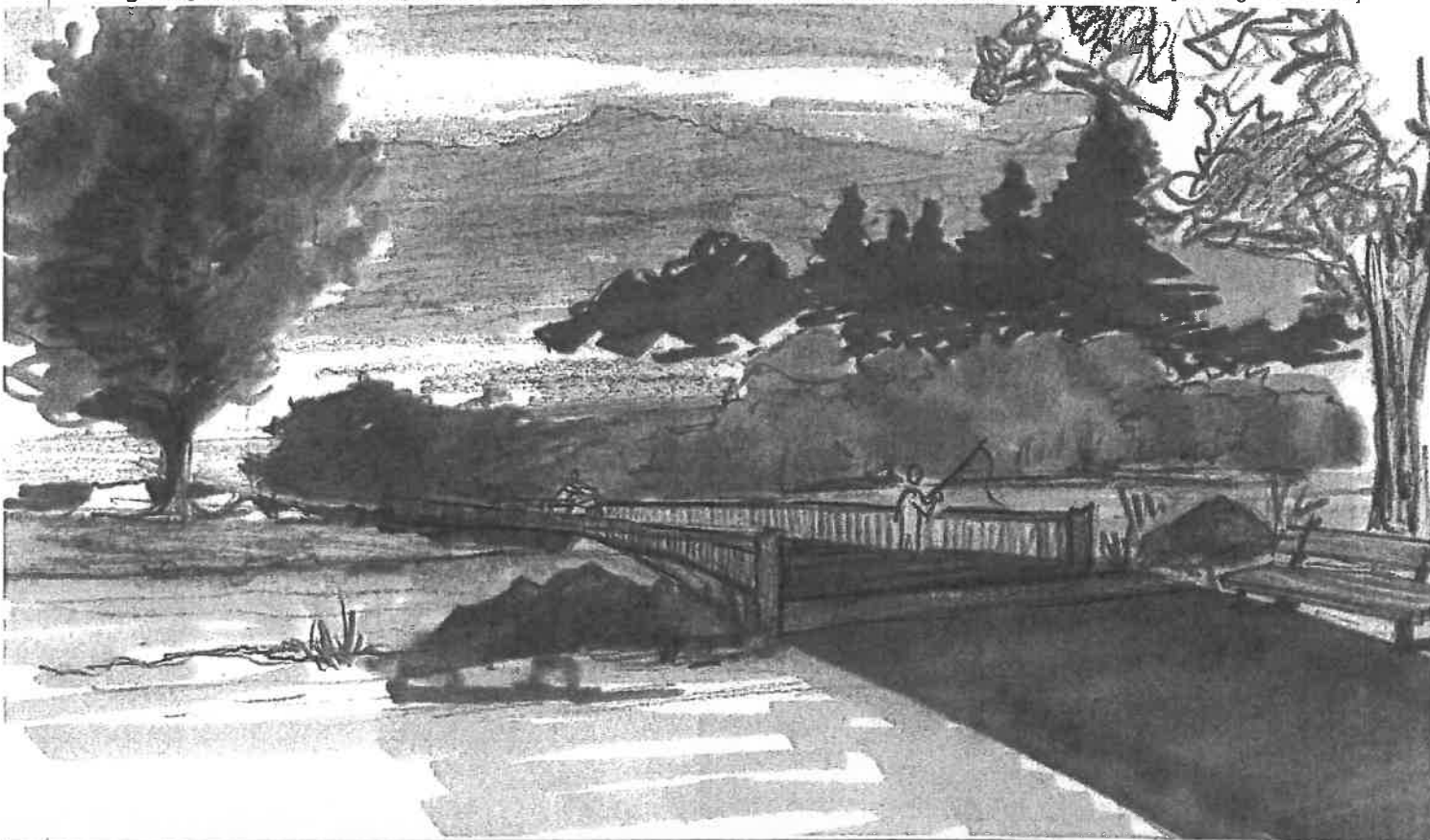
The crossing at Yamhill- Newberg Highway 240 *(Sheet 4 – Design Recommendation 3)* will need to be upgraded to a push button flashing light. This crossing is an important one because it will be used for getting to and from Yamhill High School (the regional high school). At the Yamhill Design Workshop, the recommendation was made for the route connecting to the center of Yamhill and Yamhill High School to be located on the south of Highway 240 (which becomes East Main Street in Yamhill). The crossing is needed to connect to the existing sidewalk on the south side of East Main Street. Yamhill County should work with the City of Yamhill and the Yamhill Carlton School District to extend the sidewalk all the way to the crossing with the Yamhelas Westsider Trail.

On the north side of Highway 240, the rail corridor was sold to the landowner with an easement attached to the deed allowing for a trail through the parcel. The trail should be routed the farthest west possible on this landowner's property. A chain link fence is desired by the landowner. This fence could be decorated with artwork by local and/or Yamhill Carlton High School artists. At the north end of this landowners property, the trail will need to curve back to align with the rail corridor heading north to Cove Orchard. *(Sheet 4 – Design Recommendation 4)*

In areas where agriculture traffic must cross the trail, crossing gates could be installed *(see artist example of agriculture crossing on page 19)*.







#### **An artist rendering of a long bridge.**

The image below was developed at the Yamhill table during the Design Workshop held in the Fall of 2015. It depicts the long bridge and rest area design *(Sheet 4- Design Recommendation 1)*, along with their potential uses. [Drawing not to scale.]



# Yamhelas-Westsider Trail Concept Plan

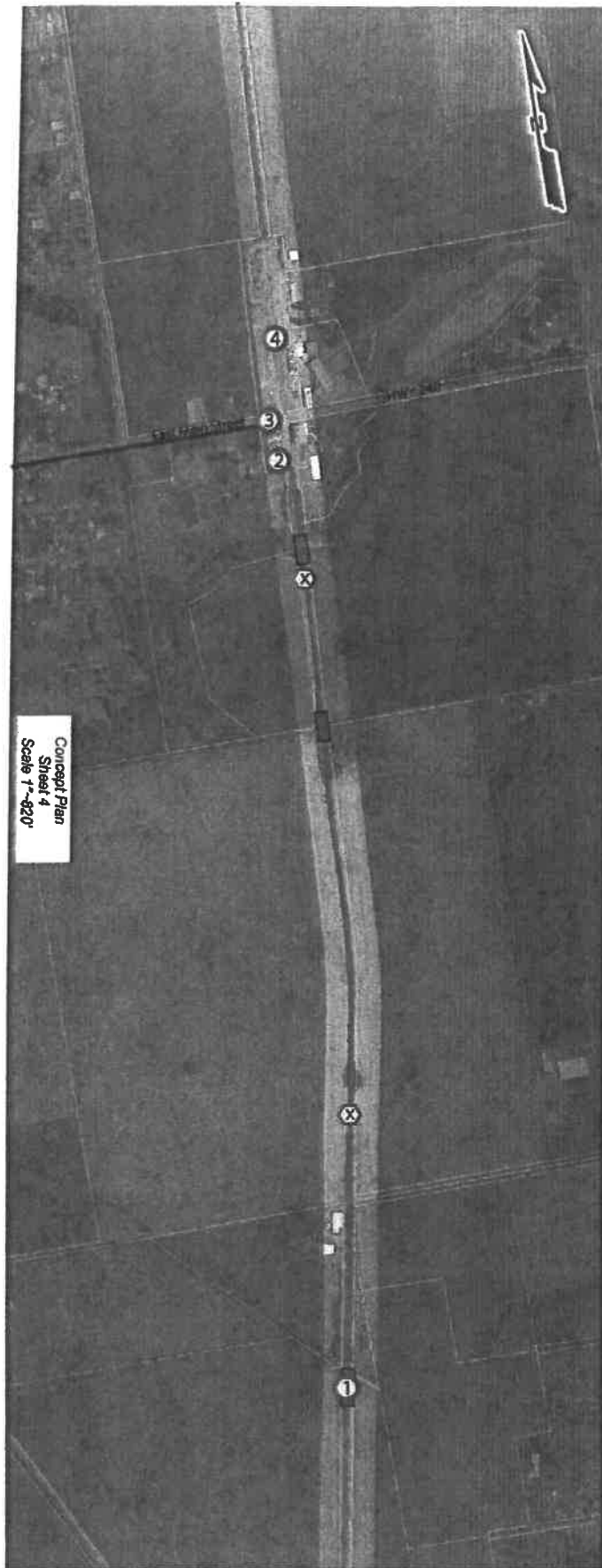
## Sheet 4: Yamhill

-  Proposed trail
-  Property lines
-  Proposed trail connection to Yamhill
-  Design Recommendations
-  Agriculture Crossing
-  Bridge

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

### Notes

- ① Bridge and Rest Area
- ② Trail leaves corridor and is located to the west of the Mid-Columbia Bus Depot
- ③ East Main Street crossing
  - signage
  - striping
  - pedestrian activated flashing lights
  - removable bollards
- ④ Easement through private property
  - Chain fence along Fruit Hill property
  - With potential input and art murals from local high school students



Concept Plan  
Sheet 4  
Scale 1"=820'




**NORTH OF YAMHILL** *Sheet 5*

No Recommendations.

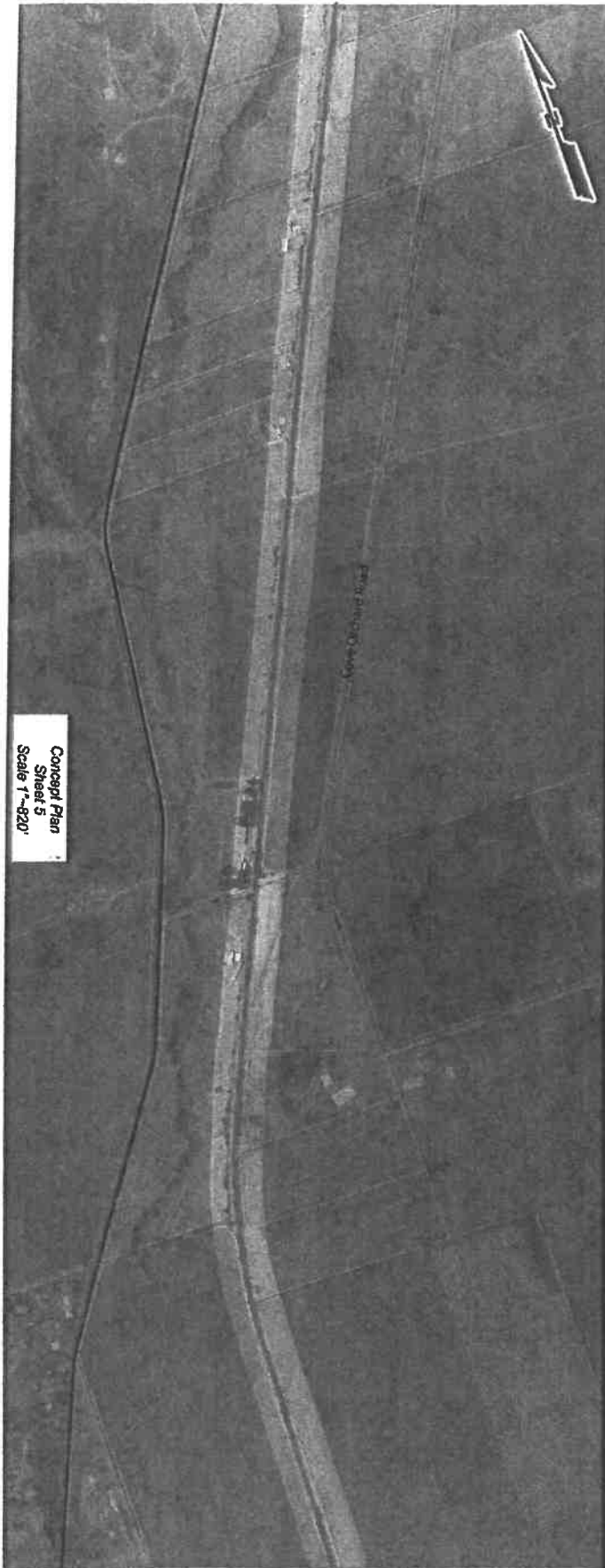


## Yamhelas-Westsider Trail Concept Plan

### Sheet 5

-  Proposed trail
-  Property lines
-  Highway 47

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.



Concept Plan  
Sheet 5  
Scale 1"=820'



## **COVE ORCHARD** *Sheet 6*

As the rail corridor enters into Cove Orchard, several landowners are utilizing the abandoned rail-line as a driveway for their property. No agreements exist for this use. There are three alternatives for this situation. The first is to work with the landowners and make accommodation for their use on the rail trail. The second is to have the landowners develop the platted street behind (north) of their houses. The third is for the trail to be routed on the undeveloped platted street behind the houses (north).

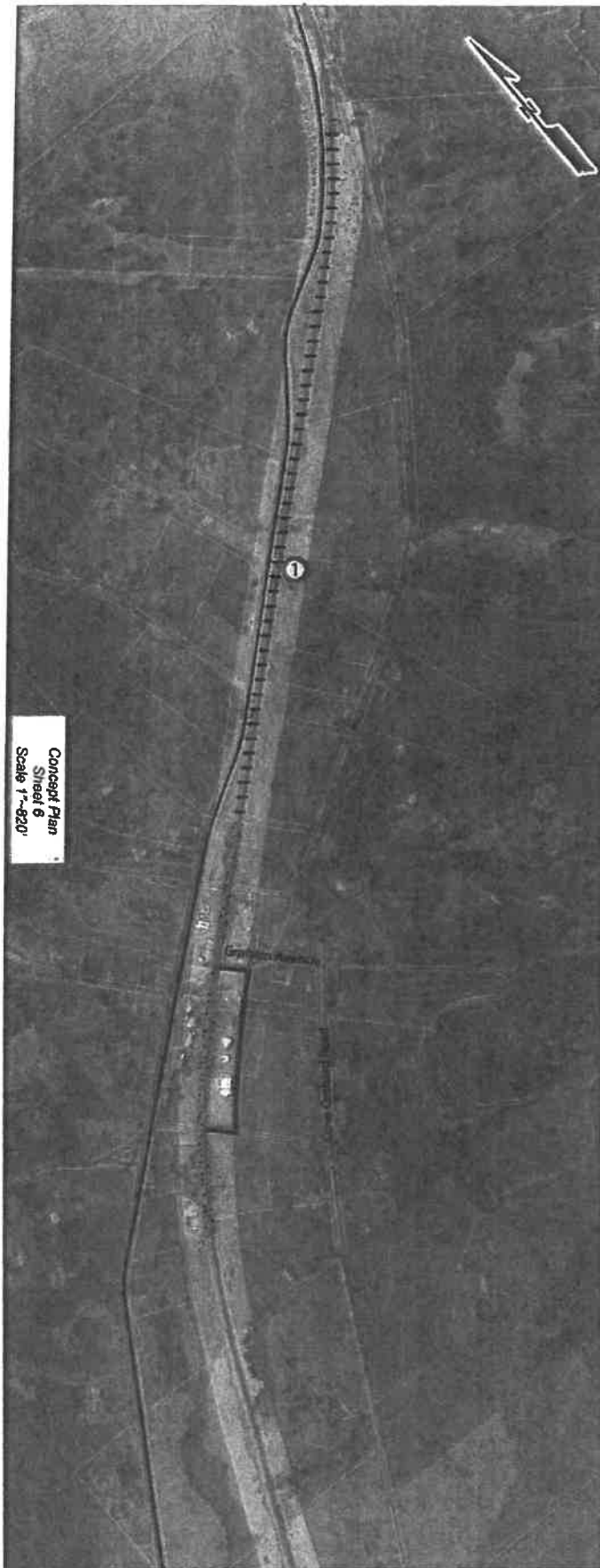
As the rail line goes through Cove Orchard, many houses are close to the trail corridor. This area is a good place to plant vegetative screening for the benefit of both the trail users and the landowners.








Continuing north of Cove Orchard, the trail corridor goes through an area where the surrounding landscape drains down to the vicinity where the rail line is located. As a result, this area can become marsh-like during the wet times of the year. This area will need to be studied to determine if there is a need to construct an elevated tread (punchion, turnpike, boardwalk, etc.)

Also north of town is the site of a derailed locomotive that was buried in a sinkhole. It is recommended to develop an interpretive sign and rest stop at this location. (*Sheet 6 – Design Recommendation 1*)

# Yamhelas-Westsider Trail Concept Plan

## Sheet 6: Cove Orchard



-  Proposed trail
-  Property lines
-  Potential trail alignment alternative
-  Highway 47
-  Design Recommendations
-  Vegetative screen
-  Potential wetland area, may need elevated trail
  - Boardwalk, puncheon, or turnpike to elevate trail from wet area and allow for drainage

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

### Notes

- ① Interpretive signage
  - locomotive burial site
  - low point & wetland

## **SOUTH OF GASTON** *Sheet 7*







Halfway between Wapato School Road and Flett Road, the rail corridor crosses a small creek; a bridge will need to be installed at this point. (*Sheet 7 – Design Recommendation 1*)

The intersection of Flett Road and the rail line has the potential to be a dangerous intersection. The rail corridor crosses Flett Road approximately 35 feet east of the intersection of Flett Road and Highway 47. This could lead to issues with vehicles, particularly trucks, turning from the higher speeds of Highway 47 (speed limit of 55 mph) on to Flett Road and having less than 40 feet to stop. This intersection will need to be studied and engineered in a way to allow for safe passage of trail users while accommodating the safety and needs of motorists on Highway 47 and Flett Road. One option is to work with the landowners to the north of this intersection to see if it would be possible to move the trail intersection with Flett Road to the north anywhere from 20 to 100 feet. (*Sheet 7 – Design Recommendation 2*)

In areas where agriculture traffic must cross the trail, crossing gates could be installed (*see artist example of agriculture crossing on page 19*).

# Yamhelas-Westsider Trail Concept Plan

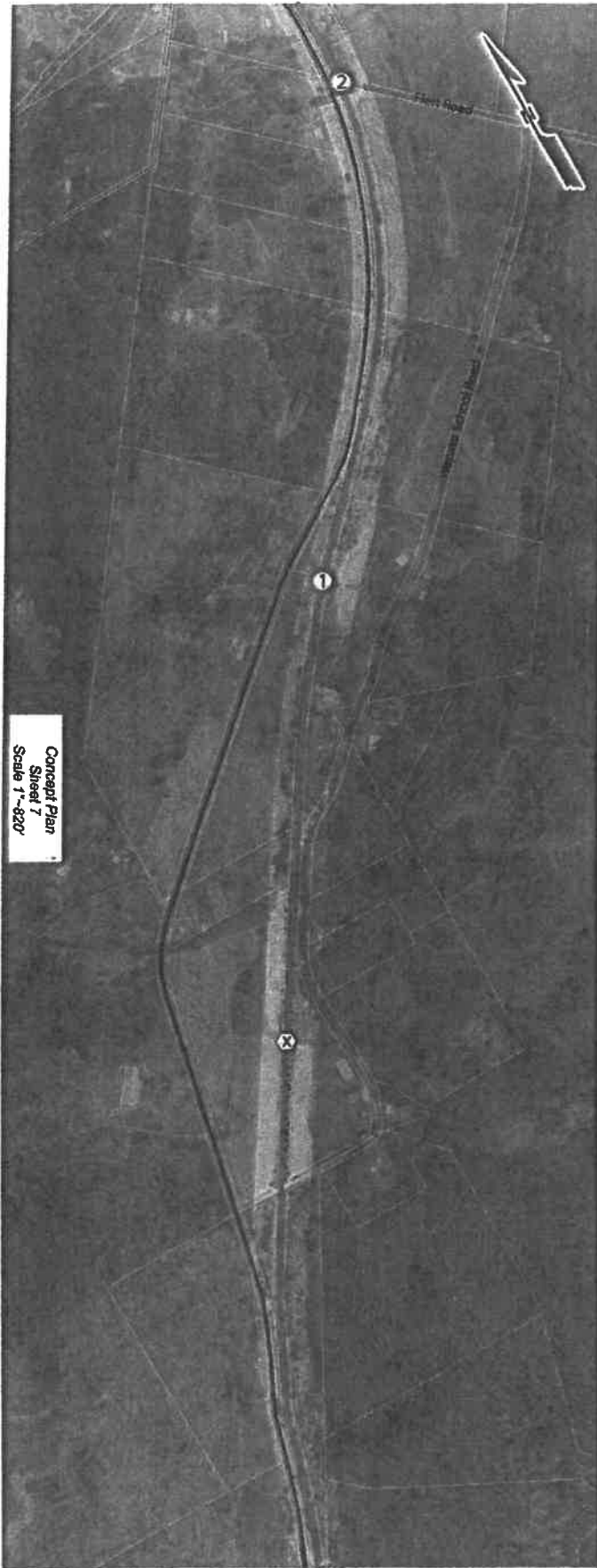
## Sheet 7

-  Proposed trail
-  Property lines
-  Highway 47
-  Design Recommendations
-  Agriculture Crossing
-  Vegetative screen

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

## Notes

- ① Creek crossing  
- small bridge required
- ② Flett Road crossing  
- study to design safe crossing



Concept Plan  
Sheet 7  
Scale 1"=820'

## **SOUTH OF GASTON** *Sheet 8*

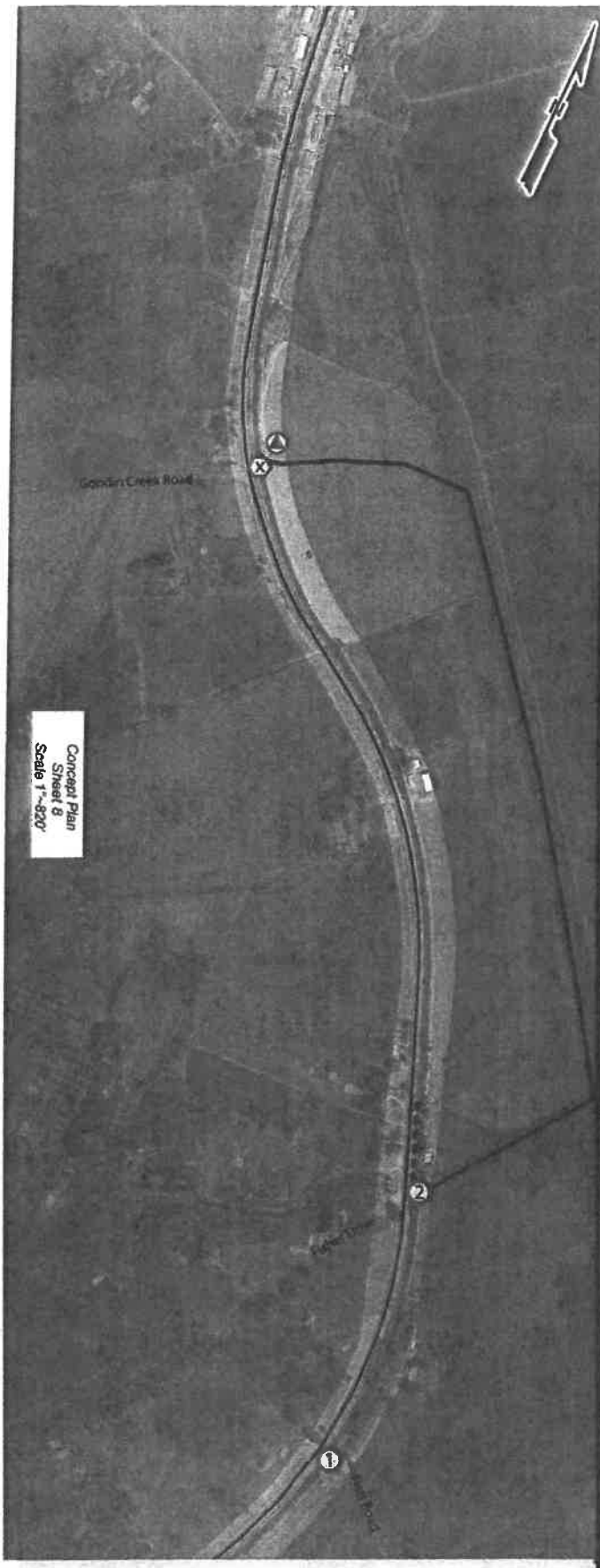
Near the intersection of Fisher Road and Highway 47, the rail corridor has been purchased by the landowner. This is approximately 255 feet of right of way. Either an easement or purchase of this right of way will need to be made. Otherwise, the trail will need to be placed within the Highway 47 right-of-way corridor or the trail will need to be routed to the east (behind the landowner's residence) and on to the United States Fish and Wildlife Service's (USFWS) lands where a dike heading north could be utilized as the trail bed. The trail could rejoin the rail alignment around the intersection of Highway 47 and Goodin Creek Road on the USFWS lands. (*Sheet 8 – Design Recommendation 2*)










The USFWS lands directly across from Goodin Creek Road could also serve as a potential equestrian trailhead for the northern section of the rail trail. It is not recommended that this be developed until the need for such a facility is studied.

In areas where agriculture traffic must cross the trail, crossing gates could be installed (*see artist example of agriculture crossing on page 19*).

# Yamhelas-Westsider Trail Concept Plan




## Sheet 8



-  Proposed trail
-  Property lines
-  Highway 47
-  Potential trail alignment alternative
-  Trailhead, proposed
-  Design Recommendations
-  Agriculture Crossing
-  Vegetative screen
-  Trail to hug highway 47 ROW with minimal trail ROW width. Easement or ODOT agreement needed.

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## Notes

-  Potential USFWS Trailhead
  - Equestrian friendly
  - Restrooms, proposed
  - National Wildlife Refuge sign & map
-  Flett Road crossing
  - study to design safe crossing
-  Work with land owner to provide trail access or develop alternative trail alignment.

### **GASTON** Sheet 9 and Gaston Detail

Gaston will serve as the northern trailhead for the Yamhelas Westsider Trail. The Gaston Trailhead can also serve as a trailhead and entrance for the USFWS Wapato Lake Unit, where several miles of trails on the old dikes are being studied for development. This trailhead will be located on the south end of Gaston close to the Yamhill and Washington County border. The trailhead could be developed on either side of the county line; however, it seems more practical to develop it on the Washington County side. The best location for the trailhead is at the intersection of the rail corridor and the access road to Wapato Lake (*Sheet 9 – Design Recommendation 2/ Proposed Gaston Trailhead*). It is recommended for the City of Gaston to work with Washington County on acquiring and developing the rail corridor for the trailhead. In addition, these two entities can work to develop an upgraded transit stop just north of the trailhead (see Gaston Detail).

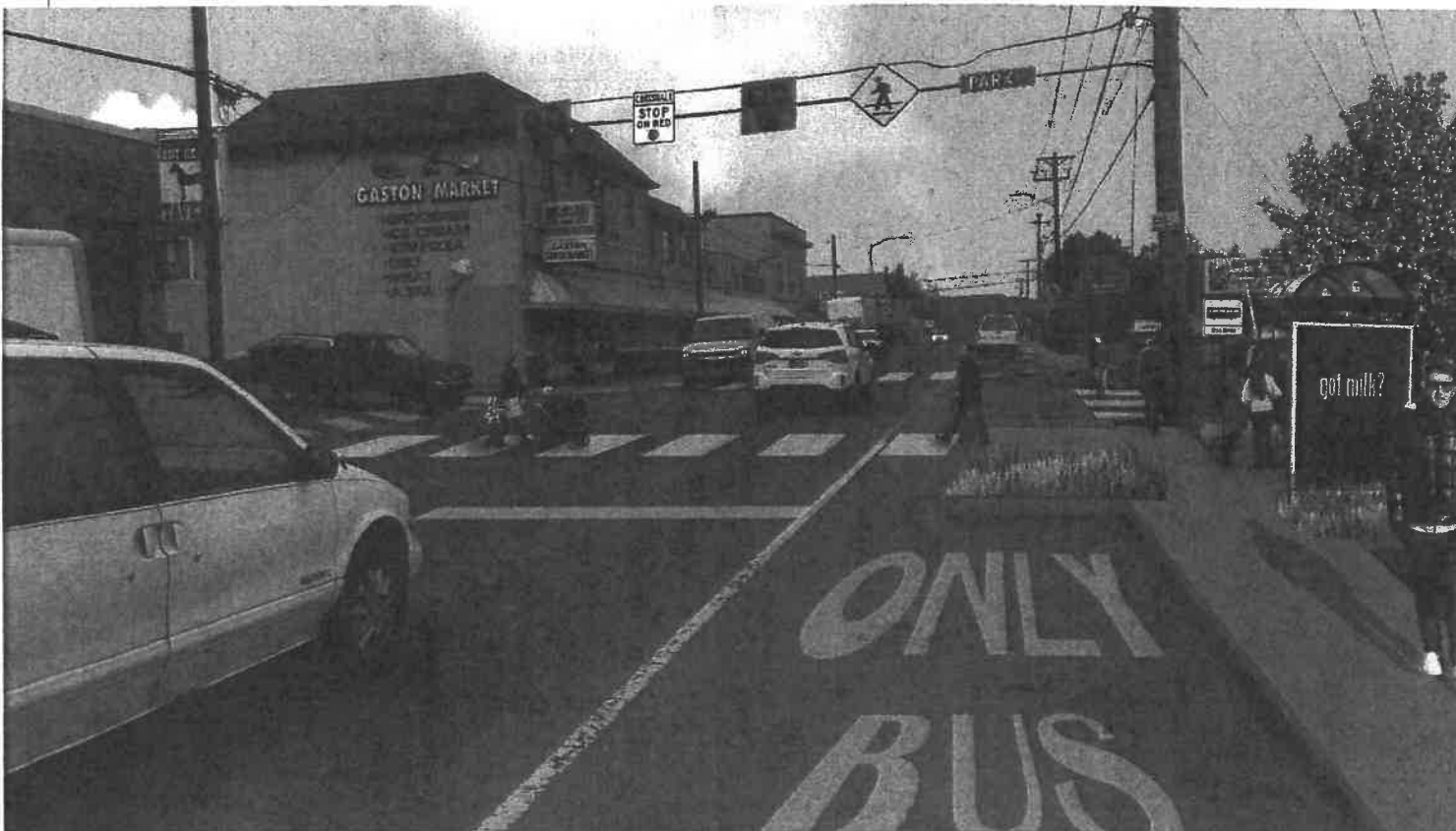
There are currently unmarked and marked pedestrian crossing across Highway 47 due for major upgrades. This is to ensure the safe passage to the rail trail for Gaston residents and visitors alike. The most southern crossing of Highway 47 recommended for upgrade is at Olson Road (*Sheet 9 – Design Recommendation 1*). The other crossing is at Park Street (*Sheet 9 – Design Recommendation 4*). Both crossings should receive pedestrian triggered flashing lights in addition to striping and signage.

The City of Gaston would like to see the trail utilize Onion Lane (which would be dedicated for trail use only) in downtown Gaston rather than continue on the existing rail corridor. This is to allow for more parking and business opportunities in the commercial district. (*Sheet 9 and Gaston Detail*)

At the north end of town a bridge will need to be constructed over the Tualatin River.








#### ***An artist rendering of Park Street Crossing in Downtown Gaston.***

The image below illustrates an enhanced streetscape in the commercial corridor of Gaston, along Highway 47. As well, it depicts Park Street Crossing. This new traffic stop light is envisioned to help pedestrian and kids cross Highway 47 safely. Bumpouts, striping, cross-walk lights, and vehicular setbacks help to ensure visibility of pedestrians. The current bus stop in the City of Gaston will be enhanced, and marked. Sidewalks and swales will be added along Highway 47 to help connect residents and visitors alike to many amenities such as the Yamhelas Westsider Trail, Parking, Businesses, and Wildlife Access areas.








# Yamhelas-Westsider Trail Concept Plan

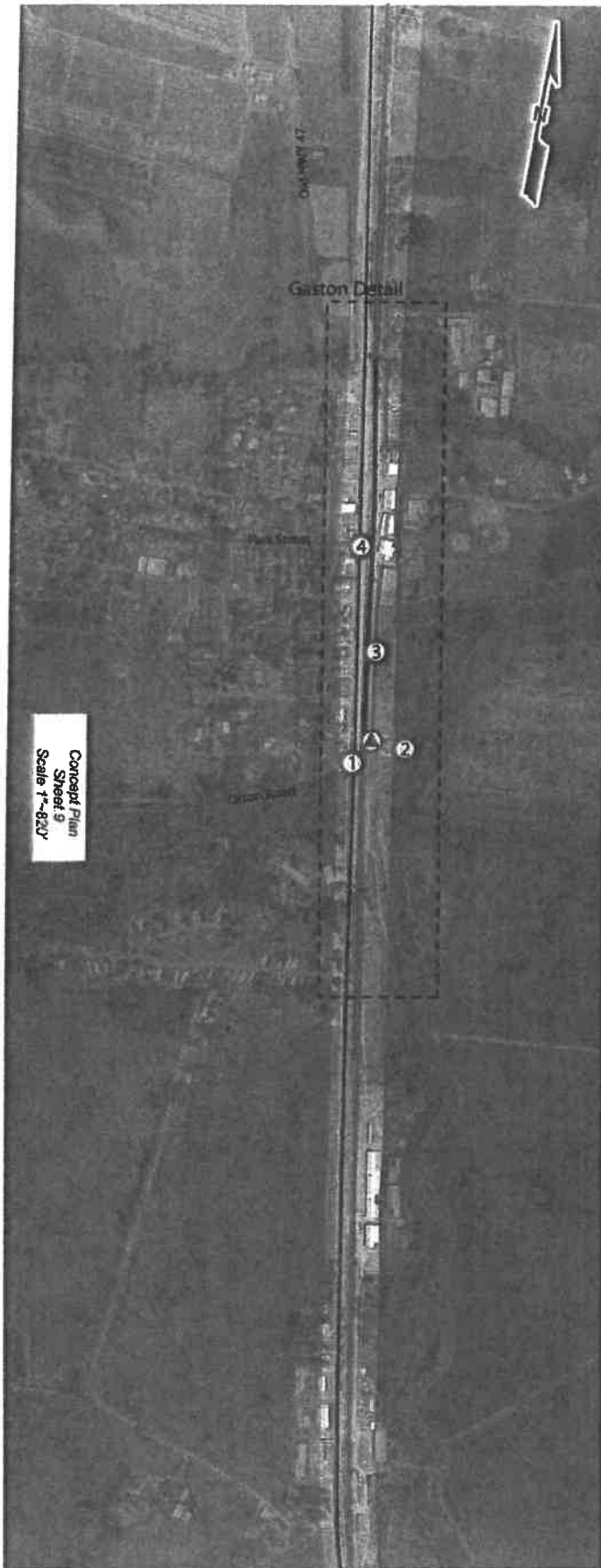
## Sheet 9: Gaston

-  Proposed trail
-  Property lines
-  Highway 47
-  Potential trail route alternative
-  Trailhead, proposed
-  Design Recommendations
-  Gaston commercial district - detail page 47

\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

### Notes

-  Proposed Gaston Trailhead
  - Proposed Restrooms
  - Informational signage
  - Picnic area
-  Olson Road crossing
  - Pedestrian access across HWY 47 to trail
  - signage
  - striping
  - pedestrian activated flashing lights
-  Access point to US Fish & Wildlife Wapato Lake National Wildlife Refuge
-  Parking
-  Park Street crossing
  - Pedestrian access across HWY 47 to trail
  - signage
  - striping
  - pedestrian activated flashing lights
  - proposed traffic light



Concept Plan  
Sheet 9  
Scale 1"=800'









## **NORTH OF GASTON** *Sheet 10*

Currently the rail abandonment ends at Scoggins Creek. On the other side of the creek, the abandoned corridor merges with an active rail line that heads north towards Forest Grove, and west to the Stimson Mill (paralleling Scoggins Valley Road). It is not recommended to have a trailhead at this location, as there is one slated within Gaston city-limits itself. In addition, if the active line heading north becomes inactive, a connection can easily be made to Forest Grove, where the next likely trailhead would be located. The other option in making a connection to Forest Grove would be to move the trail into the right-of-way along Highway 47, and utilize the road corridor as the connection to the existing trail systems in Forest Grove.

In areas where agriculture traffic must cross the trail, crossing gates could be installed (*see artist example of agriculture crossing on page 19*).

# Yamhelas-Westsider Trail Concept Plan

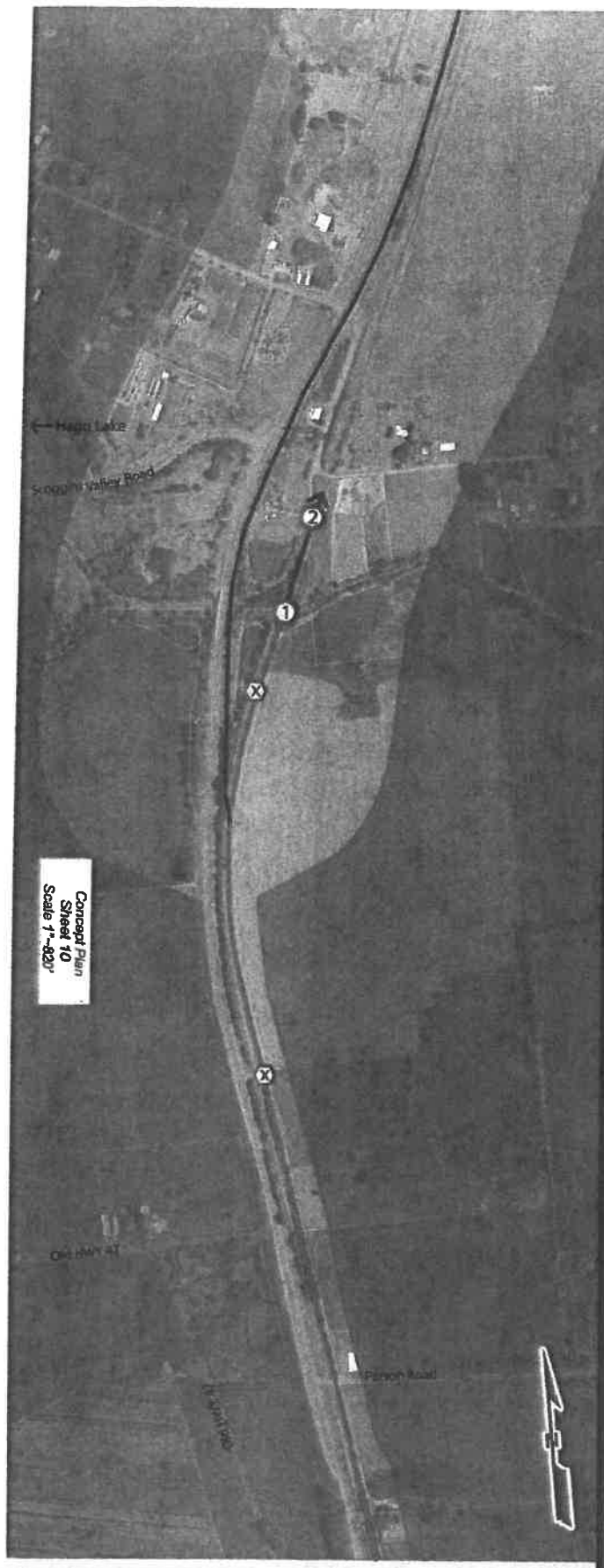
## Sheet 10

-  Proposed trail
-  Property lines
-  Highway 47
-  Potential trail extension  
alternative to Forest Grove  
- along Highway 47 shoulder  
& right-of-way
-  Design Recommendations
-  Agriculture Crossing

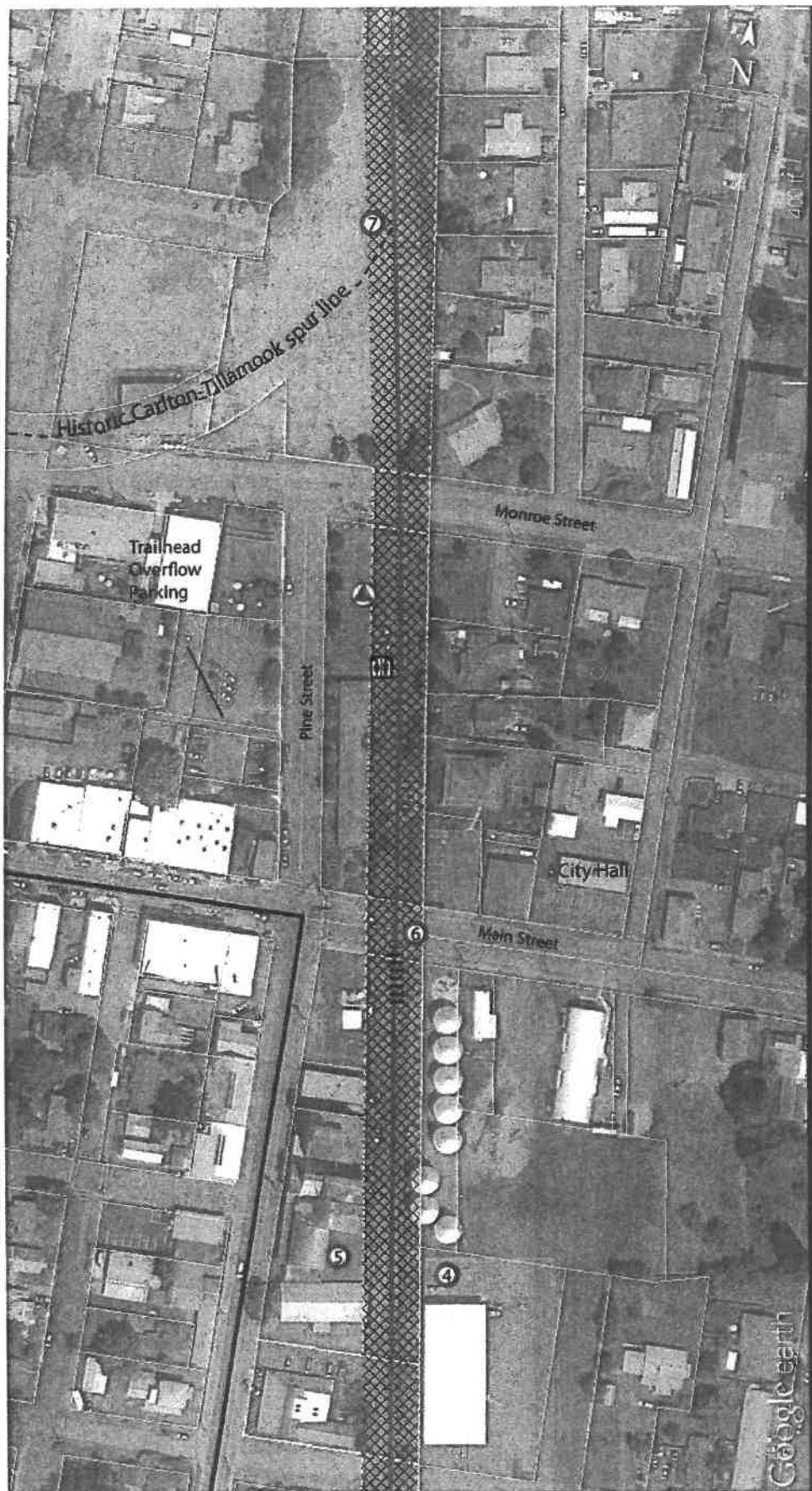
\*\*\*The highlighted portion only represents the 150' mark on either side of the proposed trail. The highlighting is only used to focus attention on the ROW, which typically is 60' but varies in places. In no way does the highlighting delineate land ownership or proposed design.

## Notes






- ① Scoggins Creek  
- End of current rail abandonment
- ② Work with railroad to acquire corridor once abandoned or establish a rail with trail.



Concept Plan  
Sheet 10  
Scale 1"=820'



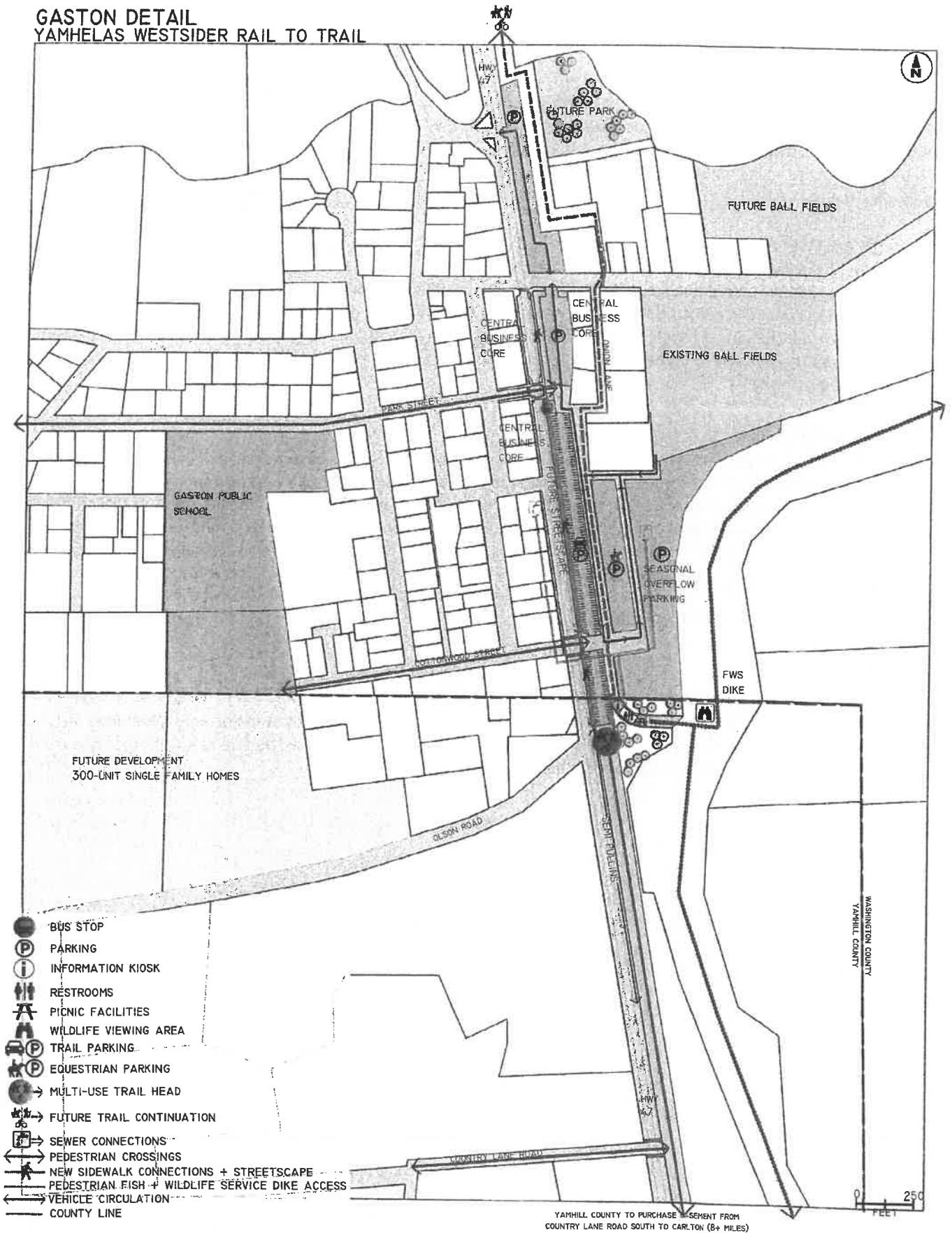
## Carlton Detail

-  Proposed trail
-  Property lines
-  Highway 47
-  Trailhead, proposed
-  Design Recommendations

## Notes

-  Carlton Trailhead
  - Paved lot, existing
  - Restrooms, proposed
  - Water fountains, proposed
  - Carlton sign & map
-  Horse Corral
-  Interpretive signage
  - Madsen grain elevator
-  East Main Street crossing
  - signage
  - striping
  - pedestrian activated flashing lights
  - removable bollards
-  Interpretive Signage
  - historical spur
-  Commercial district corridor
  - Lighting
  - Benches
  - Potential increased width
  - Potential alternate surfacing
  - Increased landscaping
  - Screening, vegetative & constructed

# GASTON DETAIL YAMHELAS WESTSIDER RAIL TO TRAIL



# **MANAGEMENT *and* MAINTENANCE**

## **YAMHILL COUNTY'S ROLE**

Yamhill County's role in the development of the Yamhelas Westsider Trail is to work directly with their project partners in developing right-of-way into a multi-modal transportation corridor. The county actively supports the project by providing staff and elected officials in the support of: grant writing, project management, technical assistance, project development, implementation, on-going maintenance and other key support roles to the activities of the Friends of the Yamhelas Westsider Trail.

## **FRIENDS OF THE YAMHELAS WESTSIDER ROLE**

The Friends of the Yamhelas Westsider Trail, a 501(c)3 non-profit, is the county's main project partner. The Friends are responsible for public outreach, marketing, fundraising and maintenance both before and after the corridor is complete. The Friends of the Yamhelas Westsider Trail initiated a partnership with the county in 2012 in order to access transportation grant funding through state and federal sources. With an active board and membership base, the group will continue to lead the trail's development efforts in partnership with both the local governments and the greater Yamhill-Carlton community.

## **MOU for MAINTENANCE**

Yamhill County is committed to assisting with regular routine maintenance such as mowing and landscaping, clearing debris and vegetation, and minor trail surface repairs. The Friends of the Yamhelas Westsider Trail have committed to participate in on-going maintenance and improvements of the trail in partnership with Yamhill County. As well, the Friends of the Yamhelas Westsider Trail have committed to any other maintenance not covered by Yamhill County utilizing local resources in the form of volunteers and in-kind donations. These commitments are to be spelled out in greater detail through a Memorandum of Understanding (MOU) that is in the process of being drafted. It is estimated that there will be a comprehensive trail maintenance cost of \$1,500 per mile each year, which for a 12-mile asphalt trail, at ten to twelve-feet wide, would be estimated at approximately \$18,000 per year.

## **EMERGENCY SERVICES**

Police, Fire and Medical were all involved in the concept design planning process.

During the design workshops it was decided to strategically place removable bollards at major trail/ street intersections for emergency access to the trail. A contingency plan is in place for the County to utilize the trail as an emergency corridor if the roads are closed due to an earthquake or other natural disaster.

# IMPLEMENTATION

## PHASING

The Yamhelas Westsider Trail will be developed in phases over time and largely dependent upon available funding. It is the goal of the county and the Friends of the Yamhelas Westsider Trail to develop the corridor to meet all federal, state and local standards and requirements, to ensure the safety of all users and the long-term sustainability of the trail. Upon completion of the acquisition phase, the project will be developed as funding is granted. Critical areas for development will be the principle areas of focus including but not limited to: stream crossings, heavily trafficked intersections, dangerous crossings, and sections of critical connections to education and economic centers.

Phase One of the project is the initial acquisition of the corridor. This phase will entail the purchase of approximately 12-miles of a right of way corridor. This section will be purchased with funds awarded through the Federal Highways Administration's Transportation Enhance grant administered by the Oregon Department of Transportation (ODOT) Transportation Alternatives Program, the Oregon Parks and Recreation Department's Local Government Grant Program, Yamhill County funds and Friends of the Yamhelas Westsider Trail funds. There are also two land donations for the project that are located in the downtown core of Carlton. The total acquisition of the corridor will not be completed during this phase; however, Yamhill County and the project partners will continue to work towards the acquisition of the entire corridor.

Phase Two of the project will focus on the development of the trail between the rural cities of Yamhill and Carlton. Connecting these two communities directly addresses the major safety concerns related to transportation and commuting along Highway 47. This section is approximately three miles long and has three stream crossings. In August 2016, Yamhill County was awarded a grant through the Connect Oregon VI Grant Program, funded by Oregon State Lottery. This successful grant application will initiate the trail's development with the planning, design and engineering of all three stream crossings, as well as the construction of the largest bridge over Stag Hollow Creek. Once these bridges are complete, a rugged continuous path between Yamhill and Carlton will be established. Completion of this section will include the entire build-out of the trail to comply with federal multipurpose transportation corridor standards.

Phase Three will consist of continued development of the trail to the north of Yamhill and south of Carlton. These phases are approximations and the county and project partners will utilize federal, state and local resources to ensure that the project is developed in compliance with all applicable policies, regulations, rules and statutes.

## FUNDING

The initiation of this project began with the successful grant application to the Oregon Statewide Transportation Improvement Program's (STIP) Transportation Enhancement Program, which is funded by the Federal Highways Administration. The project was awarded a total of \$1.7 million with a total project budget of approximately \$1.9 million. Both Yamhill County and the Friends of the Yamhelas Westsider Trail provided a cash match for this grant. The grant funded the initial planning, preliminary engineering, NEPA Categorical Exclusion process, right-of-way services and future acquisition of approximately 8.4 miles of railroad right-of-way. The majority of the corridor is owned by the Union Pacific Railroad Company and all purchased parcels will be bought directly from the company. The Oregon Parks and Recreation Department awarded the project a grant for \$126,000 for acquisition in 2014. All available funding will be used to purchase the corridor in spring/summer of 2017.

In August 2016 the project was awarded \$1,102,186 by the Connect Oregon VI State Lottery grant program. This grant will fund the planning, design and engineering of three stream crossings and the construction of the Stag Hollow Creek Bridge. Yamhill County and the Friends of the Yamhelas Westsider Trail will continue to pursue grants at the federal, state and local level, including foundations. The partnership of the private and public entities allows for the project to be funded by a variety of sources beyond traditional government funding pathways. The non-profit group also solicits direct donations from the public as well as organized strategic fundraising campaigns.

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## IMAGE CITATIONS

Mountainous Country-Side Near Gaston Oregon (page 8)

Image found at: <<https://www.winebags.com/wp-content/uploads/2015/11/elkcove600.jpg>>

Willamette Valley (page 9)

Image found at: <<https://yamhillvineyardsbb.com/area.h46.jpg>>

Yamhill County Vineyards and Natural Landscape (page 9)

Image found at: <[http://www.yamhillvalleywines.com/pub/photo/thumb/trisaetum\\_vine\\_harvest\\_08\\_cropto\\_700x360.jpg](http://www.yamhillvalleywines.com/pub/photo/thumb/trisaetum_vine_harvest_08_cropto_700x360.jpg)>

Tualatin Watershed Natural Habitat (page 9)

Image found at: <[https://www.fws.gov/uploadedImages/Region\\_1/NWRS/Zone\\_2/Tualatin\\_River\\_Complex/Tualatin\\_River/Images/IMG\\_5703\(1\).JPG](https://www.fws.gov/uploadedImages/Region_1/NWRS/Zone_2/Tualatin_River_Complex/Tualatin_River/Images/IMG_5703(1).JPG)>

Tualatin River National Wildlife Refuge (page 9)

Image found at: <[https://upload.wikimedia.org/wikipedia/commons/0/0f/Tualatin\\_River\\_National\\_Wildlife\\_Refuge\\_pond\\_in\\_fall.JPG](https://upload.wikimedia.org/wikipedia/commons/0/0f/Tualatin_River_National_Wildlife_Refuge_pond_in_fall.JPG)>

Doug Fir Tree (page 10)

Image found at: <<https://www.pinterest.com/explore/douglas-fir-tree/>>

Wapato Flower (page 10)

Image found at: <<http://3.bp.blogspot.com/-LgCSp3d7fDo/UNAz-NMp4il/AAAAAAAAAM/z1LLJ7VPNEM/s1600/DSCN7489.JPG>>

Townsend Big Eared Bat (page 10)

Image found at: <<https://www.nps.gov/chis/learn/nature/townsend-bats.htm>>

Great Blue Heron (page 10)

Image found at: <<http://dunescenter.org/wp/wp-content/uploads/2015/01/great-blue-heron-in-water.jpg>>

Western Meadowlark (page 10)

Image found at: <<http://greglasley.com/images/W/Western-Meadowlark-0020.jpg>>

Historic Train Depot (page 13)

Image found at: <<https://mac100yearsago.files.wordpress.com/2012/05/rr-depot.jpg>>

Agriculture Equipment on the Road (page 17)

Image found at: <<http://www.farmsafety4kids.net/uploads/4/8/3/3/48337231/211243372.jpg>>

Trails for Health (page 17)

Image found at: <<http://www.railstotrails.org/media/40579/benefits-health.jpg>>

Bicyclist on Shoulder of Road (page 17)

Image found at: <[https://www.transportation.gov/sites/dot.gov/files/docs/RSA4\\_0.jpg](https://www.transportation.gov/sites/dot.gov/files/docs/RSA4_0.jpg)>

Multimodal Trail Sculpture Example (Page 18)

Image found at: <[http://www.essexheritage.org/sites/default/files/styles/news\\_event\\_full/public/field/image/geordie\\_vining\\_bicycling\\_the\\_clipper\\_city\\_rail\\_trail.jpg?itok=quy4ajVX](http://www.essexheritage.org/sites/default/files/styles/news_event_full/public/field/image/geordie_vining_bicycling_the_clipper_city_rail_trail.jpg?itok=quy4ajVX)>

Multimodal Trail Art Example (page 18)

Image found at: <[http://www.visitiowa.org/uploads/images/wl-640xhl-480xq-95~SubImage01\\_1302960.jpg](http://www.visitiowa.org/uploads/images/wl-640xhl-480xq-95~SubImage01_1302960.jpg)>



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*The Friends of the Yamhelas Westsider Trail and the National Park Service: Rivers, Trails, Conservation Assistance Program (RTCA) would also like to thank all the individuals who participated in the design workshop, community members who provided feedback via the online survey, and citizens who took the time to attend and provide input at any of the public meetings.*

# **APPENDIX**

## **YAMHELAS WESTSIDER TRAIL**

Community Profile  
Community Survey  
Community Survey Results

PAGE II-V  
PAGE VI-XV  
PAGE XVI-XVII

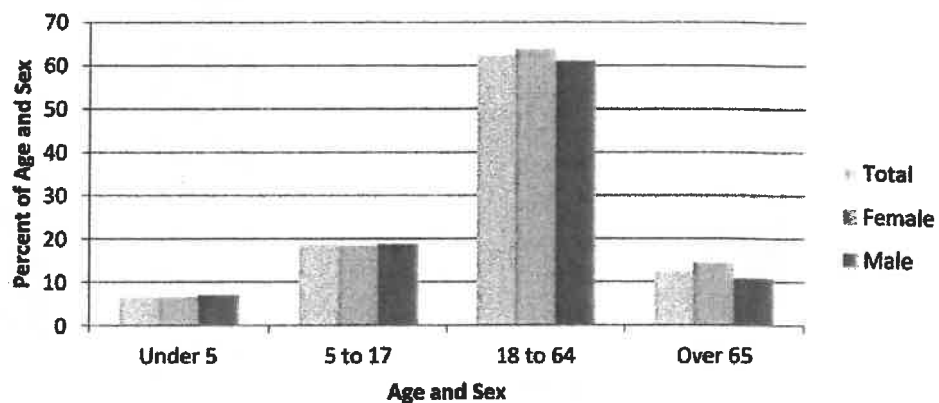
# Yamhill County Community Profile

This data was selected from the Yamhill County Community Health Assessment to inform the design process of the Yamhelas-Westsider Trail Charrette on November 14, 2015.

## Demographics

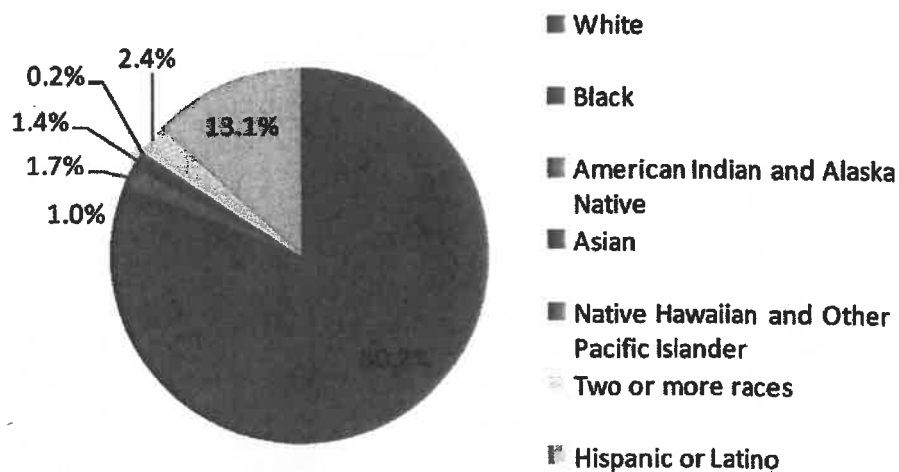
Source: Community Health Assessment  
Yamhill County, Oregon  
2013

### Yamhill County Age and Sex



Source: American Community Survey, 2005-2009

### Yamhill County Race/Ethnicity

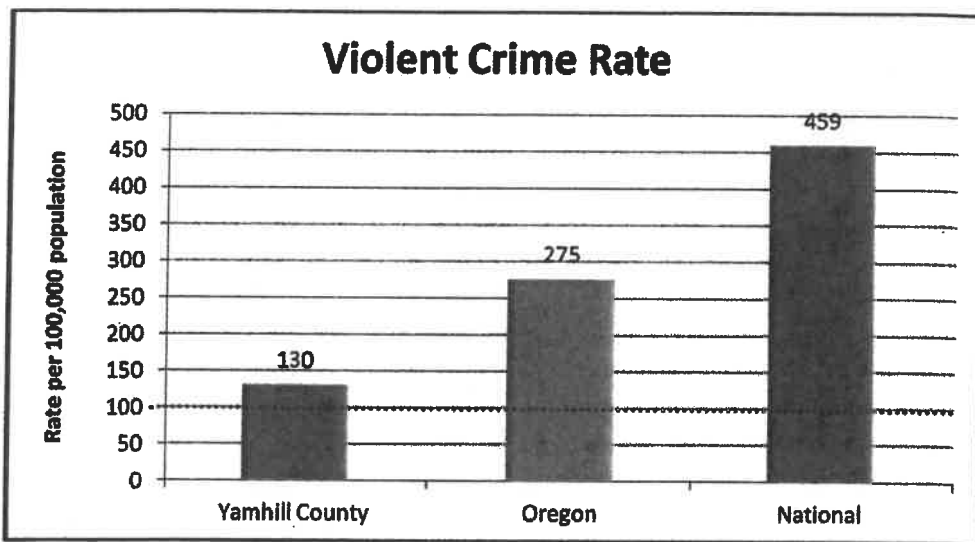


Source: U.S. Census Bureau, 2010

# Safety

Source: Community Health Assessment  
Yamhill County, Oregon  
2013

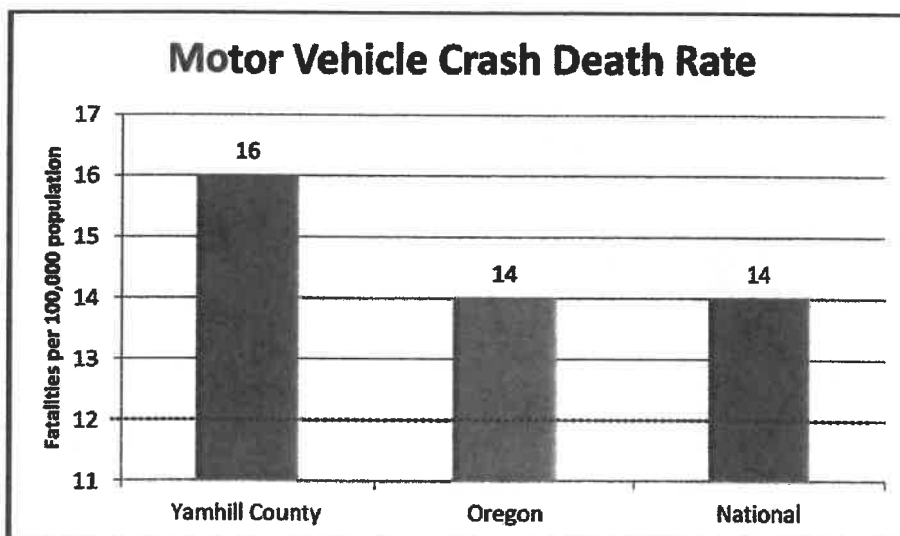
High levels of violent crime compromise physical safety and psychological well-being. Crime rates can also deter residents from pursuing healthy behaviors such as exercising out-of-doors.



**Target:**  
**100**  
2006-2008  
County Health Rankings

Source: Criminal Justice Information Services, 2008

According to the Centers for Disease Control and Prevention (CDC), motor vehicle crashes are a leading cause of death in the United States. Injuries from motor vehicle accidents are also burdening our emergency departments where more than 2.3 million adult drivers and passengers were treated for injuries in 2009 (CDC).



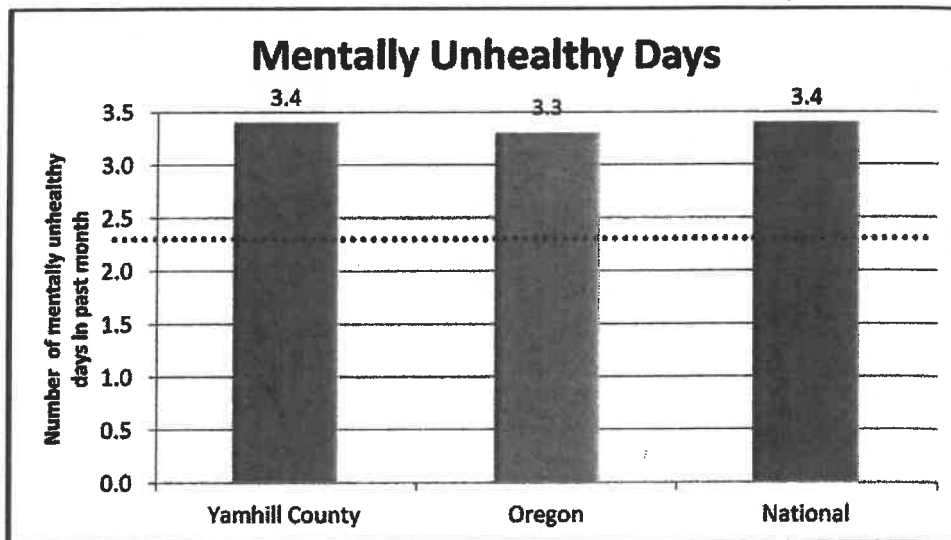
**Target:**  
**12**  
2001-2007  
County Health Rankings

Sources: National Vital Statistics System, 2001-2007; Fatality Analysis Reporting System, 2000-2007

## Physical & Mental Health

Source: Community Health Assessment  
Yamhill County, Oregon  
2013

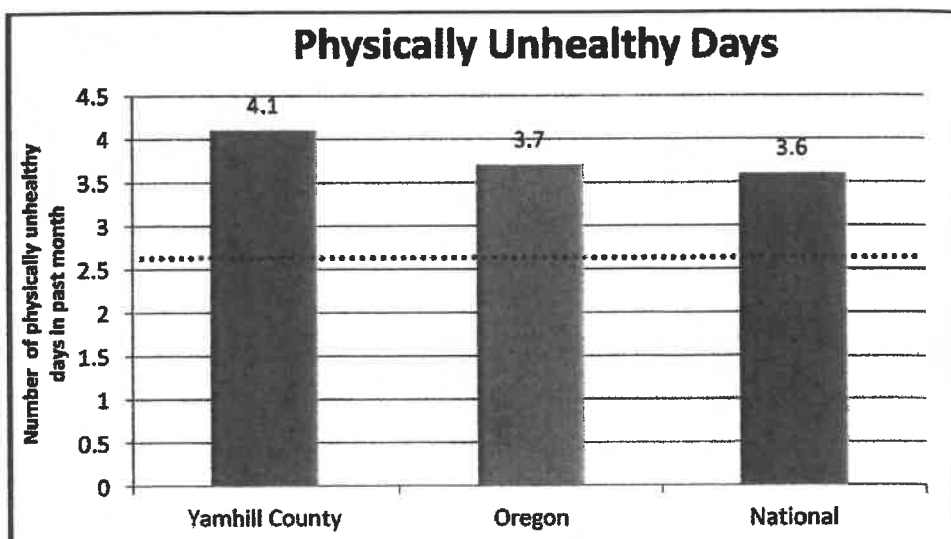
Overall health depends on both physical and mental well-being. Measuring the number of days when people report that their mental health was not good represents an important facet of health-related quality of life. The County Health Rankings considers health-related quality of life to be an important health outcome.



**Target:**  
2.3  
2003-2009  
County Health Rankings

Source: Behavioral Risk Factor Surveillance System, 2003-2009

In addition to measuring how long people live, it is also important to include measures of how healthy people are while alive. The County Health Rankings considers people's reports of days when their physical health was not good a reliable estimate of their recent health.

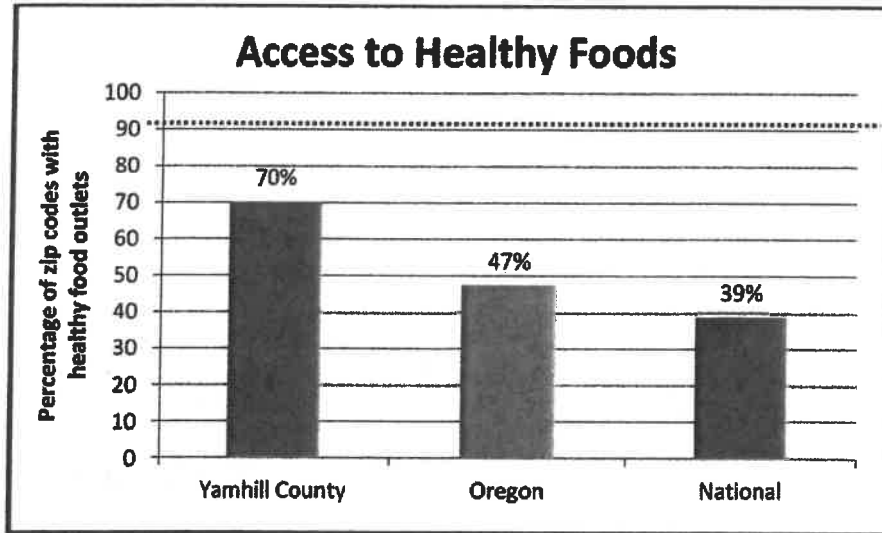


**Target:**  
2.6  
2003-2009  
County Health Rankings

Source: Behavioral Risk Factor Surveillance System, 2003-2009

# Accessibility

Source: Community Health Assessment  
Yamhill County, Oregon  
2013

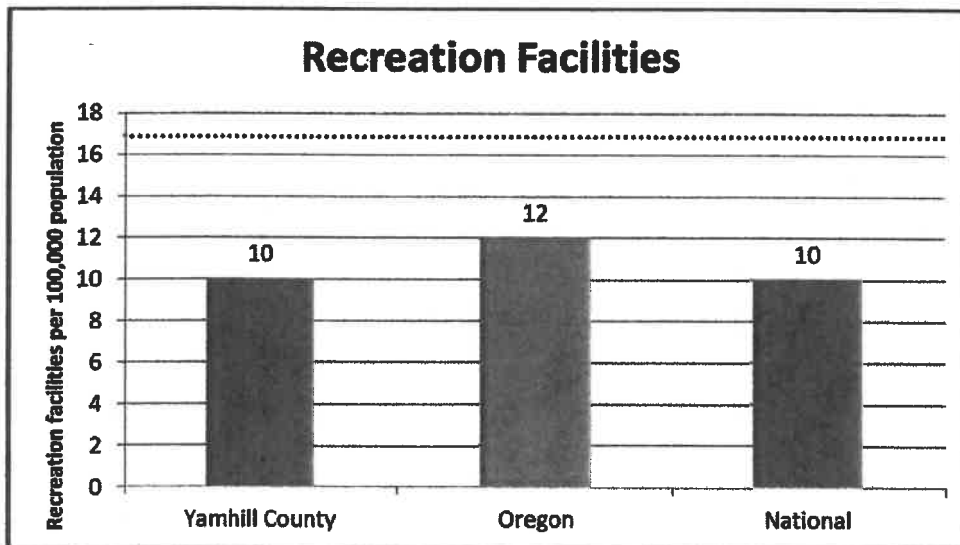


**Target:**  
**92%**  
2008  
County Health Rankings

A link has been established between the consumption of healthy food and overall health outcomes.

Source: Health Indicators Warehouse, 2006

The availability of recreational facilities can influence individuals' and communities' choices to engage in physical activity. Proximity to places with recreational opportunities is associated with higher physical activity levels, which in turn is associated with lower rates of adverse health outcomes associated with poor diet, lack of physical activity, and obesity.



**Target:**  
**17**  
2008  
County Health Rankings

Source: County Business Patterns, 2008

# COMMUNITY SURVEY

## Yamhelas Westsider Rail-Trail

### 1. Welcome!

Welcome to the Yamhelas Westsider Rail-Trail Questionnaire!

Friends of the Yamhelas Westsider trail are conducting this community survey to better understand the needs and desires of the community. We'd like you to take about 5 to 10 minutes to answer some questions that will help the people who are planning the Yamhelas Westsider Trail. Your answers will help us create the best possible trail and give us an idea of the opportunities and concerns that you might have. We will try our best to use your input in designing the project.

We know your time is important and we appreciate you taking some of it to help guide us in developing the Yamhelas Westsider Trail.



### 2. What is the Yamhelas Westsider Trail?

We'd like to give you some important information before we ask you any questions so that you are better informed.

The Yamhelas Westsider Trail is the non-motorized recreational trail to be built in phases along the abandoned Union Pacific Railroad right of way starting south of Carlton and continuing North through Gaston. This multi-use trail will be handicap access open to hikers, bicyclists and equestrians. The trail will be built in phases and Funding has been made for the first phase. This survey is designed to gain information for comprehensive planning so the trails completion best matches the vision and expectations of all who will be using and or living near the trail.

The Yamhelas Westsider Trail will convert the historic Westsider rail line into a 17-mile multi-use recreational trail running from just north of Highway 99W at Gun Club Road to just north of Gaston. It will link up with the State Highway Bicycle Trail to Forest Grove and Henry Hagg Lake. The railway grade is gradual and will provide recreational opportunities for walkers, hikers, runners, bicyclist, and horseback riders. Access is readily available from the county roads it crosses and nearby Highway 47. The route has magnificent vistas of the Coastal Range and passes many Century Farms. The cities of Carlton, Yamhill and Gaston, situated along the trail, will provide opportunities for shopping, dining and wine tasting.

Yamhelas, comes from the name of the Native Americans who lived in this area. Westsider, was the nickname of the rail line when it was constructed in the 1870's along the west side of the Willamette Valley. Yamhelas Westsider Trail, was chosen to reflect the heritage and embody the new use for this historic corridor.

### 3. How Did This Effort Get Started?

## Yamhelas Westsider Rail-Trail

The Friends of the Yamhelas Westsider Trail started from a chance meeting of Lyell Gardner And Ken Wright on the Main street of Carlton in the spring of 2012. Lyell threw out the idea of converting the abandon railroad to a trail suggesting that it be organized under the 501c (3) of Northwest Oregon Resource Conservation & Development Council, Inc., an organization Lyell directs. The idea took flight from there gaining community, local and state government support. Funding has come from private community contributions. a\$1.4 million grant through ODOT with a 10% match from Yamhill County. Future funding is seen coming from more grants, private individual contributions/donations and pro bono contributions. Current and future funds will be used to help Yamhill County complete the purchase of the remainder of the abandoned Right of Way from Union Pacific, trail development, and promote community involvement, awareness and support.



### 4. Who Is Involved In The Planning & How Long Will It Take?



## Yamhelas Westsider Rail-Trail

The Friends of the Yamhelas Westsider Trail are leading the planning and development of the trail. The Friends are actively working with Yamhill County, City of Carlton,...

The Friends and Yamhill County recently applied for a technical assistance grant from the National Park Service's Rivers, Trails and Conservation Assistance Program ([www.nps.gov/rtca](http://www.nps.gov/rtca)) to help the Friends and partners go through a public collaborative planning process for the trail.

The process began with an ongoing inventory of the publicly owned rail corridor that will determine the width of corridor, etc. Friends of Yamhelas Westsider are conducting this community survey to better understand the needs and desires of the community.

The project team consists of the Friends of the Yamhelas Westsider Trail, Yamhill County, City of Carlton and ??? will analyze the data gathered from the inventory, the community input, and then develop the trail plan accordingly. In order to allow for a comprehensive inventory and for the public to provide input to the project, the process will probably take between 12 and 20 months before a trail plan has been completed. After this time, monies through grants and private donations will be secured to construct the trail.

### 5. Will Private Property Rights Be Infringed Upon?

NO. Only land that is publicly owned or that has a public easement across it will be utilized for the trail corridor.

### 6. Who Will Develop The Trail & Maintain It?

Options for construction and maintenance will be analyzed in the master planning process. One of these is the creation of a public/private partnership to be responsible for the trail maintenance. Monies for construction will most likely come through state, federal and private funding sources (grants).

### 7. A Little About You

We'd like to know a little about you. This will help us better understand who is likely to be using the trail and for what purpose. Don't worry, we won't be asking any personal questions.

## Yamhelas Westsider Rail-Trail

### \*1. Where are you from?

- ☐ Yamhill
- ☐ Carlton
- ☐ Gaston
- ☐ McMinnville
- ☐ Forest Grove
- ☐ Lafayette
- ☐ Dundee
- ☐ Willamina
- ☐ Dayton
- ☐ Newberg
- ☐ Cornelius
- ☐ Hillsboro
- ☐ Portland
- ☐ Unincorporated Yamhill County

Other (please specify)

### 2. How old are you?

- ☐ 17 or under
- ☐ 18-25
- ☐ 26-35
- ☐ 36-45
- ☐ 46-55
- ☐ 56-65
- ☐ 66-80
- ☐ 81 or older

## 8. Use of Trail

The next questions are about how you envision using the Yamhelas Westsider Trail

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Page 4

## Yamhelas Westsider Rail-Trail

### 1. What type of activities would you or your family like to do on the Yamhelas Westsider Trail? Mark as many responses as apply

- ☐ Walk/Hike for Fun/Exercise
- ☐ Walk/Hike/Bike to Commute/Go To School/Run Errands
- ☐ Jog/Run
- ☐ Bike
- ☐ Rollerblade/Roller Skate
- ☐ Skate Board/Scooter (non-motorized)
- ☐ Sight-see
- ☐ View Wildlife/Nature
- ☐ Get Exercise
- ☐ School Field Trips/Environmental Education
- ☐ Pet Walking
- ☐ Ride Horses

Other (please specify)

### 2. Which activity would you do the most on the trail (preferred means of using the trail)? You can select only one.

- ☐ Walk/Hike for Fun/Exercise
- ☐ Walk/Hike/Bike to Commute/Go To School/Run Errands
- ☐ Jog/Run
- ☐ Bike for Fun/Exercise
- ☐ Rollerblade/Roller Skate
- ☐ Skate Board/Scooter (non-motorized)
- ☐ Sight-see
- ☐ View Wildlife/Nature
- ☐ Get Exercise
- ☐ School Field Trips/Environmental Education
- ☐ Pet Walking
- ☐ Ride Horses

Other (please specify)

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Page 5

## Yamhelas Westsider Rail-Trail

### 3. Are there any activities you think should not be allowed on the walkway? Mark as many as apply

- ☐ Walkers/Hikers
- ☐ Joggers/Runners
- ☐ Bikers
- ☐ Rollerbladers/Roller Skaters
- ☐ Skate Boarders/Scooters (non-motorized)
- ☐ Pet Walkers
- ☐ Equestrians
- ☐ No - all activities are fine
- ☐ Don't know/care

Other (please specify)

### 4. How often would you envision using the trail? Check the answer that best applies

- ☐ Never
- ☐ Once or twice a year
- ☐ Once a month
- ☐ Once a week
- ☐ 2-3 times a week
- ☐ Daily

### 5. How do you feel about the Yamhelas Westsider Trail project?

- ☐ Very Excited - Think it is a great idea.
- ☐ Excited - A good idea.
- ☐ Somewhat Excited - Have some reservations about it.
- ☐ Opposed - Don't like the idea of a trail within the publicly owned rail corridor.
- ☐ Don't Know/Don't Care

## 9. Opportunities

We'd like to find out about the potential opportunities of the trail.

## Yamhelas Westsider Rail-Trail

**1. What are the opportunities you see as the Yamhelas Westsider gets developed? Feel free to tell us about interpretive, business, educational, or any other opportunity you see when you think of a trail like this being developed. Please type in your comments below.**

### 10. Concerns

Do you see any potential challenges to developing the trail?

**1. If you have any concerns about the development of the trail or see some potential pitfalls, please take a moment to tell us about them. Please enter them by typing in the box below.**

### 11. Trail Amenities

Now we'd like to know what are the important amenities for you when using a trail like the Yamhelas Westsider Trail.

# Yamhelas Westsider Rail-Trail

## 1. Please answer how important the following trail amenities are to you.

	Very Important	Important	Somewhat Important	Not Important	Don't Know/Don't Care
Trail surface (paved, gravel, etc.)	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
ADA (Disability) accessible	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Parking at or near the trailhead	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Garbage cans	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Restrooms	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Map(s) on display	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Interpretive signs (history, natural environment, etc.)	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Water fountains	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Bike Racks	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Lighting for dusk/dark	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Benches	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Tables	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>
Equestrian Friendly Parking and Trailheads/Access	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input type="radio"/>

Other (please specify)

## 2. Which type of trail surface would your prefer?

- ☐ Paved
- ☐ Gravel/crushed rock
- ☐ Dual surface - paved trail with a 3-4 foot parallel dirt track for runners & horses
- ☐ Don't know/Don't Care

Other (please specify)

## 12. Do you own a business near or along the trail corridor?

### \*1. Do you own a business near or along the proposed Yamhelas Westsider Trail's corridor?

- ☐ Yes
- ☐ No
- ☐ Don't know/not sure

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## Yamhelas Westsider Rail-Trail

### 13. Opportunities, Impacts, or Concerns about the Trail from a business point of view...

**1. Please tell us if you see any opportunities, impacts, or concerns to your business if the Yamhelas Westsider Trail is developed. Feel free to tell us anything you'd like about how the trail, trail's website, signs, etc. could be developed to better support your business.**

### 14. Do you live or own property along the trail?

**1. Do you live or own property near or adjacent to the proposed Westsider Yamhill Trail's corridor?**

- ☐ Yes
- ☐ No
- ☐ Don't know/not sure

### 15. Property Owners

**\*1. Do you live immediately adjacent to the proposed trail corridor?**

- ☐ Yes - I live immediately adjacent to the trail's corridor
- ☐ Sort of - I live within sight of the trail's corridor
- ☐ No - not close at all.
- ☐ Don't know/not sure

### 16.

**1. Tell us about opportunities you see with the trail development. Let us know if you have any questions or concerns about the trail. Are there any design measures that we could work into the trail plan that would help your concerns such as signage, visual screening (plantings, fences, etc.)? Tell us anything else you would like about the trail and how it might impact your property.**

### 17. Would You Like To Be A Supporter For The Trail

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Page 9

## Yamhelas Westsider Rail-Trail

We are wondering if you would like to be a supporter of the trail.

**1. If you would like to become a supporter of the Yamhelas Westsider Trail, please provide your contact information below. We would prefer to contact you via email but we recognize that you might prefer a different form. If so, provide your phone or mailing address.**

**We will use this distribution list to provide you with information about the trail planning effort and other trail development activities.**

Name:	<input type="text"/>
Address:	<input type="text"/>
Address 2:	<input type="text"/>
City/Town:	<input type="text"/>
State:	<input type="text"/>
ZIP:	<input type="text"/>
Email Address:	<input type="text"/>
Phone Number:	<input type="text"/>

## 18. Thank You!

Thank you so much for taking the time to provide input to the Yamhelas Westsider Trail effort. We truly appreciate your responses which will help to guide us in our efforts.

Again, thank you.

If you have further questions about the Yamhelas Westsider Trail, you can go to [www.yamhelaswestsidertrail.com](http://www.yamhelaswestsidertrail.com) or contact Brett Henry with Yamhill County Parks. Email [henryb@co.yamhill.or.us](mailto:henryb@co.yamhill.or.us) or call 503-434-7463





# Community Survey Results

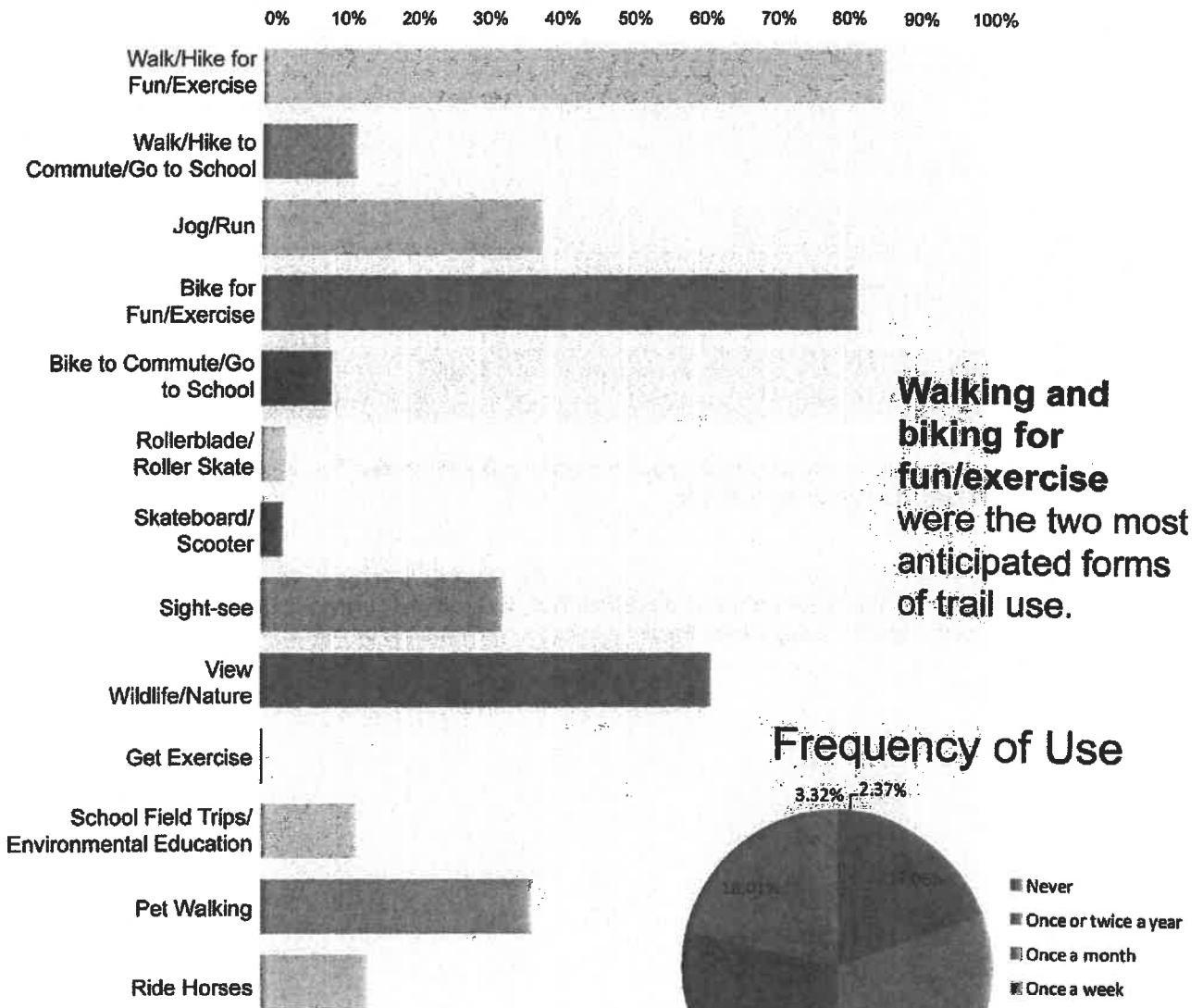
Yamhelas-Westsider Trail

February 2015 - October 2015

Over 230 surveys were collected from local residents who told us how they feel about the proposed Yamhelas-Westsider Trail and what they think it should look like. Thank you to all those who completed the survey!

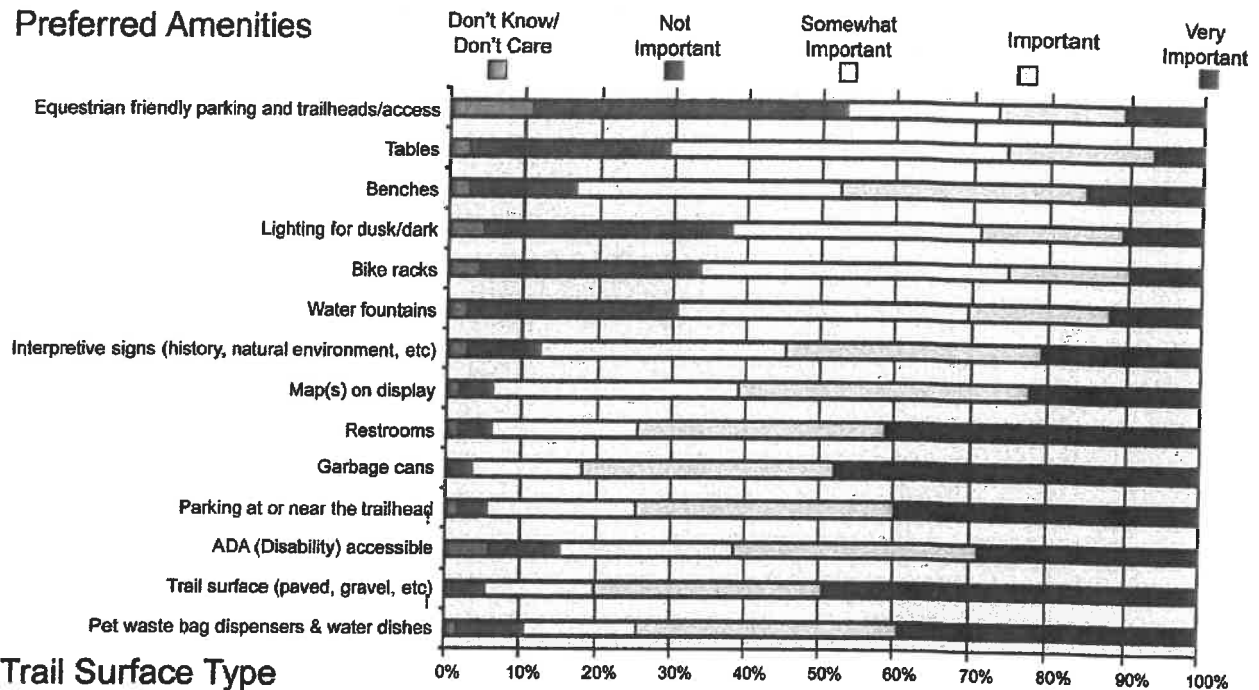
If you have any questions, contact Wayne Wiebke at 503-435-9120.

## Trail Activities

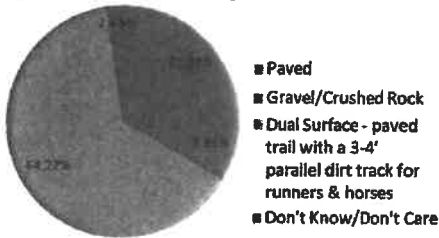


Most people said they planned to use the trail **once a month or once a week.**

## Preferred Amenities



## Trail Surface Type

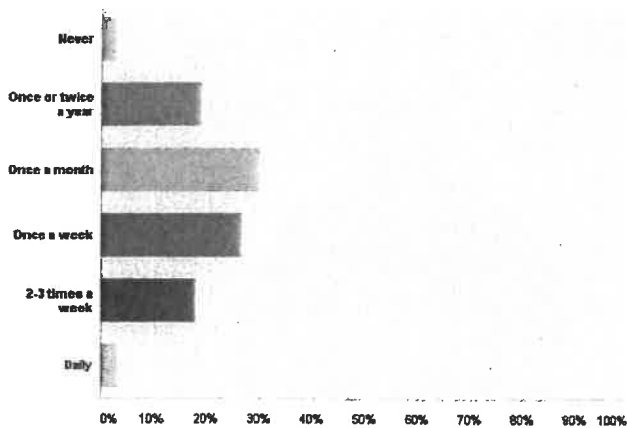


Some type of **trail surface** and a means to keep the area clean with **restrooms & garbage cans** are the most desired amenities. There's also interest in specified parking areas at **trailheads**.

Most people preferred a **Dual Surface paved & dirt trail**.

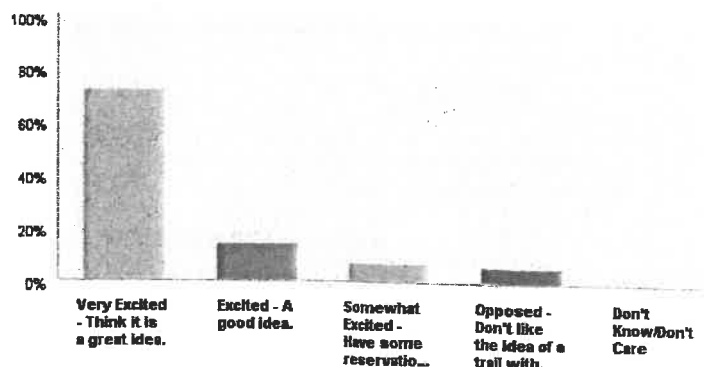
## Q6 How often would you envision using the trail? Check the answer that best applies.

Answered: 267 Skipped: 28



## Q7 How do you feel about the Yamhelas Westsider Trail project?

Answered: 276 Skipped: 19



# Trails and Agriculture: Bridging Productive and Recreational Landscapes

April 23, 2015

Trails and Agriculture: Bridging Productive and Recreational Landscapes



This is a black and white aerial photograph of a region in California. A prominent road, likely Highway 101, runs vertically through the center. Other roads are labeled with numbers like 1, 23, 150, and 101. Several towns are marked with dots and labels: Ventura, Santa Paula, Elliptore, Moorpark, and Simi Valley. The terrain appears hilly and forested.

# Origins of Study

- Find ways that existing trails have maximized compatibility with agricultural operations
- Identify methods to guide trail planning that protect the interests of both farmers and trail users

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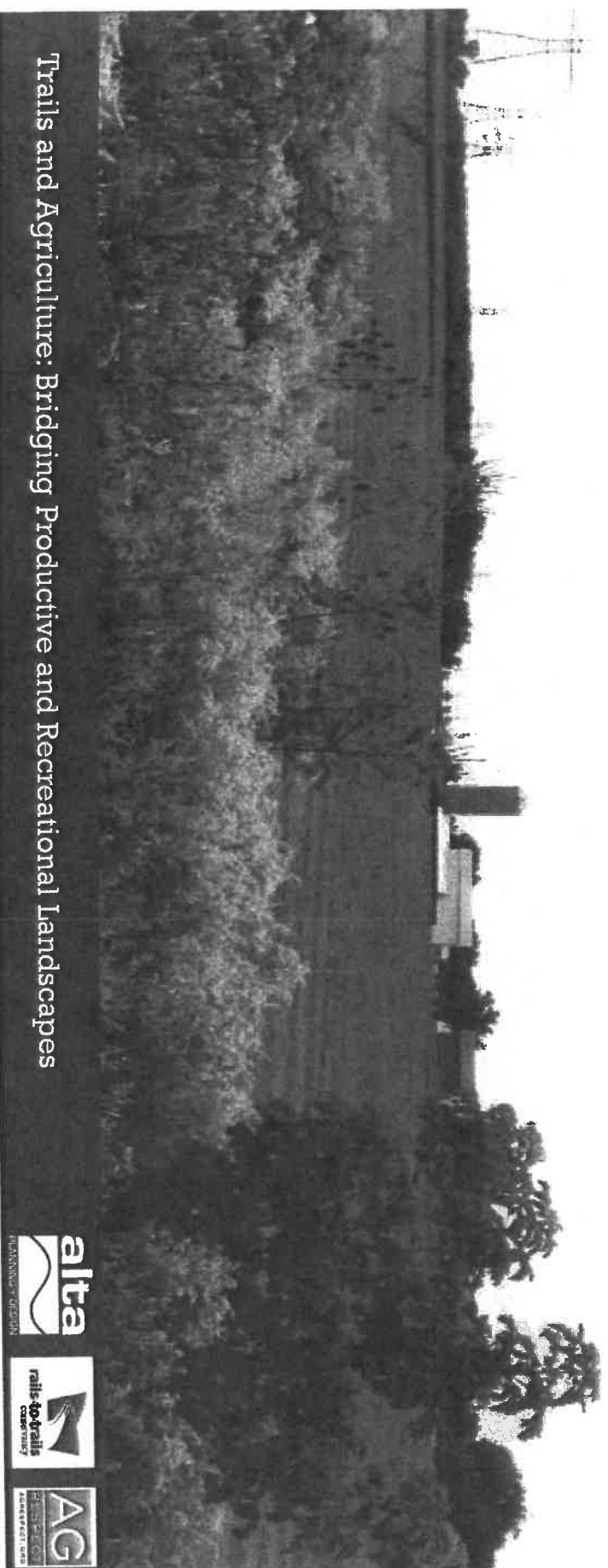
Trails and Agriculture: Bridging Productive and Recreational Landscapes



# Process

- National inventory of existing trails in agricultural settings
- Review of relevant literature and programs
- Interviews with farm and trail operators

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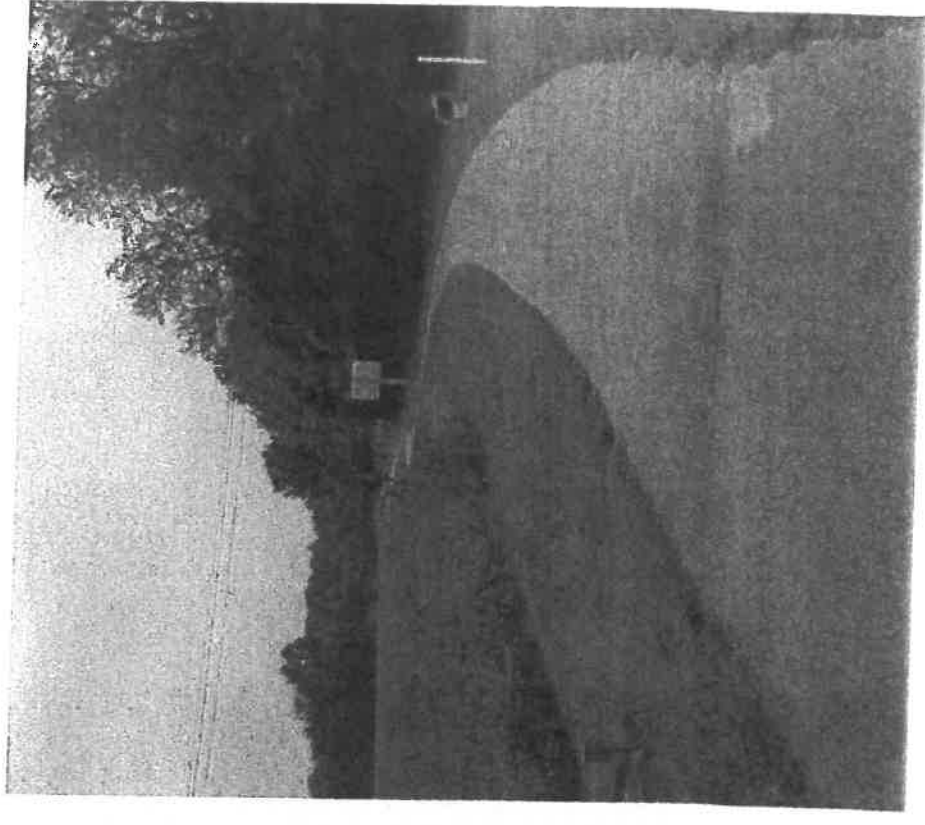
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# Trail Selection Criteria

- Pass through or adjacent to active agricultural areas
- Near high-value crops and orchards
- Paved surface and/or high levels of use
- Existing or historical rail use
- Priority given to projects in California
- Detailed information available from trail and agricultural operators



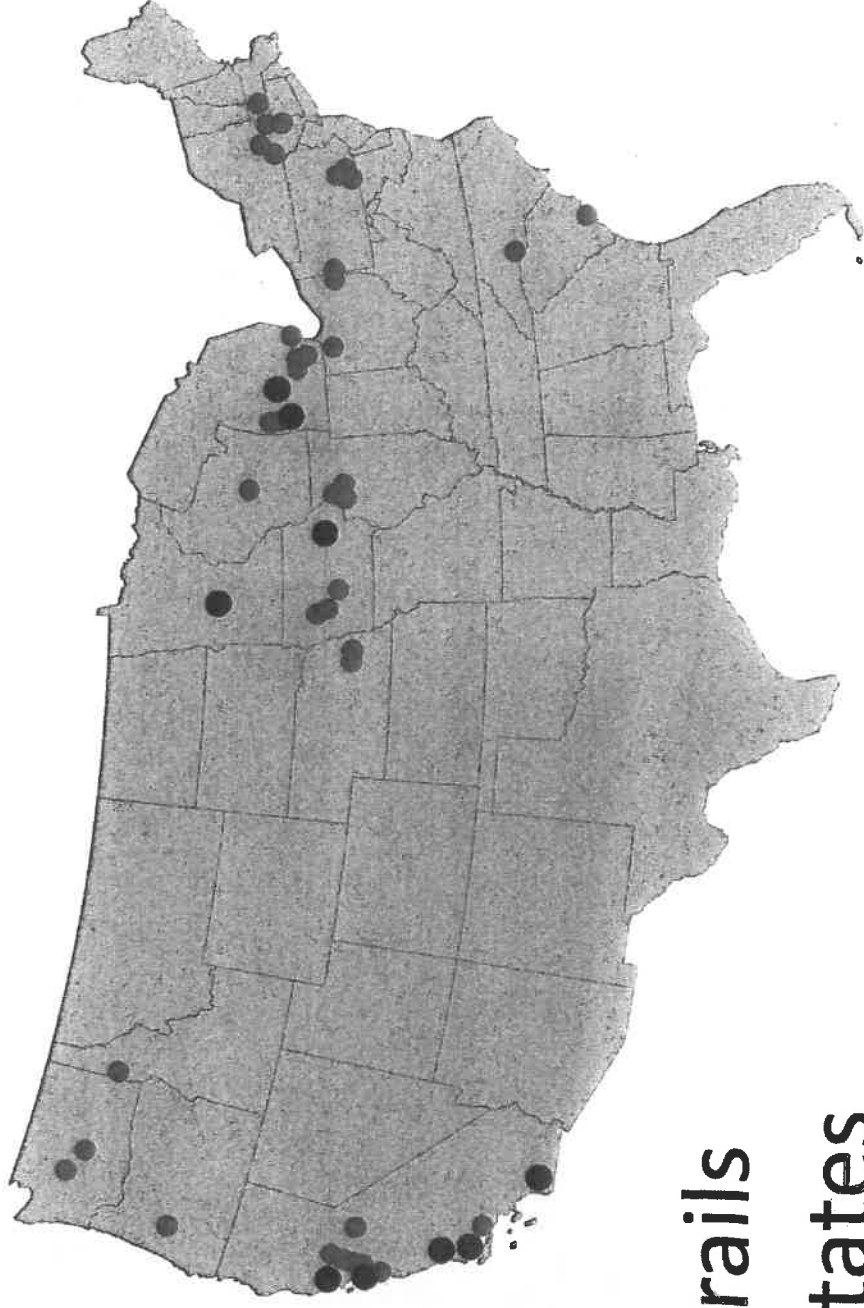
# National Trail Inventory

37 trails  
16 states





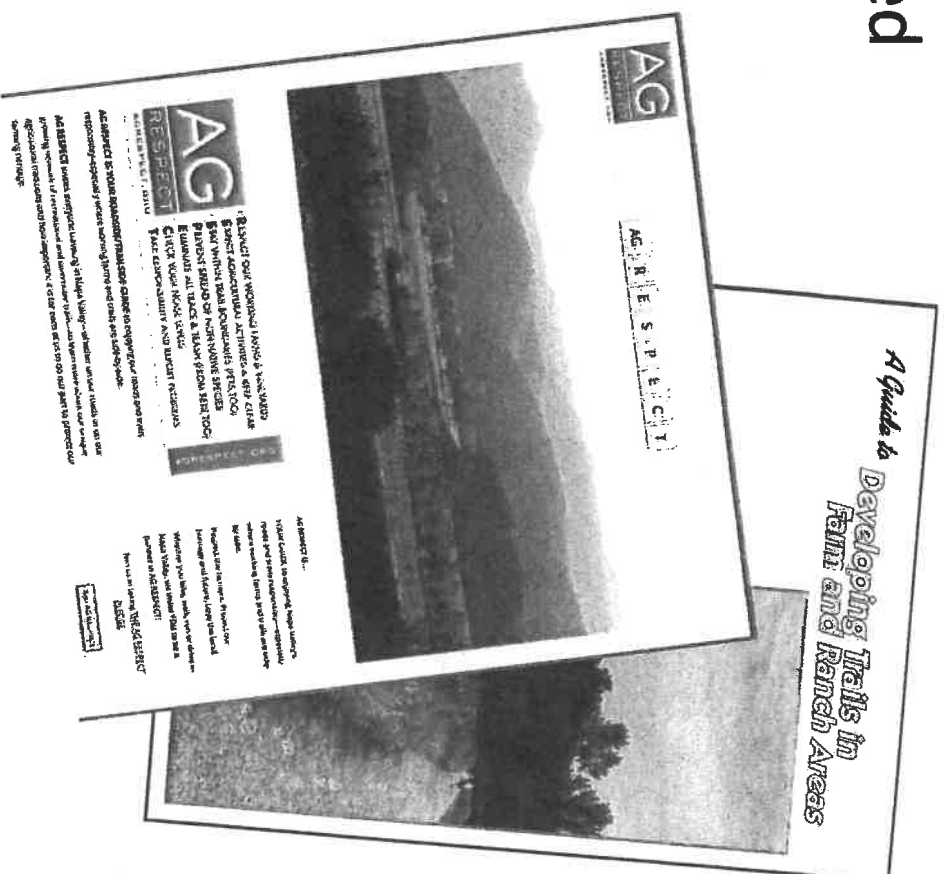
# Trails Selected for In-Depth Study



9 trails  
4 states

# Literature/Program Review

- Most literature related to agritourism
- Napa Valley's AG RESPECT program
- British Columbia's *Trails through Agriculture Areas*



# Expressed Concerns

- Theft and vandalism
- Crop contamination and weeds
- Public safety
- Disruption of operations
- Encroachments onto trail
- Shading crops

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Trails and Agriculture: Bridging Productive and Recreational Landscapes



# Strategies for Success

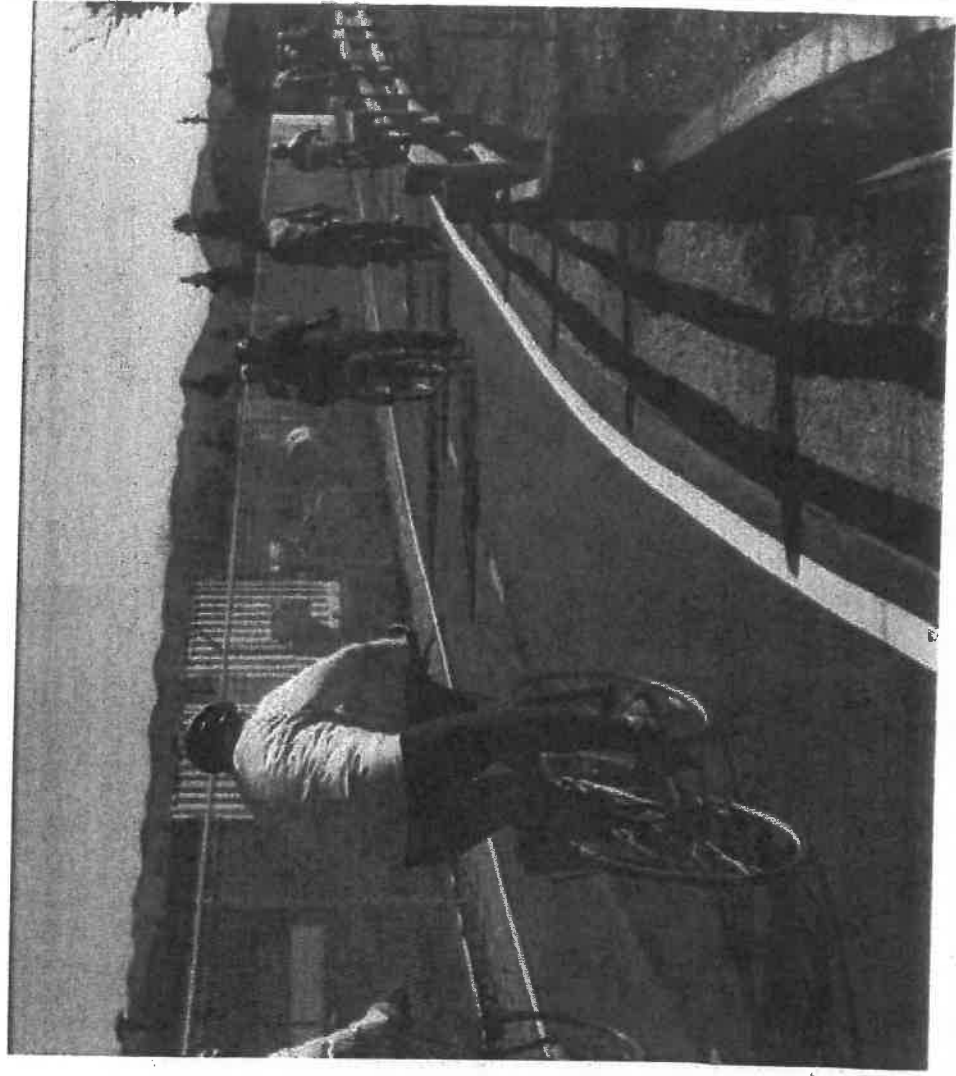
- Design Strategies
- Management Strategies
- Indemnification
- Building Consensus



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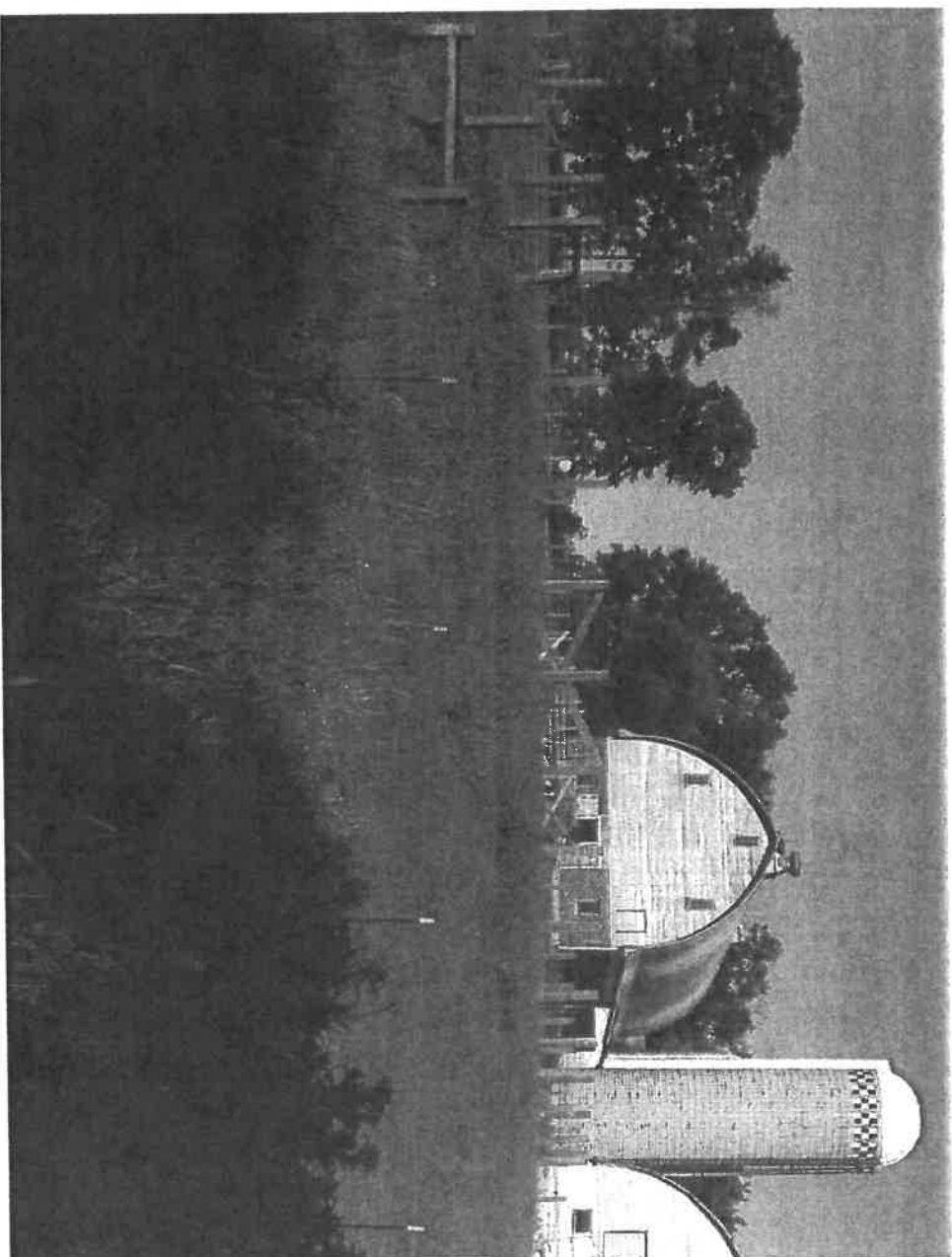
# Design Strategies

- Fencing
  - Lodge pole
  - Chain link



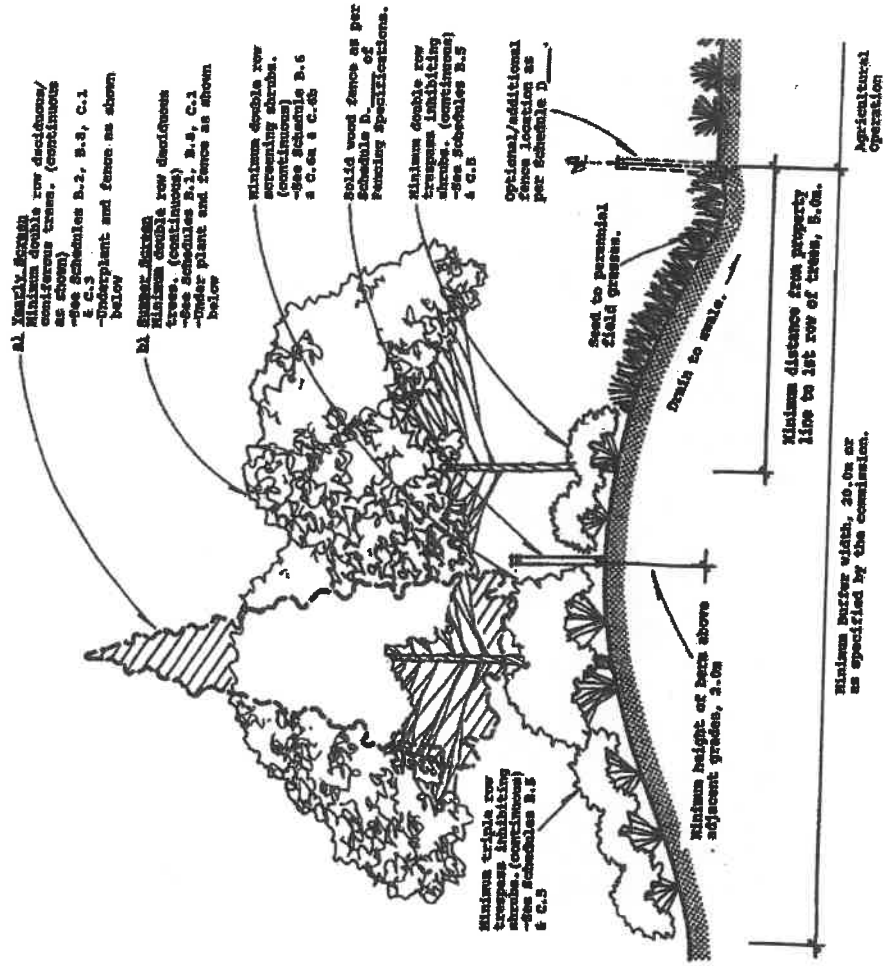
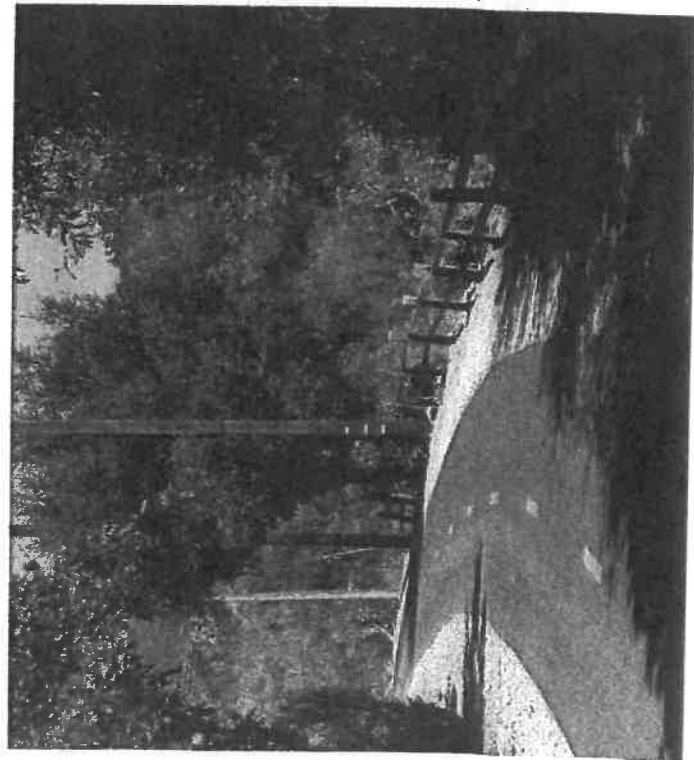
# Design Strategies

- Fencing
- Wire



# Design Strategies

- Hedges
- Buffers



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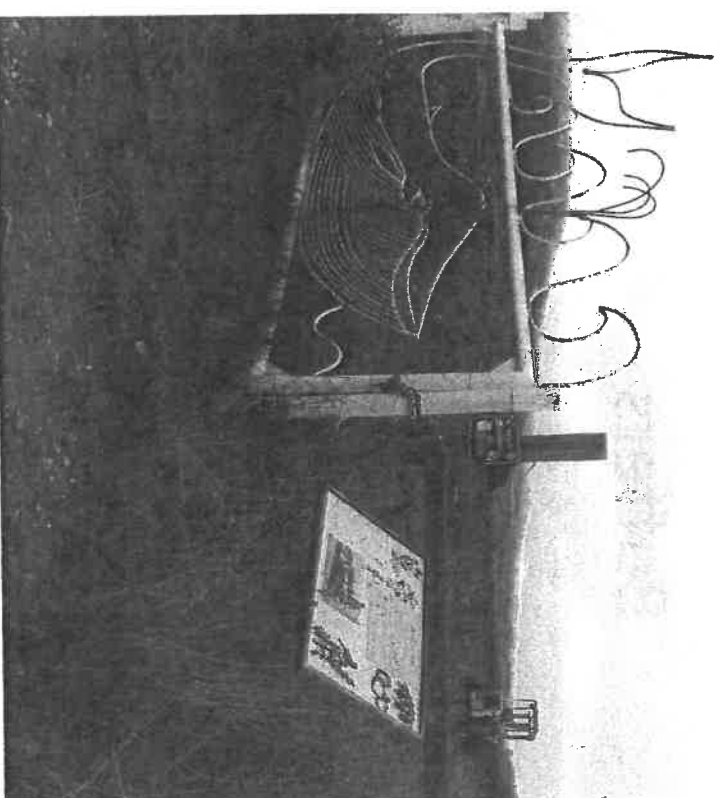


# Design Strategies

## Trail Crossings

- Design trail to minimize crossings
- Discuss trail crossings with each farmer to ensure adequate crossings are provided

*Requests from farmers are handled on an individual basis, and are almost always related to crossings.*  
- Steve Hahn, Washington State Parks





# Design Strategies

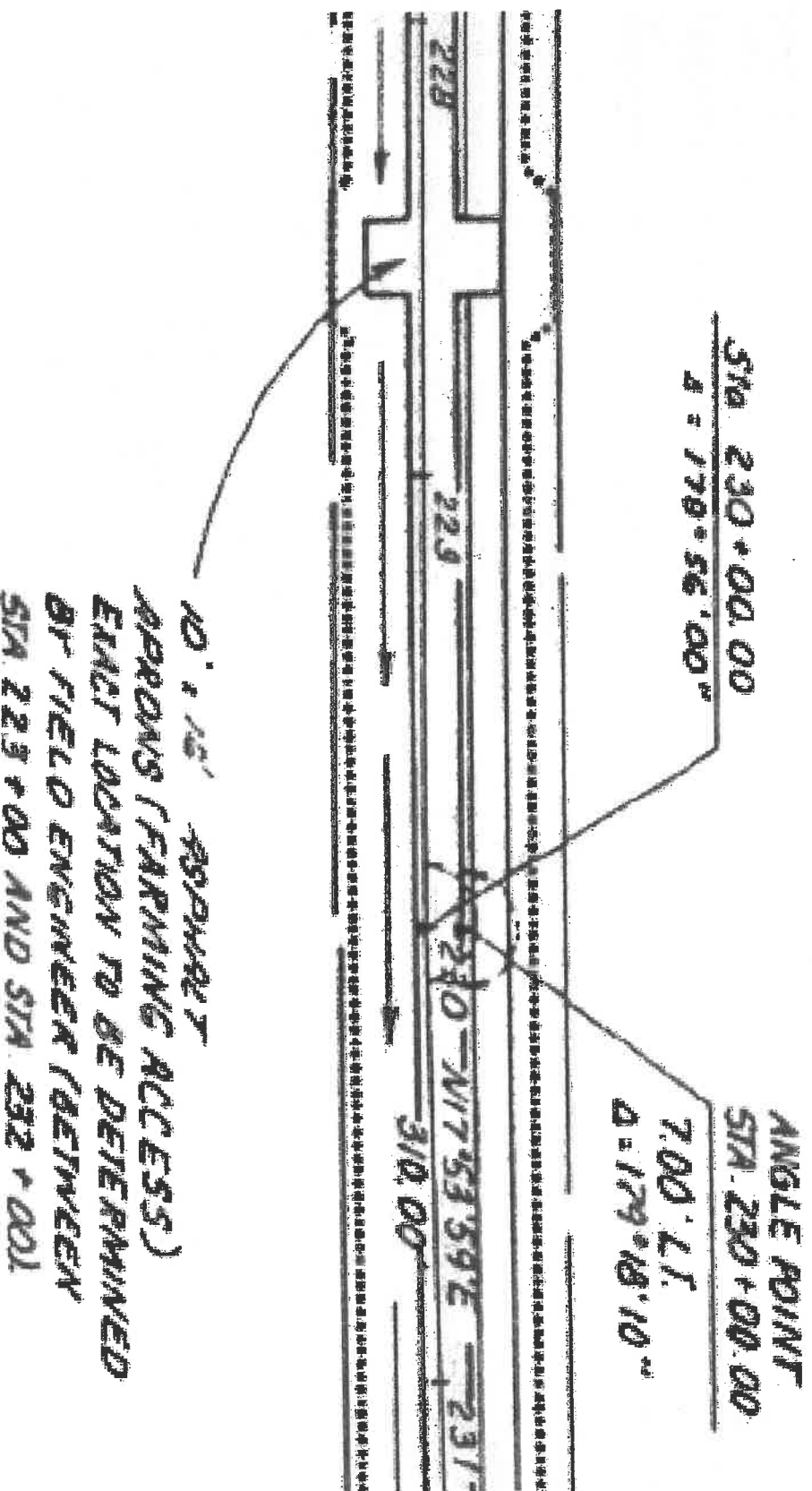
## Trail Crossings & Gates

- Where needed, add gaps in fencing and/or gates to accommodate farm equipment
- Reinforce crossings to minimize trail damage from farm equipment

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# Safe Farm Equipment Crossings

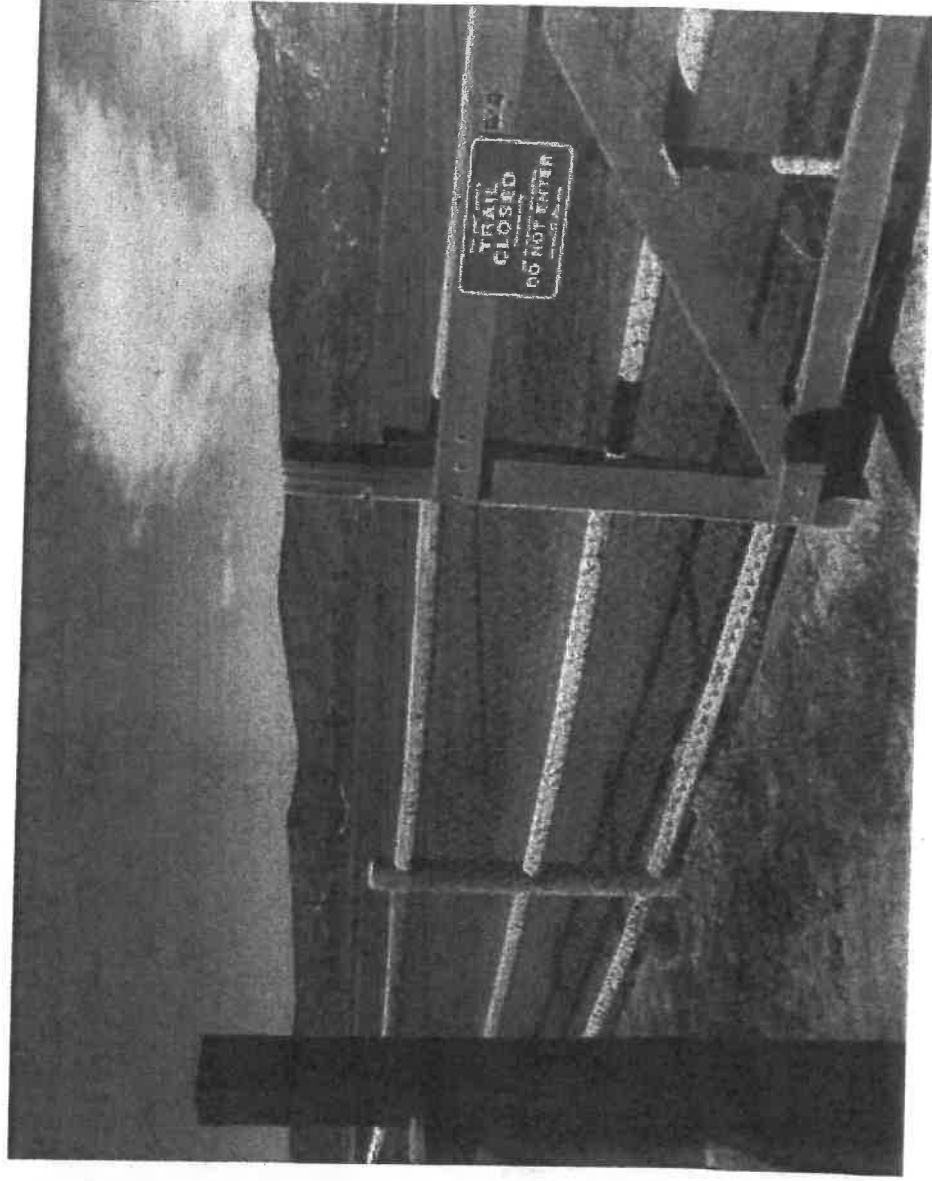


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# Design Strategies

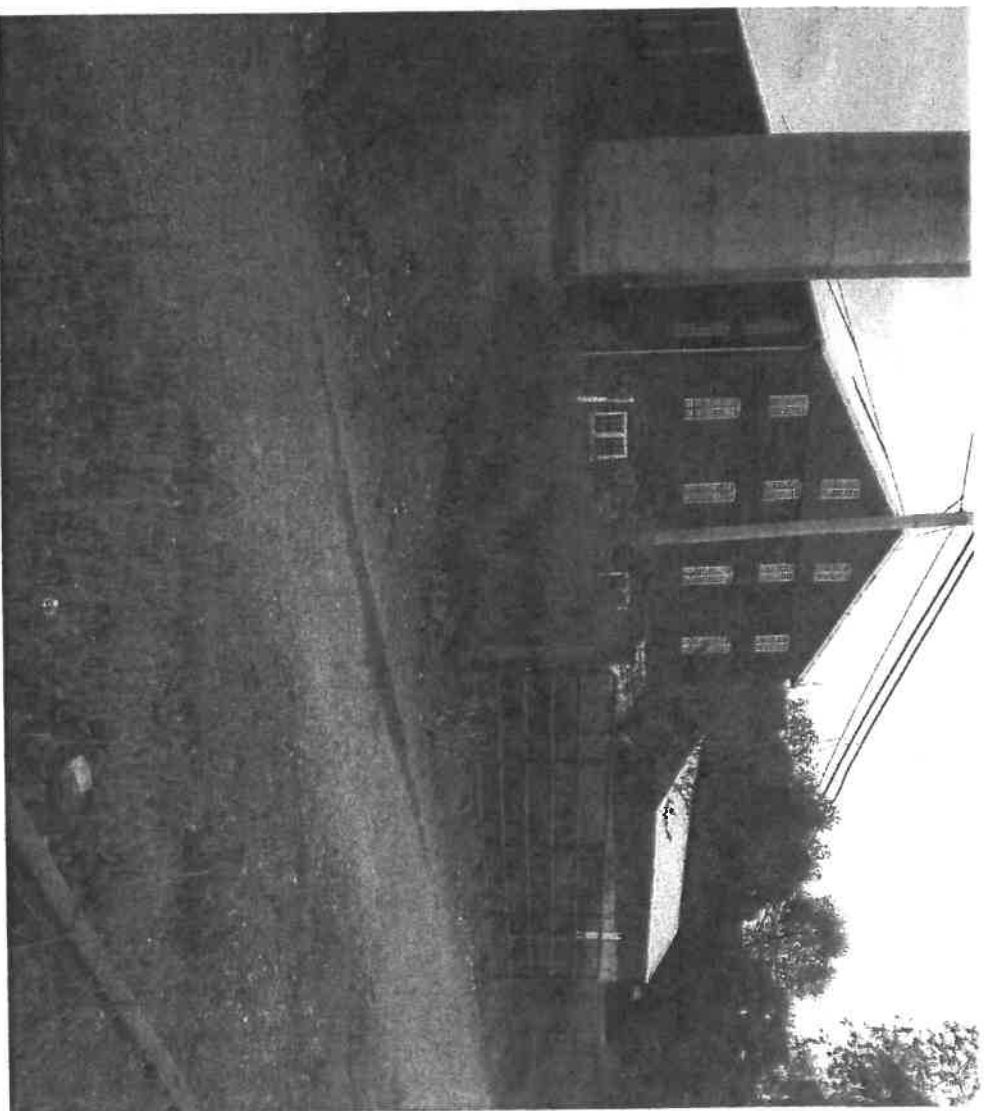
## Gates

- Allow trail closure for specific farm operations



# Crossings

- In places where the trail did not have a good base at a farm crossing it should be reinforced.
- Signs need a greater distance setback than at a normal intersection to allow wide farm equipment plenty of clearance.

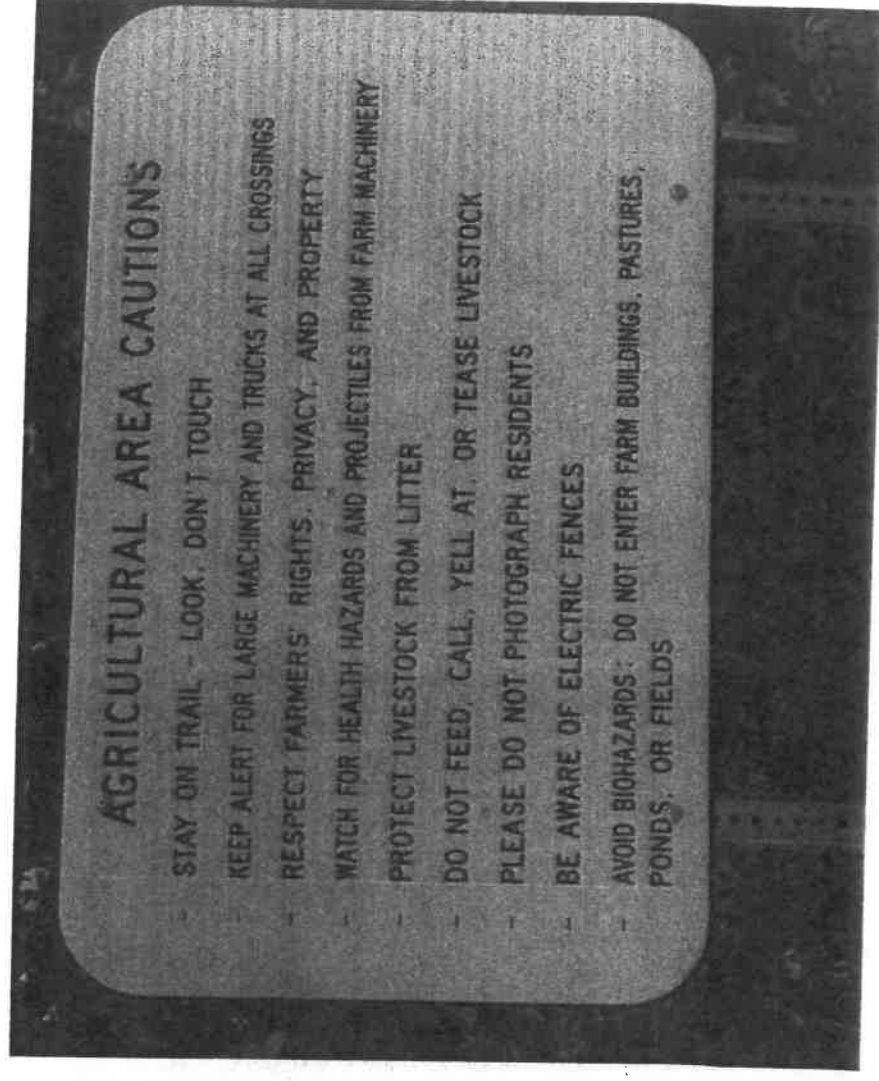


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# Design Strategies

## Signage Types

- Regulatory
  - Stay on the trail/no trespassing
- Etiquette
  - Watch for and yield to farm equipment
- Warning
  - Spraying schedules
- Informational
  - Notice of trail closures
- Wayfinding



# Design Strategies

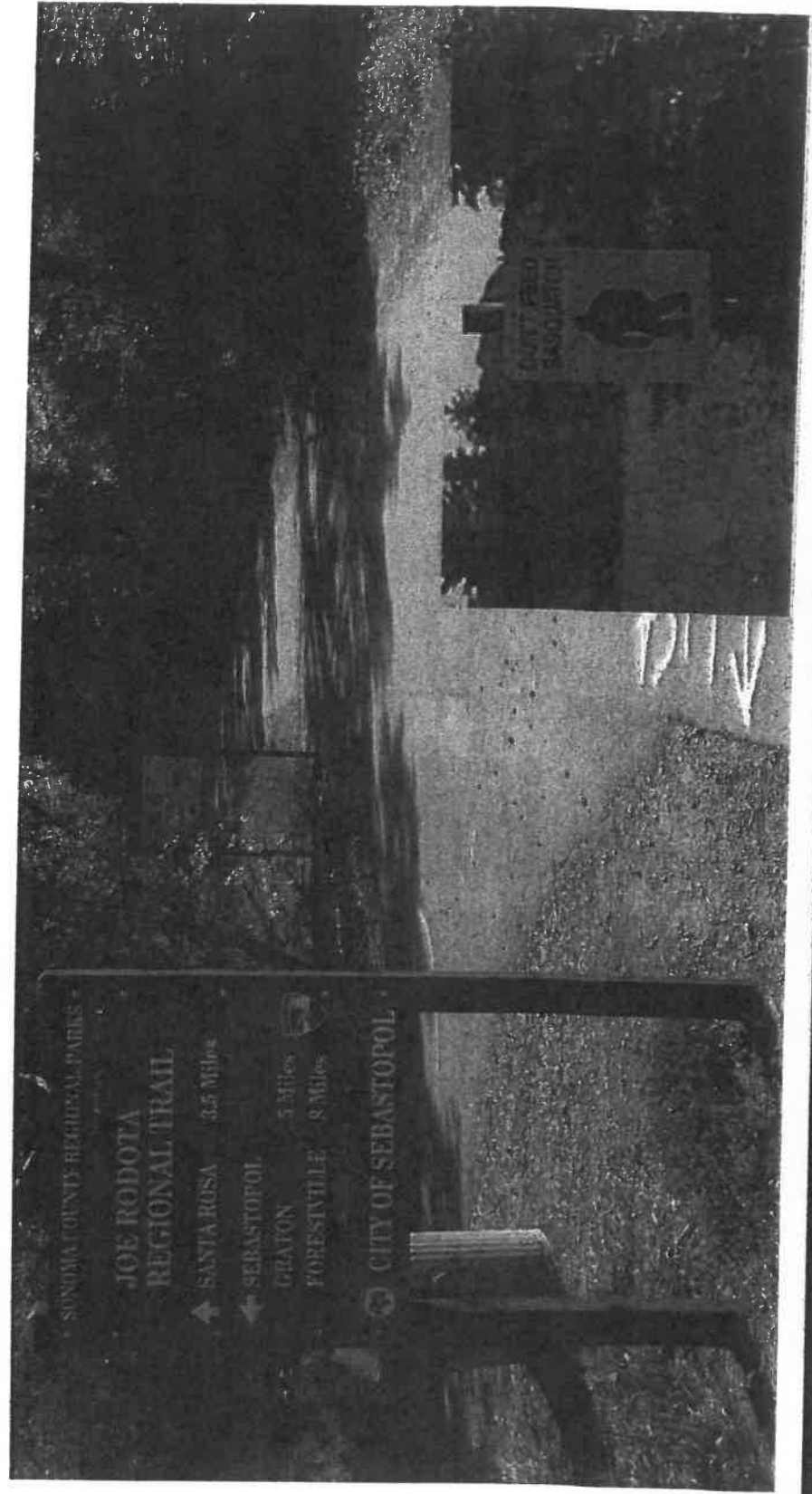


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# Design Strategies



Trails and Agriculture: Bridging Productive and Recreational Landscapes



# Design Strategies





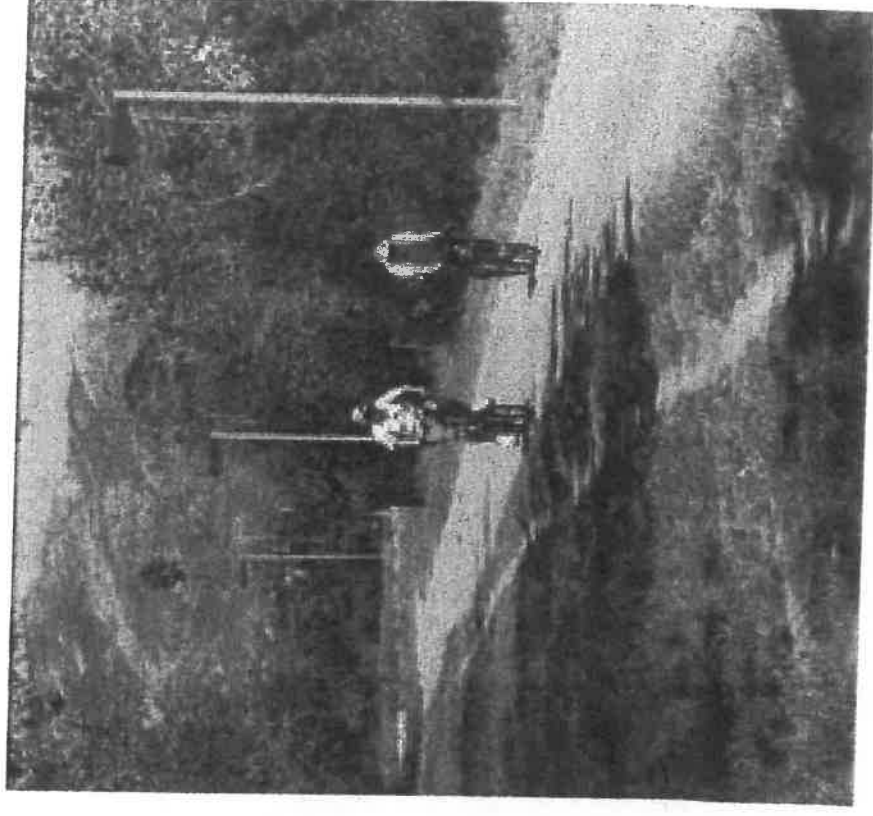
# Design Strategies

## Lighting

- Improves safety
- Allows use by commuters

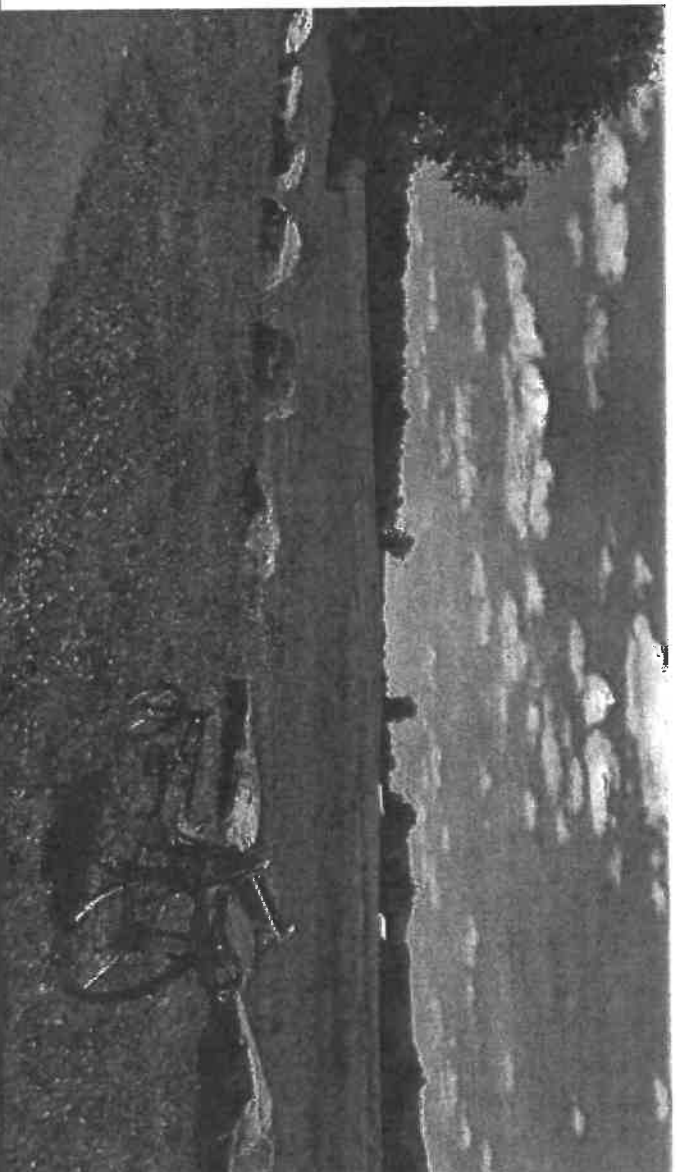
*A bike path makes theft harder, not easier. We installed lights, and it gets so much use there are always eyes on the trail.*

*- Matt Dobberteen, Santa Barbara County Public Works*



# Management Strategies

- Spray during specific hours
- Gate closures to be requested by farmers, allowing for spraying and equipment crossings



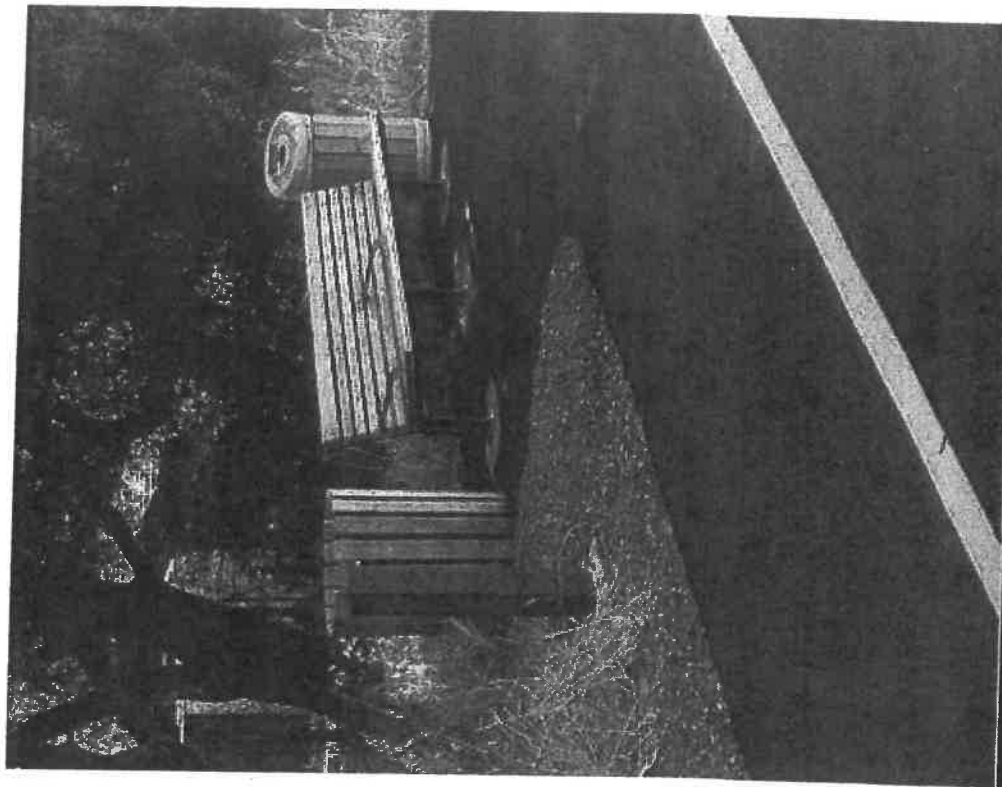
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# Management Strategies

- Regular and responsive trail maintenance
- Patrol by local authorities
- Address farmers' concerns as they arise

*If I could do it all over again, I would have pushed for more stringent trash cleanup requirements from the trail operator.*

*- Matt Witman, Citrus Farmer, San Diego, CA*



# Indemnification

Existing law protects public entities and easement grantors from legal claims

- California Government Code § 831.4
- California Civil Code § 846

Private landowners protected as long as:

- No invitation
- No fee charged
- No willful or malicious intent

Specific indemnification and insurance

- San Diego County Ordinance Number 9233
- San Diego County JPA insurance

*Indemnification was a dealbreaker – farmers and the farm bureau would have never supported the trail without it.*

*- Matt Witman, Citrus Farmer, San Diego, CA*

# What Did We Learn?

- Many projects initiated with significant community concerns
- After construction, problems few and far between
- Similar responses from both trail managers and farmers

*If someone wanted to steal from a farm, they'd find a place where no one could see them, not a trail with steady use.*

*- Matt Dobbertein, County of Santa Barbara*

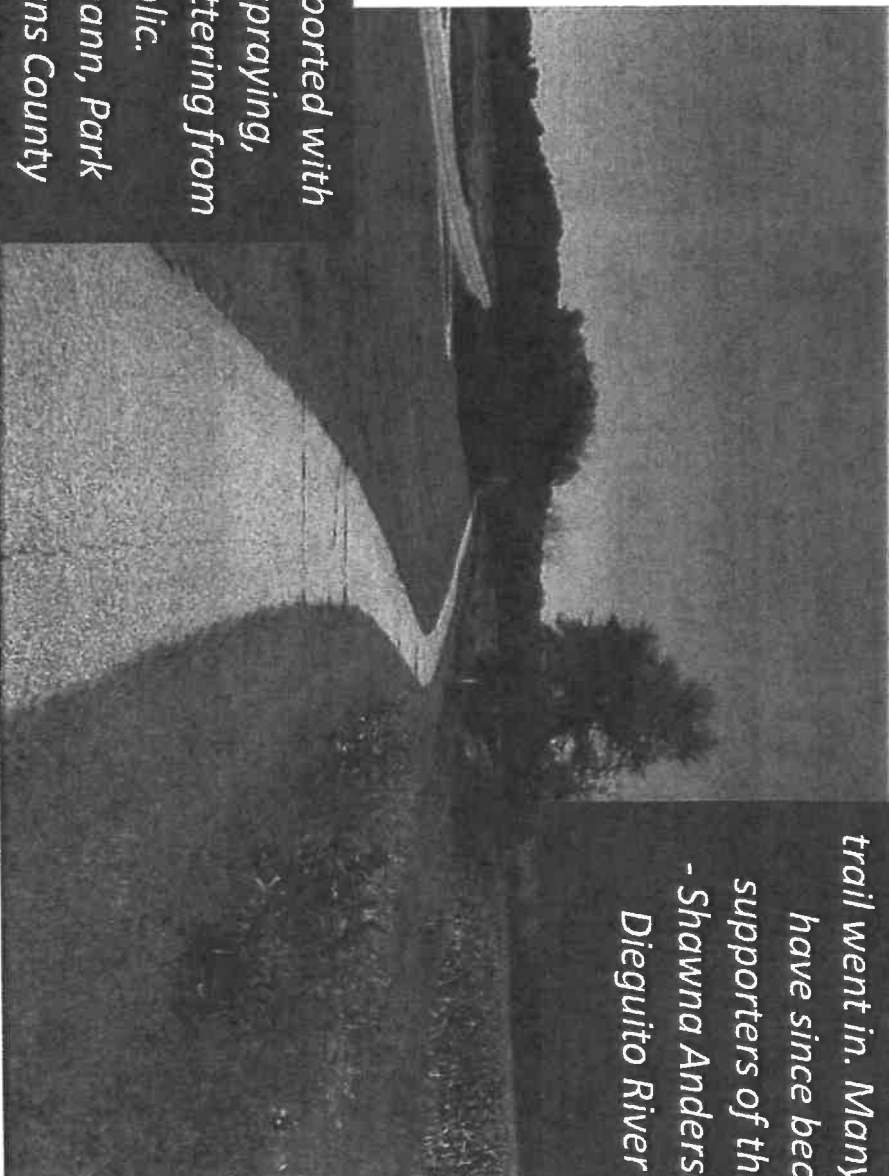
*My husband and I were initially opposed to the trail, with concern about trespassing, but this has not been an issue. And now our family uses the trail.*

*- Joyce Squires, K&J Squires Farms, Linn County, IA*

# Responses from Trail Managers

*...no complaints since the trail went in. Many farmers have since become supporters of the trail.*  
- Shawna Anderson, San Dieguito River Park

*No problems reported with agricultural spraying, trespassing or littering from the public.*  
- Peter Theismann, Park Director, Stearns County Parks, MN



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# Responses from Farmers

*[The trail] has not interfered with my operation at all, it's been a successful project.*

*- John Giusti, Farmer, San Mateo County, CA*

*I've had no problems with the trail. I walk my dog along the trail to check on my crops.*

*- Ned Welder, Farmer, Montcalm County, MI*

*We were very concerned about trespassing before the trail was built, but have had no problems with the trail or trail users. We are now trail users, it is a very positive thing for the community.*

*- Jan Pearl, Farm Owner, Montcalm County, MI*



# Infrequent Incidents

*Occasionally homeless pass through and there is minor vandalism, but not serious enough to involve the authorities. Trail users don't cut through the farm property.*

*- John Givens, Farm Operator, Santa Barbara County, CA*

*We've had 4 incidents in 20 years where the trail has been sprayed*

*by a farmer.*

*- Don Stearns, Friends of the Fred Meijer Trail, Montcalm County, MI*

*I've experienced no problems with the trail, other than a deer hunter using it on occasion.*

*- Robert Spencer, Farm Operator Montcalm County, MI*

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# Notable Incidents



*Trespassing hasn't been a major problem but one time a bicycle race took place on the trail, and one of the racers got lost and strayed onto the farm and a pack of racers followed.*

*- Matt Witman, Citrus Farmer, San Diego, CA*

# Building Consensus

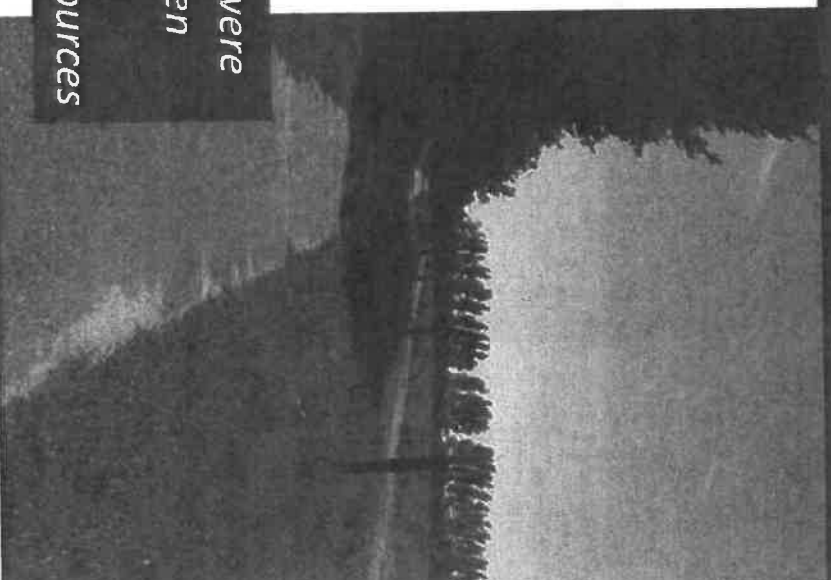
- Address individual farmers' concerns
- Keep lines of communication clear and responsive
- If a safe condition or consensus cannot be achieved, reroute the trail as necessary

*Conflicts with farmers during the planning phase were resolved early on. No conflicts or issues have been reported since.*

*- Wes Lomax, Michigan Department of Natural Resources*

*The ability to work as a team is key to success.*

*- Paul Ringgold, Peninsula Open Space Trust*



# Building Consensus (cont.)

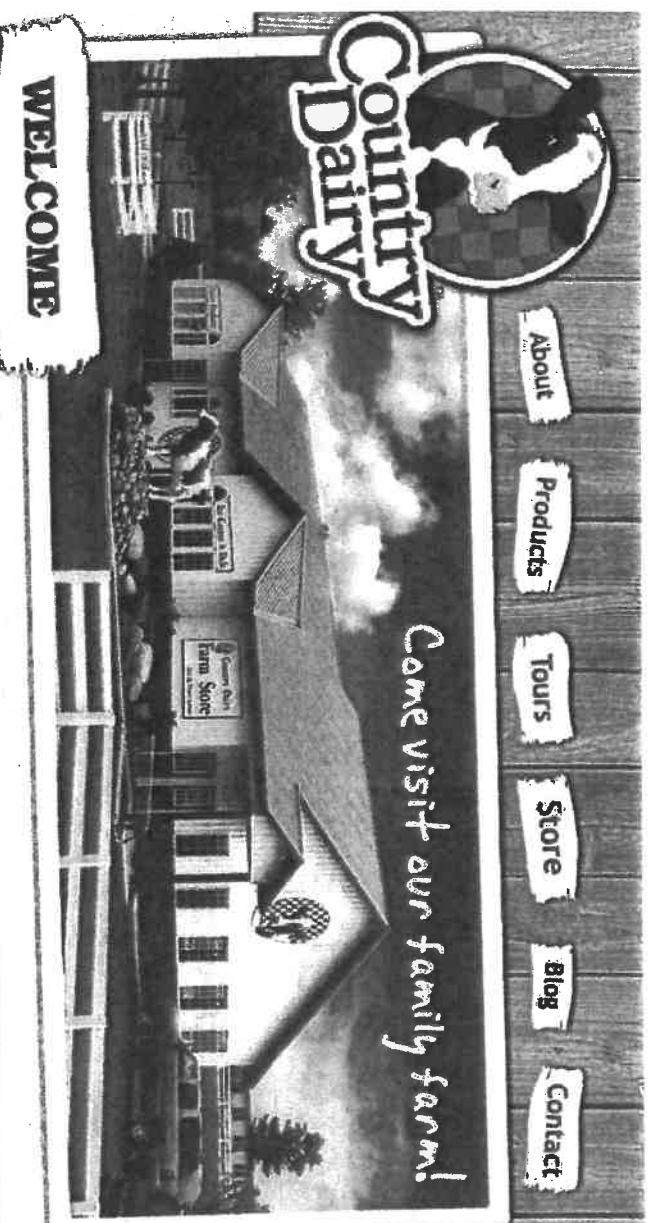
- Maintain the trail and educate trail users
- Promote trail etiquette
- Consider a wide range of design and management solutions for specific needs.



*Individual agreements were arranged with farmers and communities, formed over decades.*

*- Kevin Thusius, Director of Conservation, Ice Age Trail, WI*

# Trails Supporting the Farm Economy



*"When the trail developed, this farm only had cows next to the trail. They sold ice cream and put up a small hut on their farm next to the trail. It has grown ever since and is a destination for trail users and the public in the area." - Paul Yaak, MI DNR*

*"MI tries to work with the Agricultural community on partnership like this with farm markets and other ventures that assist locally grown goods." - Paul Yaak, MI DNR*

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# Trails Supporting the Farm Economy

- Lake Hills  
Greenbelt,  
Bellevue, WA
  - Lake Hills Farm  
Fresh Produce  
stand
  - Larsen Lake  
Blueberry Farm



# Next Steps

- Additional Research
  - How do issues vary based on types of crops and farmers' specific operational needs?
  - Larger sample size
- Design and Management Guidelines
  - Buffer materials, dimensions
  - Fencing
  - Gates/access
  - Trail crossings and closures
  - Signage

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**R**ESPECT OUR WORKING FARMS & VINEYARDS  
**E**XPECT AGRICULTURAL ACTIVITIES & KEEP CLEAR  
**S**TAY WITHIN TRAIL BOUNDARIES (PETS, TOO)  
**P**REVENT SPREAD OF NON-NATIVE SPECIES  
**E**ELIMINATE ALL TRACE & TRASH (FROM PETS, TOO)  
**C**HECK YOUR NOISE LEVELS  
**T**AKE RESPONSIBILITY AND REPORT PROBLEMS

AGRRESPECT.ORG

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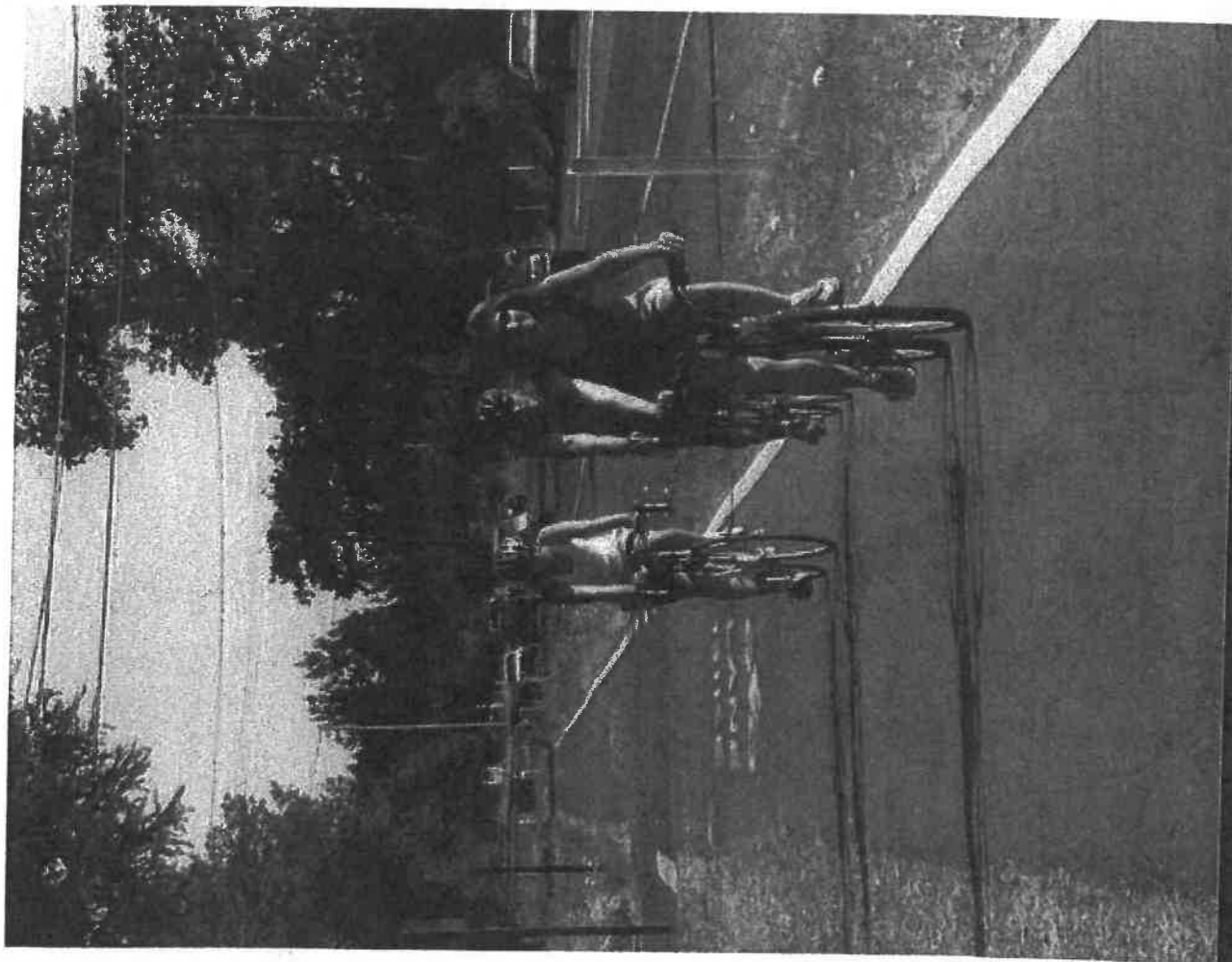




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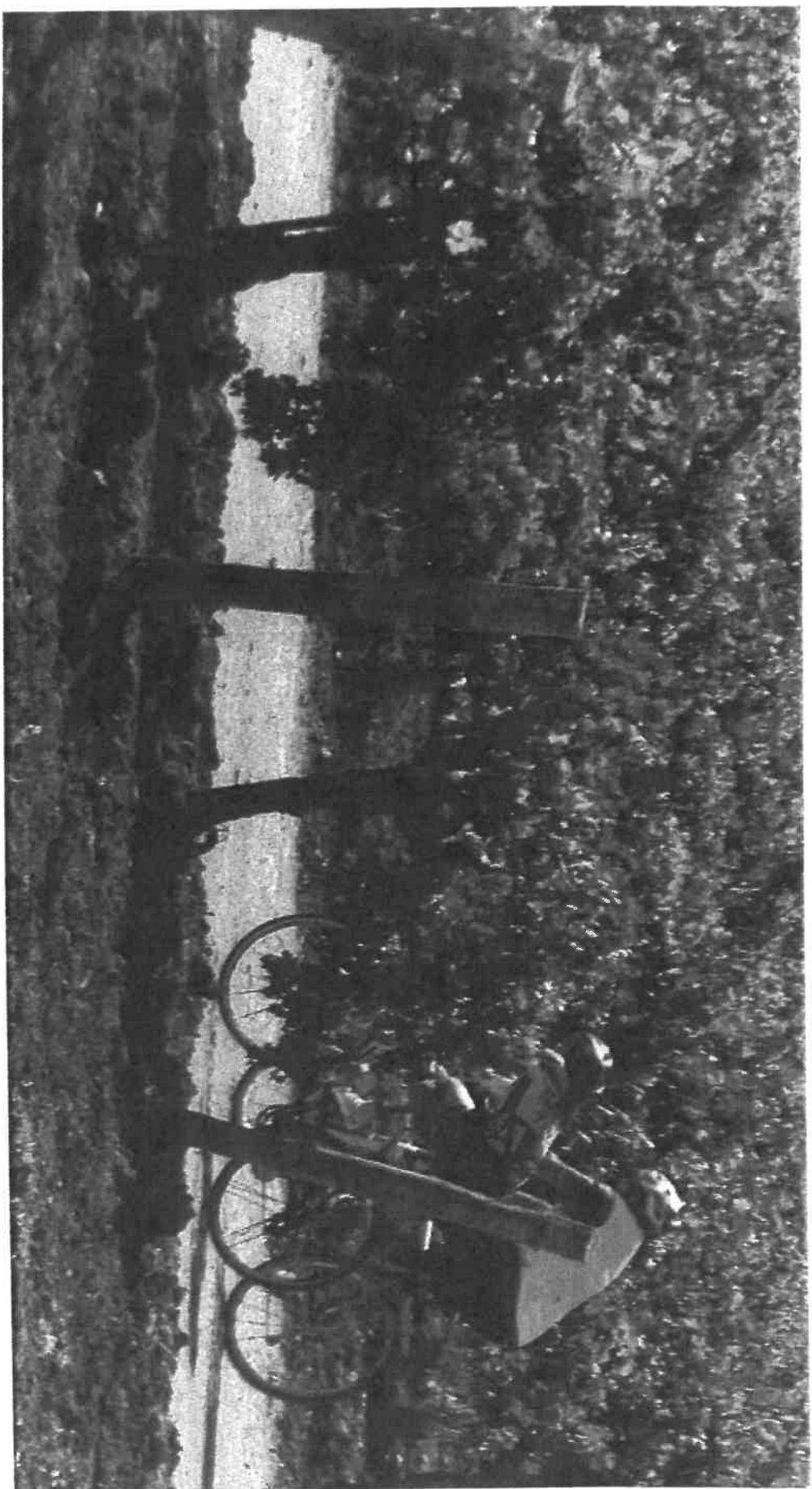










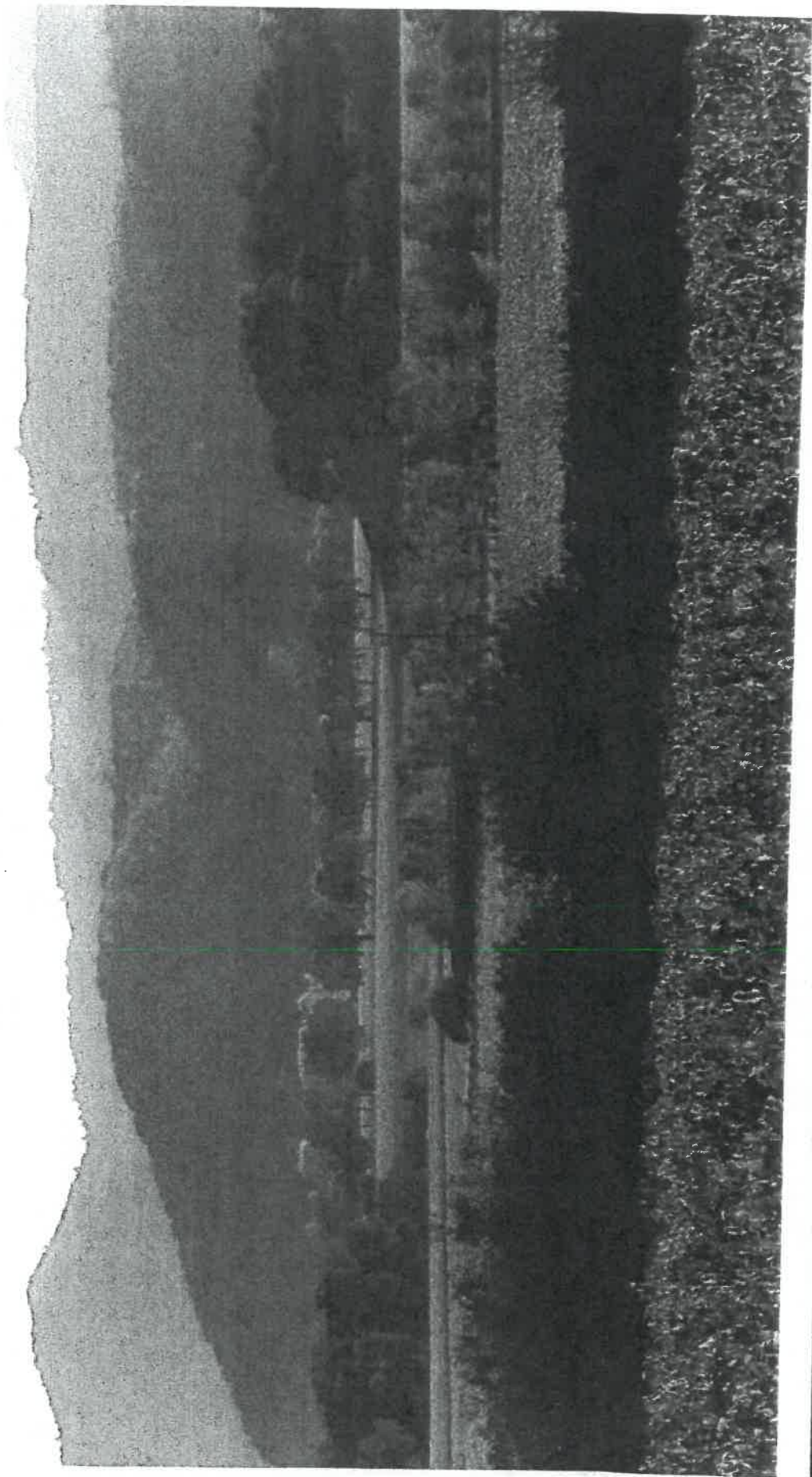


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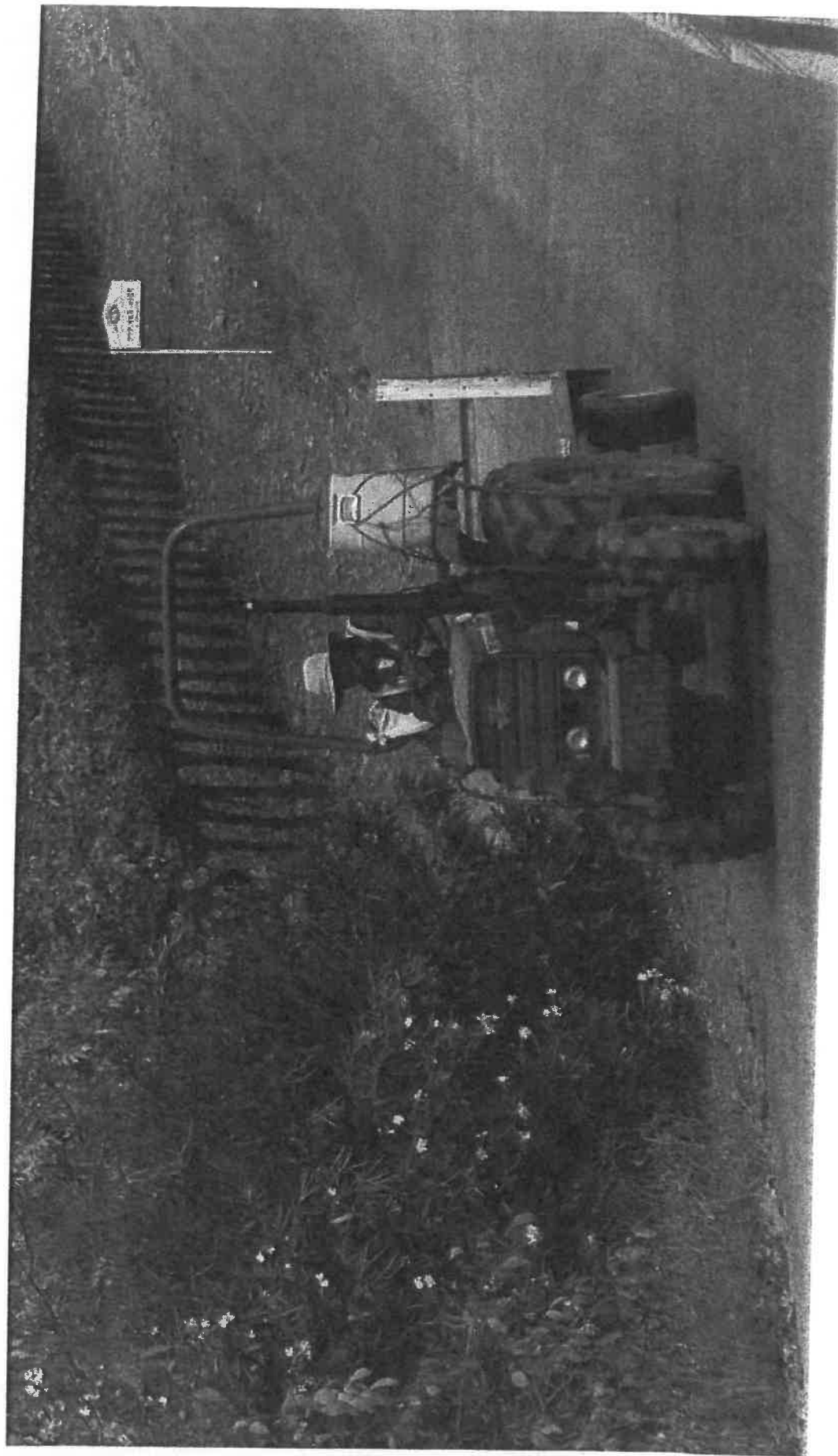
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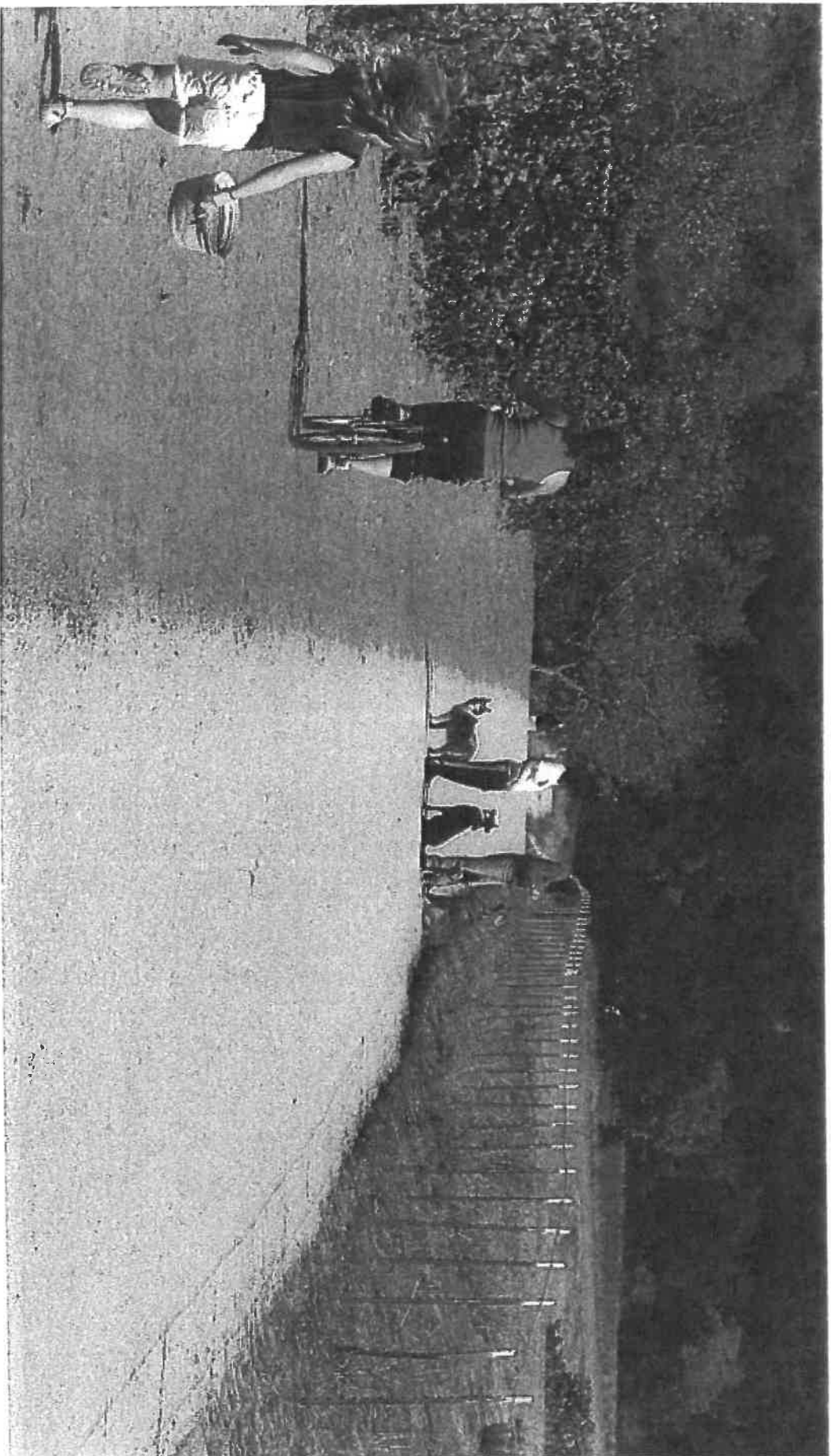










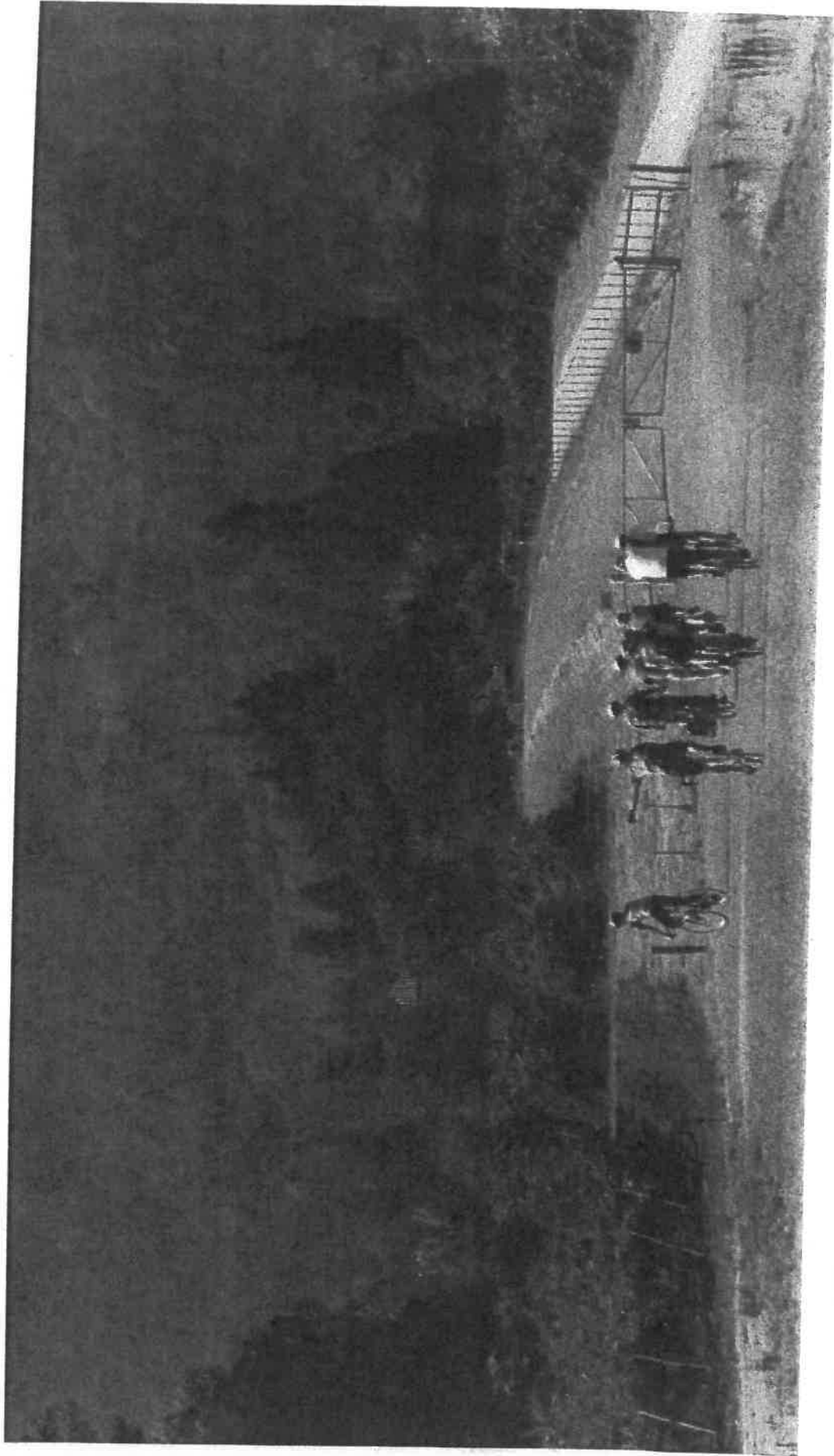


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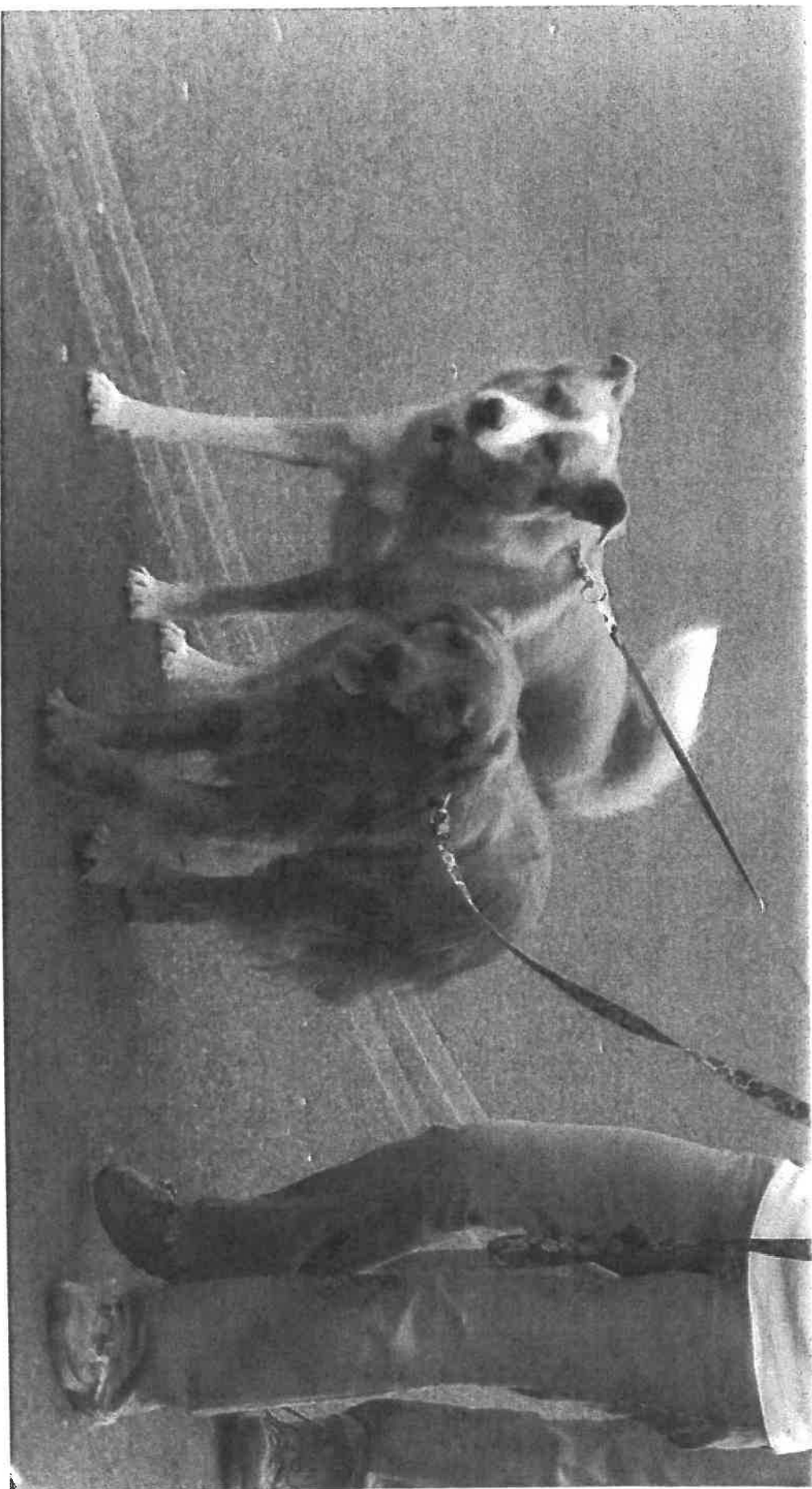




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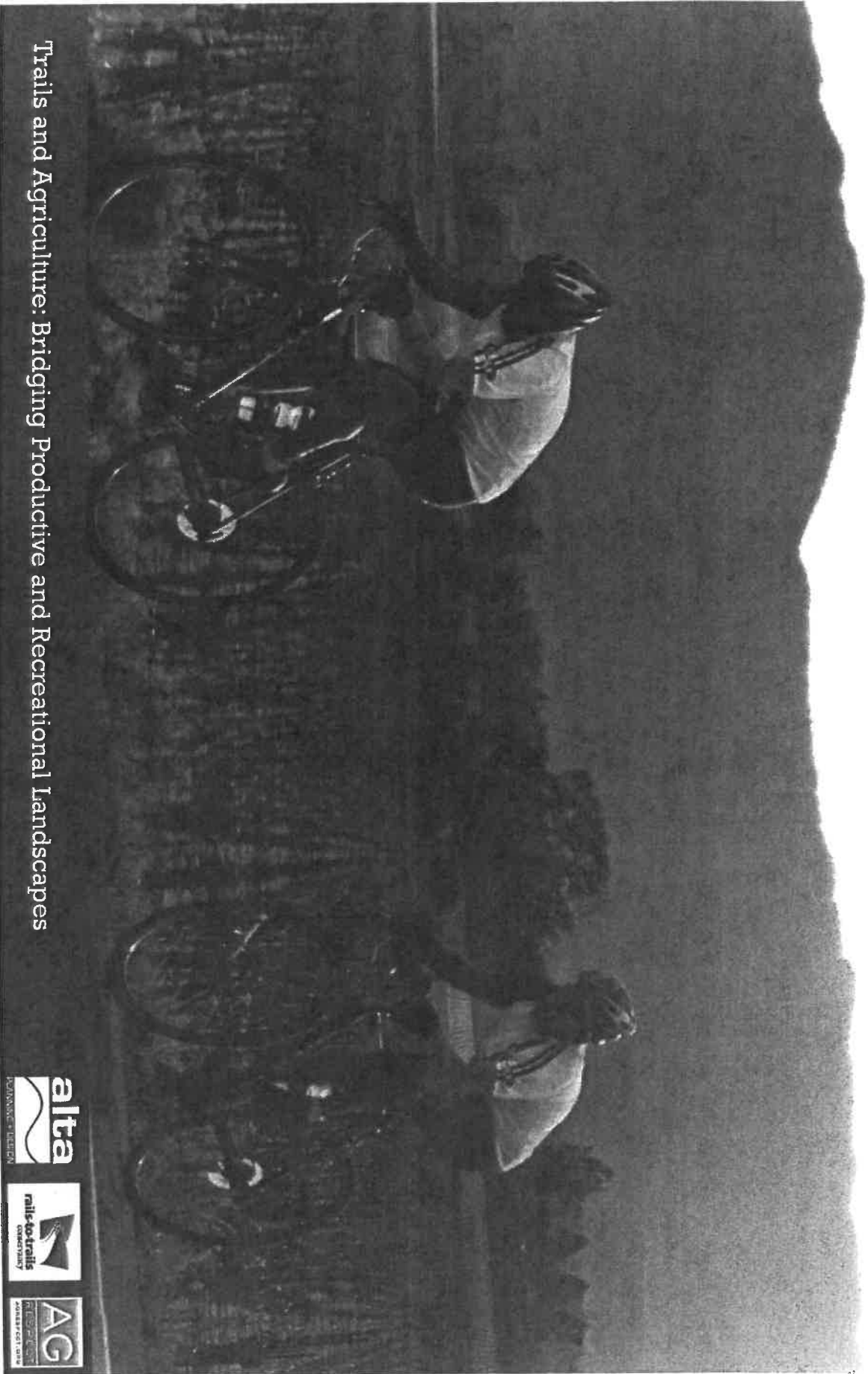


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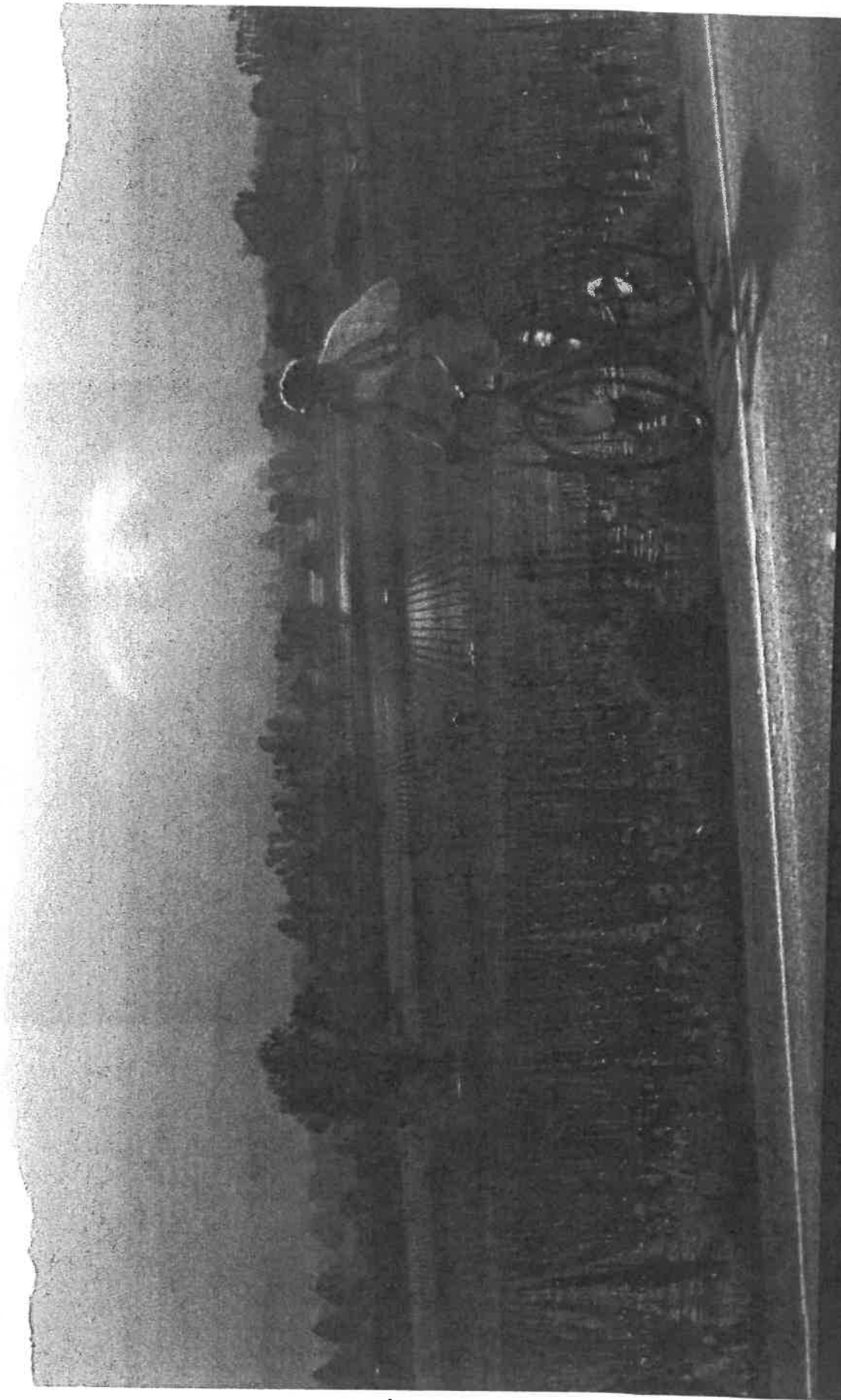
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Trails and Agriculture: Bridging Productive and Recreational Landscapes

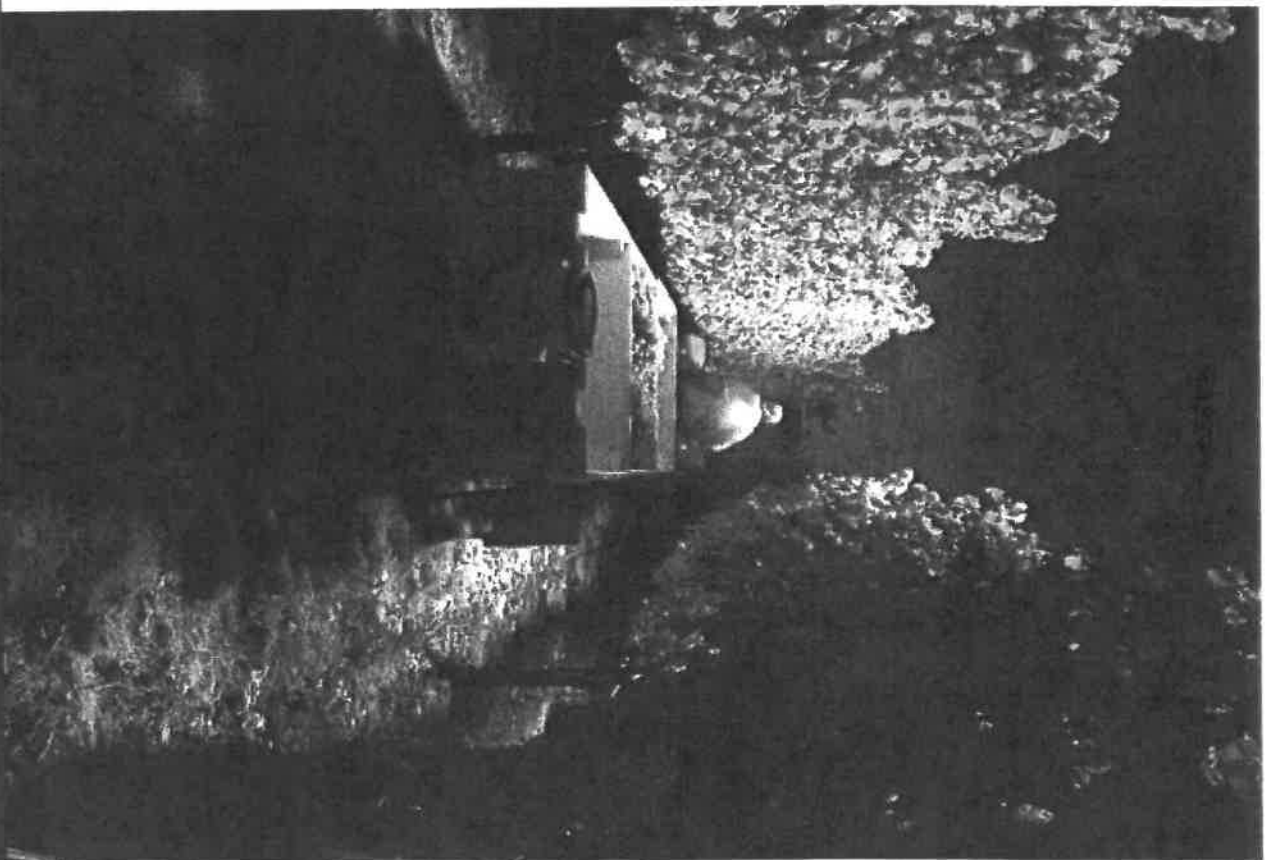




Trails and Agriculture: Bridging Productive and Recreational Landscapes

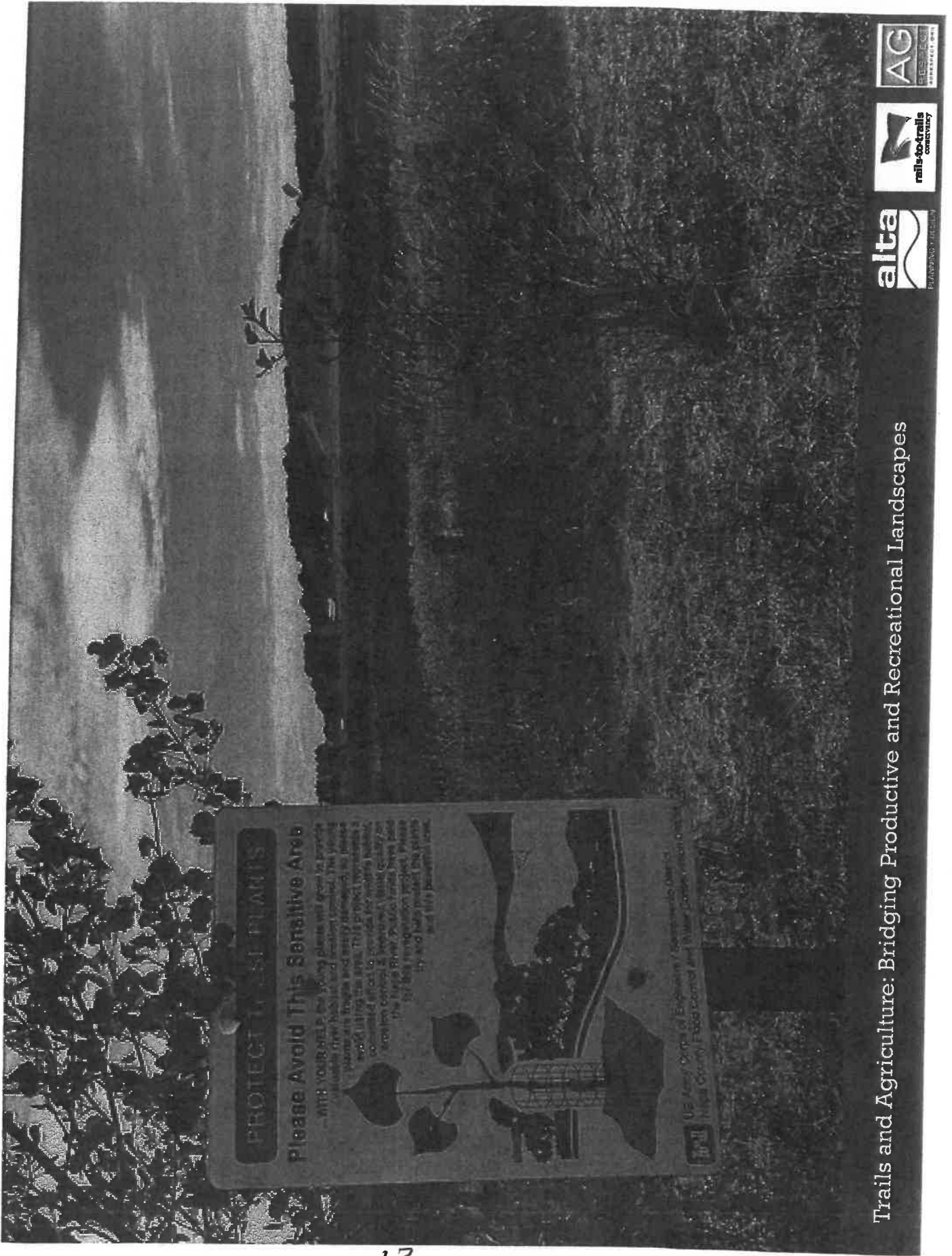




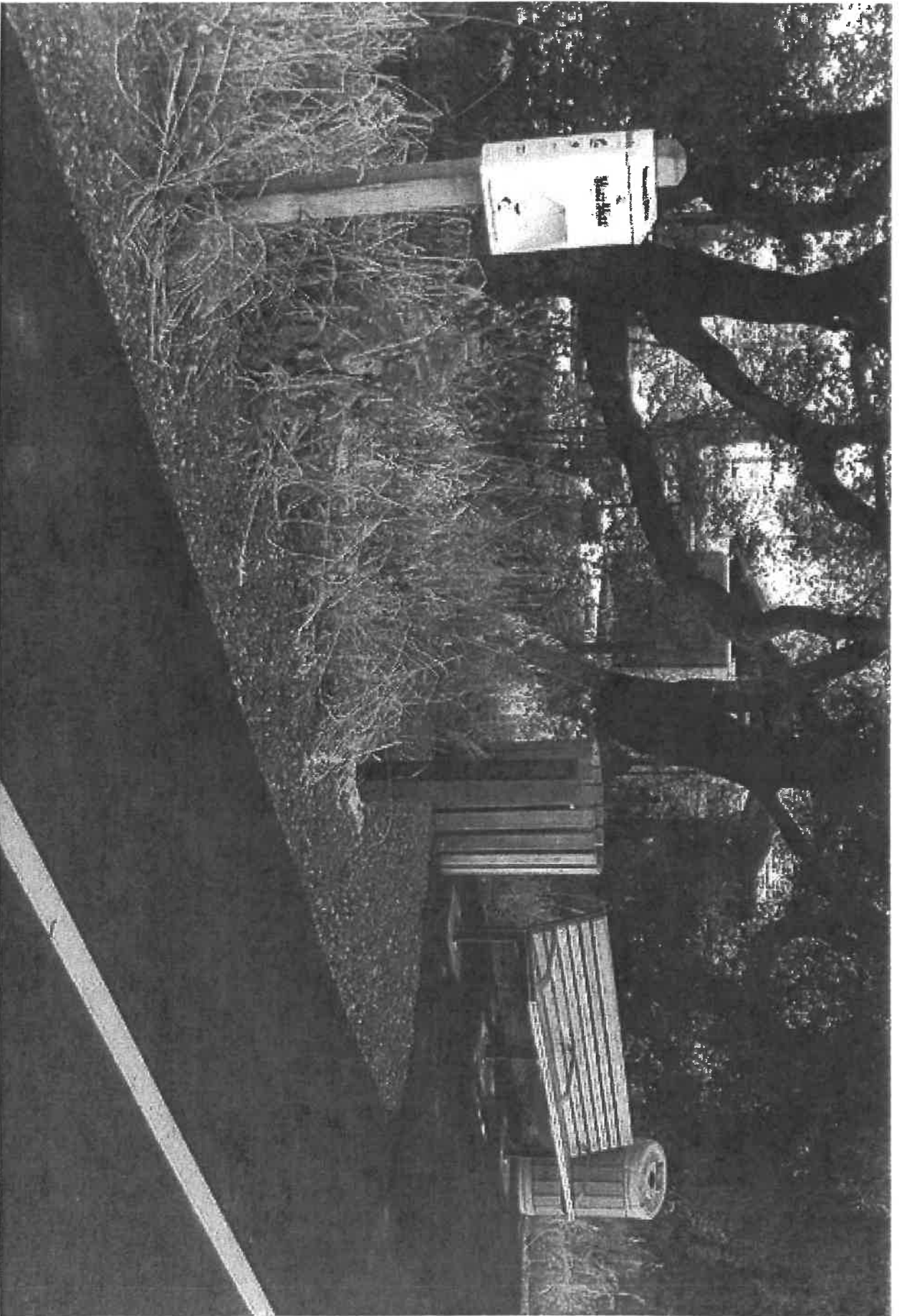


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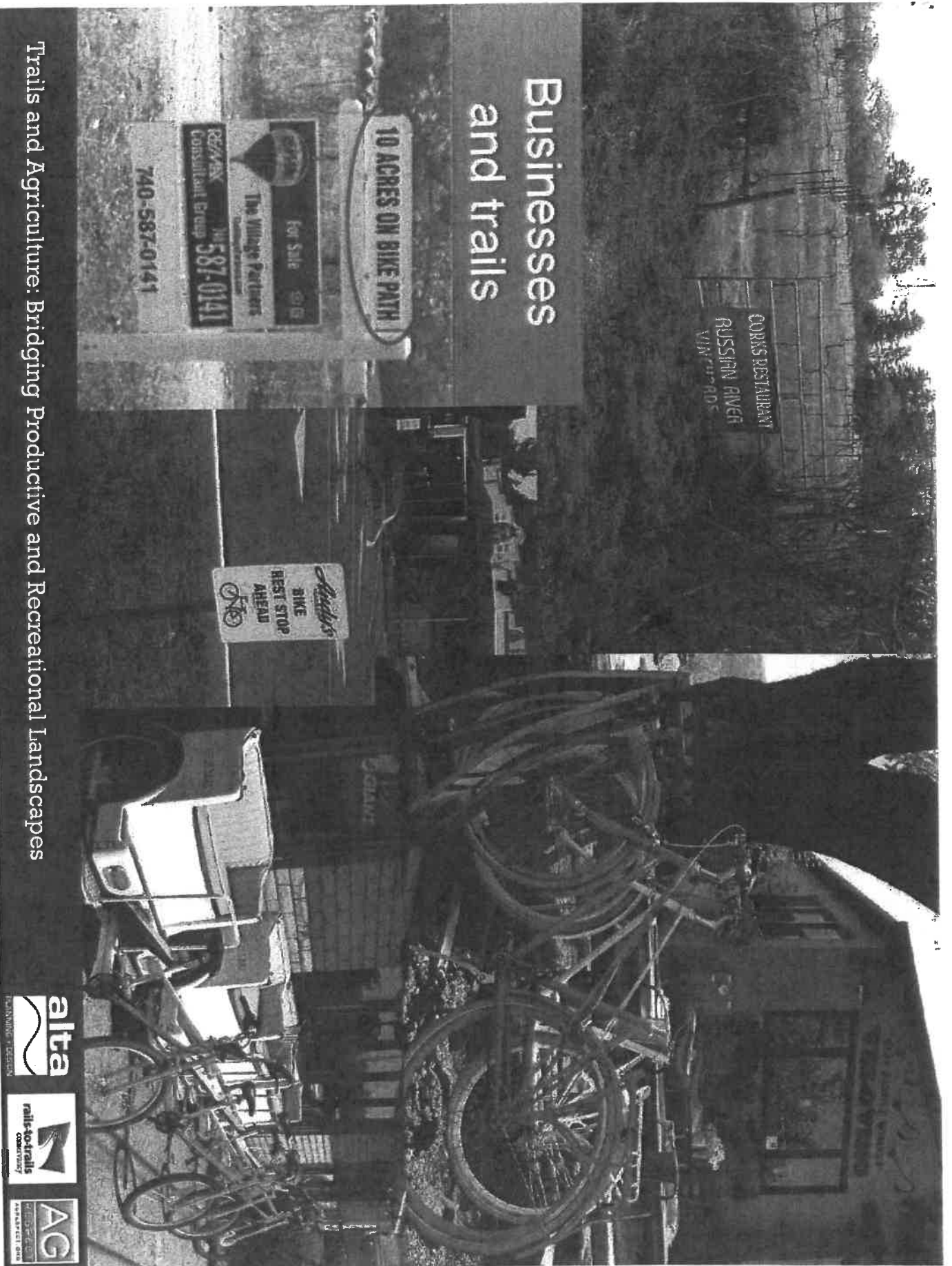


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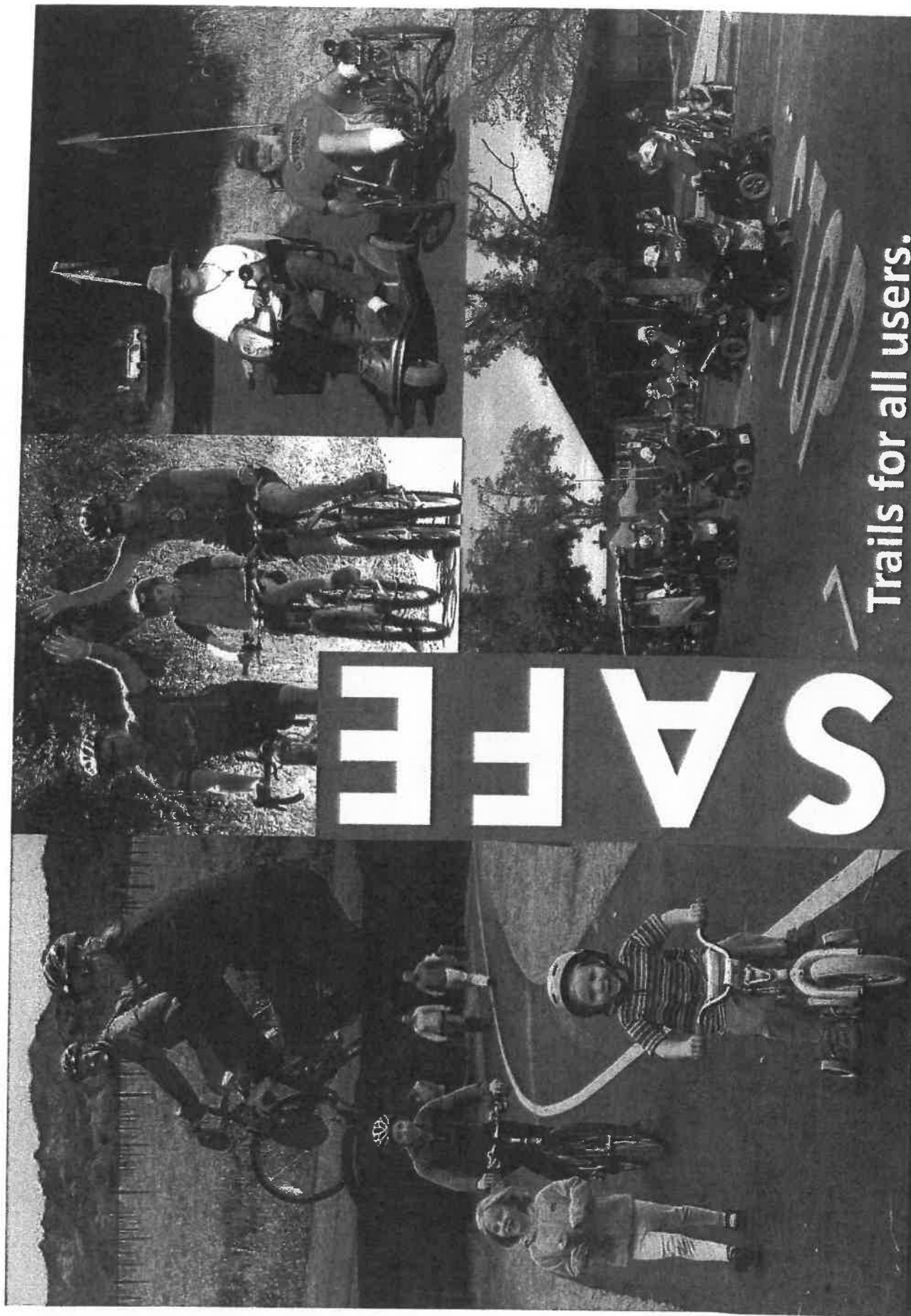






## Businesses and trails

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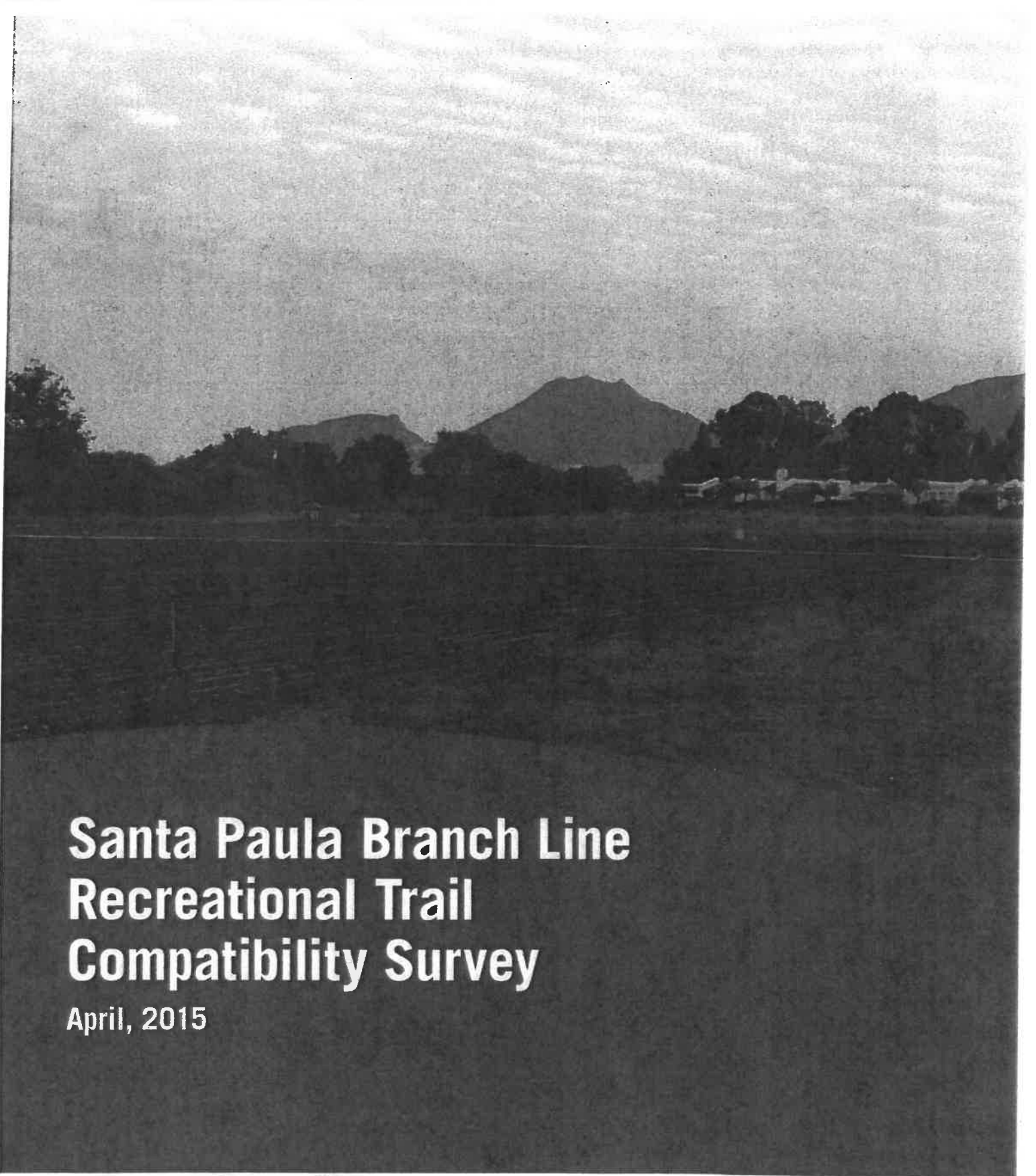
# SAFE

Trails for all users.

Trails and Agriculture: Bridging Productive and Recreational Landscapes







# **Santa Paula Branch Line Recreational Trail Compatibility Survey**

April, 2015



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# 1 INTRODUCTION

This report is an inventory and analysis of existing trails in agricultural settings, with a focus on trails that are most comparable to the context of the Santa Paula Branch Line (SPBL) in Ventura County. The objective was to find trails that have been successfully implemented and are currently operating in active agricultural areas similar to the proposed SPBL trail, and identify the challenges they faced and the factors that made them successful. Significant effort was made to contact the agricultural owner/operators adjacent to the trail in regard to their experience and perspective, as well as that of the trail owner/operator. While this study was conducted specifically for the SPBL, the findings are applicable to other areas where trails may traverse agricultural properties.

Criteria for comparable examples included:

1. Trail corridors that pass through active agricultural areas; particularly high-value row crops and orchards;
2. Paved trails that have a range and level of use comparable to what is envisioned for the SPBL;
3. Trails in California or those with a comparable agricultural/environmental setting;
4. Trails for which detailed information could be obtained for the trail owner/operator, and if possible from the adjacent agricultural owner(s)/operators;
5. Presence of or history of rail use near the trail corridor.

Over thirty trails were identified matching the first criteria, as listed in Section 3.3. Of those, nine trails with the greatest similarity to the SPBL have been selected for detailed profiles in Section 4. Information gathered for the remaining trails is presented in Section 5.

In addition to the trail research and case studies, a review of technical literature and guidelines related to trails in agricultural settings was performed. The most relevant literature, themes, and findings are summarized in Section 3.4.



## 2 BACKGROUND & CONTEXT

Stretching 32 miles from Highway 101 in the west to the Los Angeles County line in the east, the Santa Paula Branch Line (SPBL) rail corridor passes through the cities of Santa Paula and Fillmore as well as active agricultural areas. While 29 miles of track remain in active use, the future use of the corridor is to be determined. The Ventura County Transportation Commission (VCTC) purchased the corridor in 1995 from the Southern Pacific Transportation Company and manages the corridor with the potential to develop freight, commuter rail, utilities, and/or recreational trails and parks. The right-of-way averages 100 feet wide, but varies in places from 30 to 250 feet wide.

In the year 2000, VCTC adopted the Santa Paula Branch Line Recreational Trail Master Plan and certified the Santa Paula Branch Line Recreational Trail Master Plan Environmental Impact Report (EIR). The SPBL Recreational Trail Master Plan provides design guidelines, preliminary engineering, and a preferred alignment for the trail, traversing the cities of Ventura, Santa Paula, Fillmore, the community of Piru, and agricultural areas in unincorporated Ventura County. To date, three trail segments have been constructed in Santa Paula, Fillmore, and Piru. In response to significant concerns and protest from agricultural interests, trail construction in the agricultural areas of the unincorporated County was prohibited by a 15-year agreement between VCTC, the County, and property owners adjacent to the SPBL. This agreement expires in February 2015.

The rail corridor is owned in fee by VCTC. Along much of the SPBL, agricultural operations line both the north and south sides of the VCTC right-of-way and in some areas encroach onto the 100-foot right-of-way, pursuant to existing lease agreements between VCTC and the agricultural operators. Many agricultural crossings are legally entitled; of these, some are location-specific while others are generally or vaguely located. Some farmers are traveling on the right-of-way laterally without the legal right to do so. Agricultural uses along the SPBL change in response to market demand and crop viability. Currently, the adjacent properties generally include row crops and orchards (e.g., avocados and lemons).

In 2013, the County prepared engineering plans and an EIR addendum for its Piru Commuter Bicycle Path Phase III Project, which proposed construction and operation for an approximately 1-mile segment of the larger SPBL Recreation Trail in the Piru area. The project was met with significant opposition from agricultural interests, including the Farm Bureau; the Ventura County Coalition of Labor, Agriculture and Business; and the County Agricultural Commissioner. Concerns expressed by agricultural landowners and interests included vandalism, litter, increased liability, trespassing, the potential loss of the ability to cross the SPBL corridor, and the potential loss of existing farmland to buffers between recreational and agricultural uses.

As the 15-year agreement between VCTC, the County, and property owners adjacent to the SPBL approaches its end, new strategies are sought to address the relationship between agricultural and recreational interests, in the hope that the constructed portions of trail along the SPBL can be joined into a continuous whole reaching the coast. This report investigates trails that have been implemented in active agricultural areas, and reviews how they affected agricultural operations and food production. Outreach methods, negotiations between interested parties, trail and buffer design, and trail management policies and strategies are evaluated for their success or failure in balancing the needs of all stakeholders.

### 3 SUMMARY OF FINDINGS

Trails and active agriculture areas can and do exist in harmony, as demonstrated by examples from across the United States, including a wide range of use levels, trail surfaces, and management policies. The following strategies have been essential to the success of trails in agricultural areas:

- Indemnification of farmers against liability from trail use (in California there are existing statutes that provide strong protections);
- Fencing to clearly delineate trail and agricultural areas and provide barriers;
- Policies and agreements that give farmers the ability to close portions of the trail when agricultural operations would otherwise be limited by or hazardous to trail users;
- Controlled crossings that allow farm equipment to reach both sides of the trail, where necessary;
- Signage to alert trail users to the presence of active agricultural operations and instructing users to stay on the trail;
- Maintaining and observing the trail at a level to minimize vandalism and encourage a self-policing environment.

#### 3.1 Design and Management Strategies

The most common thread in successful trail planning and management in agricultural settings has been one-on-one cooperation between trail operators and adjacent farmers and landowners. By developing these individual relationships, trail managers are able to accommodate concerns of farmers that are specific to the land features, crops, operations and machinery required for unimpeded farming. While farm bureaus and other agricultural representative bodies have, as a matter of policy, opposed recreational uses adjacent to farmland, individual farmers adjacent to the trails analyzed in this study have reported very little, if any, conflict with trail operations, trail users, or have had their farming operations hampered by adjacent trails. Illegal dumping, when it has been documented, has been the responsibility of the trail operator to clean up, and the presence of the trail removes the farm operator's responsibility. Concerns of trespassing, theft, and vandalism have not been supported by evidence.

Of all documented management practices, one of the most common is the ability to close the trail, or portions thereof, to allow agricultural operations such as spraying to occur without the danger of affecting trail users. Design measures that have helped minimize conflict include fencing and/or planted buffers between trails and crops, and the design and maintenance of regular trail crossings and gates for farmers.

The topic of trails through agricultural areas deserves additional ongoing study. As evidenced in this report, existing studies of trails, policies, and guidelines to address the interactions of recreation with farms are scarce. This study attempts to bring some of the strategies already in place in trails throughout the nation into an organized collection, while highlighting the most effective management techniques, design elements, and outreach methods. Of utmost importance is that trail planners and operators make direct contact with adjacent farmers and landowners and allow flexibility in trail design and management to meet the individual needs of affected stakeholders.

## 3.2 Indemnification

The California Government Code includes protections for landowners and facility operators from legal claims by recreational users. Counties and trail operators have, in some cases, chosen to implement specific policies to further indemnify trail-adjacent farmers and landowners from liability for any harm that may come to trail users. Applicable California codes and a selection of county and operator-specific policies are reproduced below.

California Government Code § 831.4 provides protection to public entities and easement grantors from liability to users of recreational trails, regardless of trail surface:

831.4. A public entity, public employee, or a grantor of a public easement to a public entity for any of the following purposes, is not liable for an injury caused by a condition of: (a) Any unpaved road which provides access to fishing, hunting, camping, hiking, riding, including animal and all types of vehicular riding, water sports, recreational or scenic areas and which is not a (1) city street or highway or (2) county, state or federal highway or (3) public street or highway of a joint highway district, boulevard district, bridge and highway district or similar district formed for the improvement or building of public streets or highways. (b) Any trail used for the above purposes. (c) Any paved trail, walkway, path, or sidewalk on an easement of way which has been granted to a public entity, which easement provides access to any unimproved property, so long as such public entity shall reasonably attempt to provide adequate warnings of the existence of any condition of the paved trail, walkway, path, or sidewalk which constitutes a hazard to health or safety. Warnings required by this subdivision shall only be required where pathways are paved, and such requirement shall not be construed to be a standard of care for any unpaved pathways or roads.

*(California Government Code Section 831.4. <http://www.leginfo.ca.gov/cgi-bin/displaycode?section=gov&group=00001-01000&file=830-831.8>)*

Further, California Civil Code § 846 specifically indemnifies private land owners against liability for any recreational users entering their property:

846. An owner of any estate or any other interest in real property, whether possessory or nonpossessory, owes no duty of care to keep the premises safe for entry or use by others for any recreational purpose or to give any warning of hazardous conditions, uses of, structures, or activities on such premises to persons entering for such purpose, except as provided in this section.

A "recreational purpose," as used in this section, includes such activities as fishing, hunting, camping, water sports, hiking, spelunking, sport parachuting, riding, including animal riding, snowmobiling, and all other types of vehicular riding, rock collecting, sightseeing, picnicking, nature study, nature contacting, recreational gardening, gleaning, hang gliding, winter sports, and viewing or enjoying historical, archaeological, scenic, natural, or scientific sites.

An owner of any estate or any other interest in real property, whether possessory or nonpossessory, who gives permission to another for entry or use for the above purpose upon the premises does not thereby (a) extend any assurance that the premises are safe for such purpose, or (b) constitute the person to whom permission has been granted the legal status of an invitee or licensee to whom a duty of care is owed, or (c) assume responsibility for or incur liability for any injury to person or property caused by any act of such person to whom permission has been granted except as provided in this section.

This section does not limit the liability which otherwise exists (a) for willful or malicious failure to guard or warn against a dangerous condition, use, structure or activity; or (b) for injury suffered in any case where permission to enter for the above purpose was granted for a consideration other than the consideration, if any, paid to said landowner by the state, or where consideration has been received from others for the same purpose; or (c) to any persons who are expressly invited rather than merely permitted to come upon the premises by the landowner.

Nothing in this section creates a duty of care or ground of liability for injury to person or property.

*(California Civil Code Section 846. <http://www.leginfo.ca.gov/cgi-bin/displaycode?section=civ&group=00001-01000&file=840-848>)*

San Diego County Ordinance Number 9233 (the "Trail Defense and Indemnification Ordinance") provides a similar indemnification agreement, but specifically for owners of parcels containing or adjacent to recreational trails. Specific indemnification language from the ordinance is below:

**Sec. 812.103. INDEMNITY.** The County of San Diego will defend and indemnify an owner of a parcel of land as described in this chapter, from all claims, demands or liability for injury to person or property that occurs on the trail, or incidental to use of the trail, when used for any recreational purpose, excluding injury occurring in any of the following circumstances:

- a) The owner's willful or malicious failure to guard or warn against a dangerous condition, use, structure or activity;
- b) Where permission for recreational use was granted for a consideration other than the benefit received at the time of dedication;
- c) Where the person suffering injury was expressly invited by the owner to use the trail for a recreational purpose rather than merely permitted to use it;
- d) Where the person suffering injury is a member of the owner's household.

*(San Diego County Ordinance Number 9233. <http://www.sandiegocounty.gov/cob/ordinances/ord9233.pdf>)*

Also in San Diego County, the San Dieguito Joint Powers Authority (JPA), operator of the San Pasqual Valley Agricultural Trail (see page 11), passed a resolution to specifically indemnify farmers adjacent to the trail against claims from trail users. The JPA carries insurance to assist in the legal defense of suits brought against land owners, and also assists with legal counsel. This resolution, as applied to the Mule Creek Trail (with a similar agricultural adjacency) appears in Appendix A.

In areas with active agricultural operations, adequate signage alerting trail users to farming activities and equipment should be installed to alert users to the possibility of hazardous conditions.

### 3.3 National Trail Inventory

In order to identify trails that are most applicable to the SPBL, a nationwide inventory was completed of trails that pass through or adjacent to active agricultural lands. Data was gathered for each of these trails and used to determine which trails were most comparable to the SPBL. Trails included in this national inventory are shown in Table 1. From this list the most pertinent nine examples were selected for more detailed case studies. Information on the remaining trails is provided in Section 5.

**Table 1 – National Trail Inventory – Trails in Agricultural Settings**

NAME OF PROJECT	STATE	COUNTY	CITY
Arundell Barranca Bike Path	CA	Ventura	Unincorporated
Bob Jones Pathway	CA	San Luis Obispo	Avila Beach
Catskill Scenic Trail	NY	Delaware, Schoharie	
Cedar Valley Nature Trail	IA	Linn	
Conewago Recreational Trail	PA	Lebanon, Lancaster	
Cowell-Purisima Coastal Trail	CA	San Mateo	South of Half Moon Bay
Fred Meijer Heartland Trail	MI	Montcalm	Edmore, MI
Goleta Bicycle Route	CA	Santa Barbara	
Hanover Trolley Trail	PA	York	
Harlem Valley Rail Trail	NY	Dutchess, Columbia	
Hart-Montague Trail	MI	Muskegon and Oceana	
Hennepin Canal Parkway	IL	Bureau, Henry, Whiteside	
Ice Age Trail	WI	Statewide	
Joe Rodota Trail	CA	Sonoma	Santa Rosa to Sebastopol
John Wayne Pioneer Trail	WA	King, Kittitas	
Lake Wobegon Trail	MN	Stearns	
Lakelands Trail	MI	Ingham, Livingston, and Washtenaw	
Latah Trail	ID	Latah	Moscow
Macomb Orchard Trail	MI	Macomb	
Mokelumne Coast to Crest Trail	CA	Alameda, Calaveras, Contra Costa, Tuolumne	

NAME OF PROJECT	STATE	COUNTY	CITY
Monterey Bay Scenic Sanctuary Trail	CA	Santa Cruz	3 miles east of Santa Cruz
Mullet Hall Equestrian Trail System	SC	Charleston	
Musketawa Trail	MI	Ottawa, Muskegon	
Norwottuck Rail-Trail	MA	Hampshire	Belchertown, Northampton
Oak Creek Trail	NE	Butler, Saunders	
Obern Trail (Atascadero Bike trail)	CA	Santa Barbara	
Ohlone Rail Trail	CA	Santa Cruz	
Raccoon River Valley Trail	IA	Dallas	
Row River Trail	OR	Lane	
Russell Boulevard Bike Path	CA	Yolo, Solano	Between Davis and Winters
San Pasqual Valley Agricultural Trail/ Mule Hill Historic Trail	CA	San Diego	San Diego
Sauk Rail Trail	IA	Carroll, Sac	Carroll, Lake View
Slippery Elm Trail	OH	Wood	
South Prong Rocky River Greenway (SE Greenway, Davidson Greenway)	NC	Mecklenburg	Davidson
Stavich Bike Trail	PA/OH	Mahoning (OH), Lawrence (PA)	
Ventura River Trail (Ojai Valley Trail Extension)	CA	Ventura	Ventura
West County Trail	CA	Sonoma	

### 3.4 Literature Review

A search and review of related literature highlights the need for research of this kind. The vast majority of existing research on the combination of recreational and agricultural uses involves either low-intensity grazing land or the establishment of agritourism. While agritourism can provide benefits to both farmers and trail users, it presents a very different situation to the SPBL, where agricultural operations are large scale and intensive. The literature summarized in Table 2 addresses trails in agricultural settings in a general way, or agritourism in a way that provides guidelines applicable to trails in agricultural areas.

**Table 2 - Literature Review Summary**

TITLE	GEOGRAPHICAL REGION	AUTHOR/ AGENCY/ PUBLICATION	YEAR PUBLISHED	NOTES/DESCRIPTION
Trails through Agriculture Areas	British Columbia	British Columbia Ministry of Agriculture	2005	A guidebook, brochure, and series of pictures to address conflict between trail users and agriculture. Directed at user education.
A Guide to Using and Developing Trails in Farm and Ranch Areas	British Columbia	British Columbia Ministry of Agriculture and Lands	2005	The guide contains suggestions and recommendations for people who are directly involved in the planning, design, development and maintenance of trails that go through agricultural lands.
Land Trusts and the Choice to Conserve Land with Full Ownership or Conservation Easements	United States	Dominic P. Parker	2004	Conservation easements, descriptions, examples where owners adjacent to easements have built fences, maintained trails.



TITLE	GEOGRAPHICAL REGION	AUTHOR/ AGENCY/ PUBLICATION	YEAR PUBLISHED	NOTES/DESCRIPTION
Recreation, tourism and the farmer	England/Wales	Michael Dower	1973	Details common concerns and conflicts. Encourages farmers to embrace tourism, provide services for visitors. Recommends the development of information similar to the materials in British Columbia.
Governing recreational activities in Ireland: a partnerships approach to sustainable tourism	Ireland	Thomas van Rensburg	2006	
Protecting and managing private farmland and public greenways in the urban fringe	Hartford, CT	Robert L. Ryan	2004	Bring recreation and conservation organizations together with local farmers in greenway planning decisions.
Rail-Trails and Community Sentiment	United States	RTC	1998	General strategies for reducing conflict
Ag Respect	Napa Valley, CA	Napa Valley Vine Trail Coalition	Online, ongoing	Media campaign created by the Napa Valley Vine Trail Coalition in partnership with the Napa County Farm Bureau and Napa County Regional Park and Open Space District, to promote awareness among recreational users visiting agricultural areas.

## 4 DETAILED TRAIL INFORMATION

The following section provides nine trails that were found to be most applicable to the SPBL. A general summary of each trail's context and history is provided, along with details on the trail's physical characteristics; specific design features to accommodate agriculture; and management strategies essential to the trail's operation in an agricultural setting. Where possible, contact information for and feedback from trail and agricultural operators is supplied.

### 4.1 San Pasqual Valley Agricultural Trail, San Diego County, CA

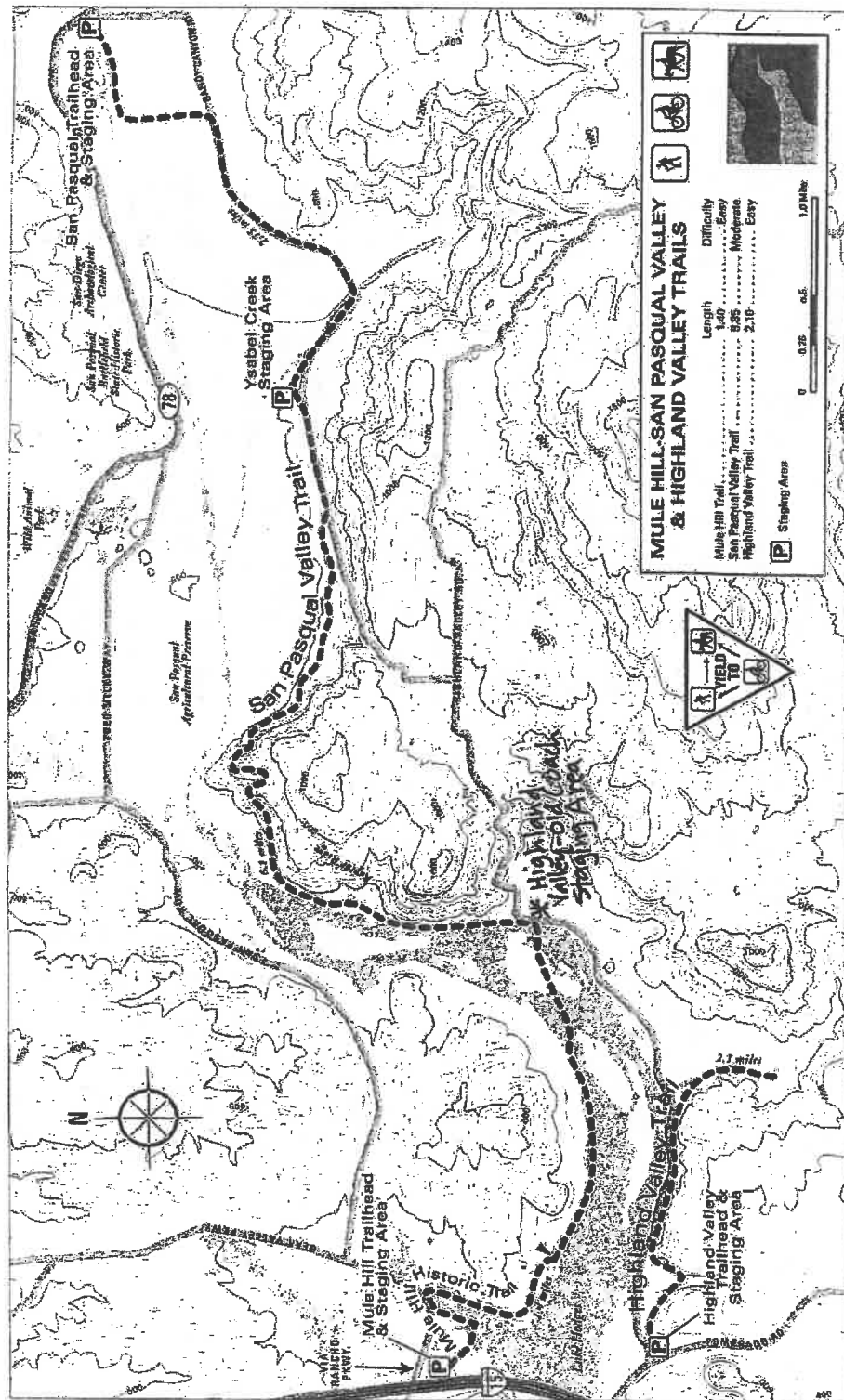
#### Summary

The San Pasqual Valley Agricultural Trail (SPVAT) was opened in June 1, 2002. The trail goes through an agricultural preserve owned by the City of San Diego, which leases the land to private farmers. During the planning phase of the trail, significant resistance was presented by local farmers and the San Diego Farm Bureau, primarily out of fear of theft and vandalism. The trail follows the edges of farm properties; farmers occasionally need to bring equipment across the trail.

(continued on next page)

Trail Features	Specific Design and Management Measures
<p><b>Trail operator:</b> San Dieguito River Park Joint Powers Authority (SDRP JPA)</p> <p><b>Length:</b> 8.75 miles</p> <p><b>Trail width:</b> 12' overall. 4' in oak grove areas, 6-8' in other constrained areas.</p> <p><b>ROW/Corridor width:</b> Varies; generally 20'</p> <p><b>Trail surface:</b> Unpaved native surface</p>	<ul style="list-style-type: none"> <li>• Specific fencing was designed for the trail, modified from the park department's standard lodgepole fencing. Chicken wire inserts were added approximately one foot from the ground to allow wildlife to pass through;</li> <li>• Gates allow sections of the trail to be closed;</li> <li>• Signage installed to alert the trail-users of trail closure for spraying and to stay on the trail;</li> <li>• Farmers can dictate trail closure (within reason, i.e., preferably not on weekends) for maintenance and crop spraying. This protocol was developed and approved by the County Farm Bureau, County Farm Advisor's Office, and the affected farmers;</li> <li>• The SDRP JPA chose to indemnify the farmers against liability issues relating to those using the trail.</li> </ul>
<p><b>Trail use:</b> Equestrian, hiking, and biking</p> <p><b>Type of crops and operations:</b> Orange groves, avocados, asparagus, squash, ornamental flowers, and row crops</p> <p><b>Trail owner/operator contact information:</b> Shawna Anderson, San Dieguito River Park, 858-674-2275 x13, shawna@sdrp.org</p> <p><b>Agricultural operator contact information:</b> Matt Witman, Witman Ranch</p>	<p><b>Feedback from Involved Parties</b></p> <ul style="list-style-type: none"> <li>• Shawna Anderson: There have been no reported incidents of theft, vandalism, or liability issues to this date.</li> <li>• Many farmers who were initially opposed to the trail now support it.</li> <li>• An agreement was made early after the SDRP JPA listened to the concerns of the farmers and created specific design and management plans to create a mutually beneficial relationship between the trail and agricultural industry.</li> <li>• One segment required the removal of orange trees to make space for the trail. The owner of the trees was compensated for the value of the trees and their future crop value.</li> <li>• Matt Witman: Citrus farmer, primarily orange groves, some organic farming;</li> <li>• Heavily involved in the early planning process. Primary concerns were trespassing and litter from the trail contaminating crops and affecting farm inspections;</li> <li>• Indemnification of farmers was "a dealbreaker" – the farmers and farm bureau would have never supported the trail without it;</li> <li>• Trespassing has not been a major problem overall, there was one instance where a bicycle race took place on the trail, and one of the racers got lost and strayed onto the farm and a pack of racers followed. No damage was done but it was not an ideal scenario;</li> <li>• Chainlink fencing is important, as it provides a better psychological barrier for trail users than lodgepole, and also keeps dogs off the farm;</li> <li>• If he could do it all over again, he would have pushed for more stringent trash cleanup requirements from the trail operator.</li> </ul>

# Trail Map



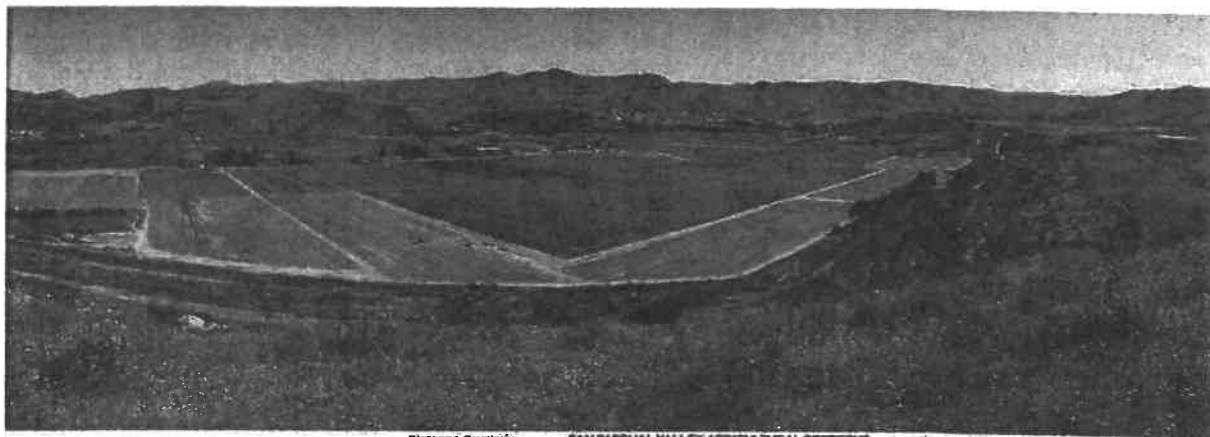
Source: [http://www.sdrp.org/images/mule\\_hill\\_trailmap.jpg](http://www.sdrp.org/images/mule_hill_trailmap.jpg)

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## Project Photos



Source: <http://www.fsdv.org/photosRVViews.html> (friends of San Diego River Valley)



Source: <http://jamescoffeestudios.smugmug.com/San-Dieguito-River-Park/Hikes/Valley/20090228-Ysabel-Creek-to-1>

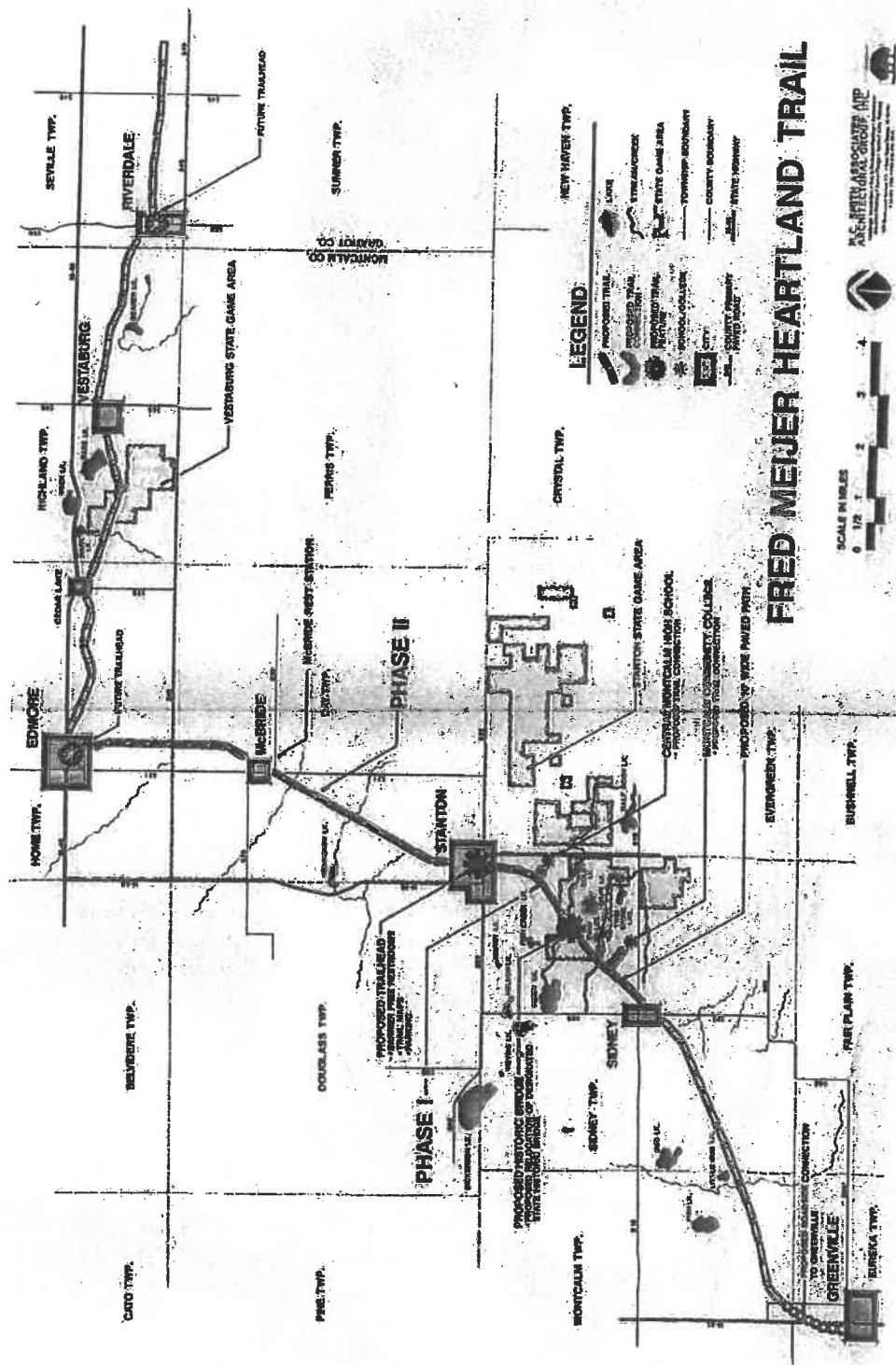
## 4.2 Fred Meijer Heartland Trail, Montcalm County, MI

### Summary

In 1994 Fred Meijer and other donors funded the purchase of the abandoned rail line and its transformation into a recreation trail. Paving was begun with grants from ISTEAL, DALMAC, and many generous private donations. Concerns from farmers included trespassing fears and restricted access to land on both sides of the trail. Other than agriculture, hunters also opposed the trail as it was used for hunting prior to development. During the trails development there were multiple outreach events between the trail developers and the public. Two hearings were held and there were petitions both for and opposing the trail. While an agreement with all adjacent farmers could not be reached, the trail was constructed with overall public support. All trail funding is from private donations and trail memberships.

Trail Features	Specific Design and Management Measures	
<p><b>Trail operator:</b> Friends of the Fred Meijer Heartland Trail</p> <p><b>Length:</b> 41 miles</p> <p><b>Trail width:</b> 10' with 2' shoulders; 14' total</p> <p><b>ROW/Corridor width:</b> 50' on each side of center line</p> <p><b>Trail surface:</b> Paved asphalt</p> <p><b>Trail use:</b> Bikers, walkers, roller blade enthusiasts and joggers</p> <p><b>Type of crops and operations:</b> Potatoes, soy, hops, corn, beans, hay, wheat, alfalfa, oats</p> <p><b>Owner/operator contact information:</b> Don Stearns, President, 989-235-6170 dkstearns@centurylink.net</p> <p><b>Agricultural owner/operators:</b> Ned Welder; Jan Pearl (property owner, leases to a farmer); Robert Spencer</p>	<ul style="list-style-type: none"> <li>• Bollards were put in place on the trail to limit vehicular use and dumping. All keyed the same with emergency responders having access to keys.</li> <li>• Gates installed to allow farmers to cross.</li> <li>• MI state law indemnifies farmers for injury to trail users.</li> <li>• When trail was constructed, a wide apron was installed to allow combines and semis to cross.</li> <li>• Signs posted to warn users to watch for farm equipment crossing.</li> </ul> <th data-bbox="715 1032 1476 1104">Feedback from Involved Parties</th> <ul style="list-style-type: none"> <li>• Don Stearns: No reports of trespassing onto farmland. Occasionally farmers have encroached into the trail buffer, spraying the trail (4 incidents in 20 years) and snowmobiles entering the trail and causing accidents have been reported. Ray Christensen, a corn farmer, was ruled against in federal court and had to pay damages for cutting down trees in the buffer within the trail's right-of-way.</li> <li>• Ned Welder: no problems with the trail. He walks along the trail to check on his crops.</li> <li>• Jan Pearl: very concerned about trespassing before the trail was built, but have had no problems with the trail or trail users. She said she was uncomfortable with change but is now a trail user and sees it as a very positive thing for the community</li> <li>• Robert Spencer: has generally experienced no problems with the trail. One issue was a deer hunter using the trail. Another is that potato farms nearby spray from the air and there has been concern about drift.</li> </ul>	Feedback from Involved Parties

## Trail Map



<http://www.montcalm.org/trail/FrontPage%20Stuff/trail/images/Map-All2.jpg>



## Project Photos



[http://trailsmichigan.com/trailpage.php?nr=69\\_Fred-Meijer-Heartland-Trail-Entire-Trail](http://trailsmichigan.com/trailpage.php?nr=69_Fred-Meijer-Heartland-Trail-Entire-Trail)



<http://vanscyoc.net/blog/archives/841-Fred-Meijer-Heartland-Trail-Michigan.html>

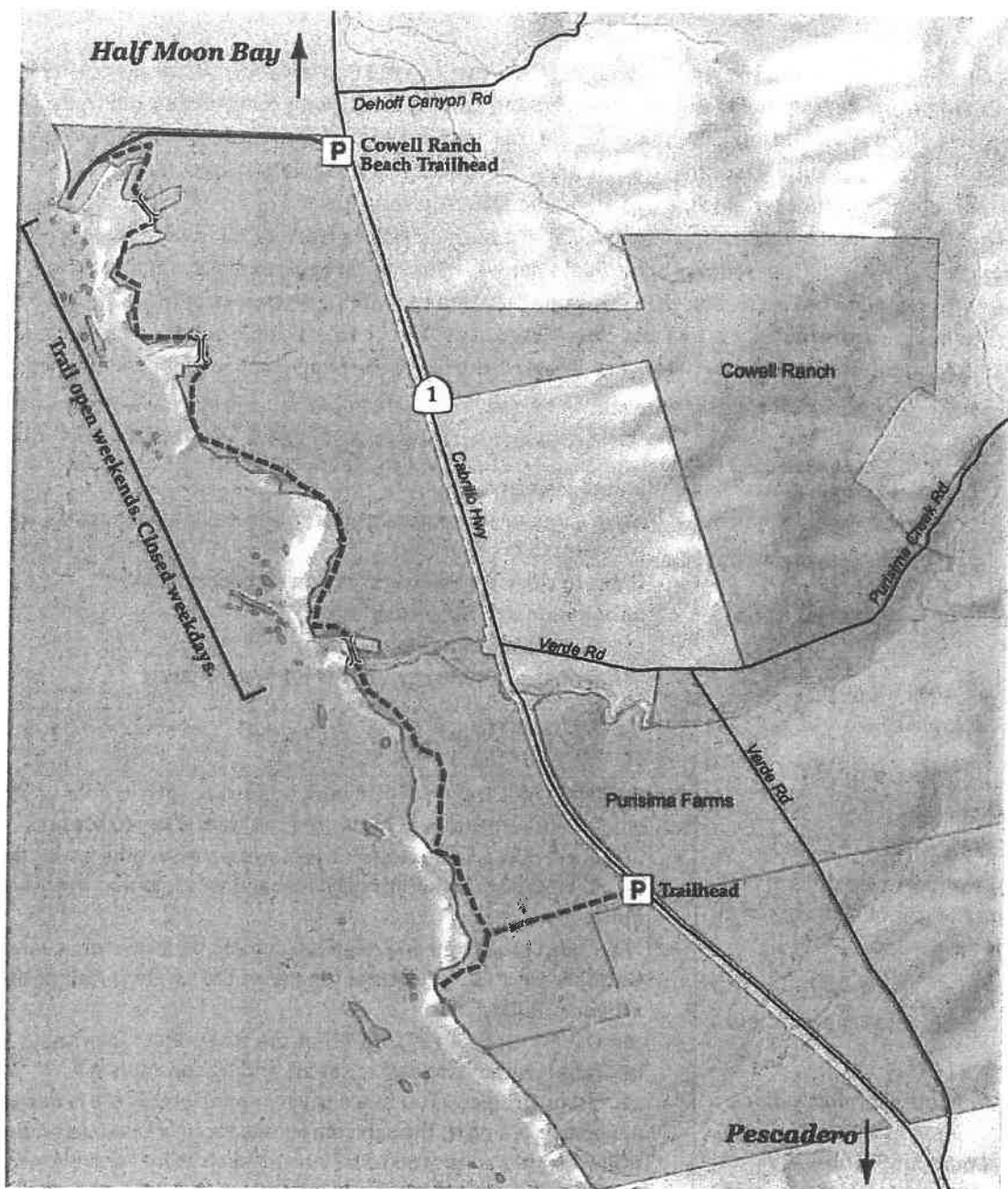
### 4.3 Cowell-Purisima Coastal Trail, San Mateo County, CA

#### Summary

The Peninsula Open Space Trust (POST), working with the California Coastal Conservancy, a state agency, bought the land to protect it from development, and later sold it to the farmer, Giusti, with conservation and trail easements in place. The design and implementation of the trail involved a lot of work with the owner/farmer to make the trail work in the agricultural setting; stout fencing; information and regulatory signs, trail gates the farmer has a right to close, within certain limits, to accommodate crop spraying and other operations; and special wide double gates to allow cattle and large equipment, such as disking tractors, to cross the trail while simultaneously closing the trail. The trail was open 7 days per week for the 1st year, except for month-long periods when it was closed on weekdays for field spraying. Then due to State parks closure of the north leg of the access due to budget constraints, the trail was closed weekdays and is currently only open weekends and holidays. POST uses volunteer docents for patrol and a local landscape restoration company for maintenance.

Trail Features	Specific Design and Management Measures
<p><b>Trail operator:</b> Peninsula Open Space Trust</p> <p><b>Length:</b> 3.6 miles</p> <p><b>Trail width:</b> 6 to 12 feet, depending on topography</p> <p><b>ROW/Corridor width:</b> Varies; 20' minimum</p> <p><b>Trail surface:</b> Unpaved, base rock surface</p> <p><b>Trail use:</b> Hikers, bicyclists, handicap accessible at most parts, no dogs or horses due to food safety concerns with adjacent farm fields</p> <p><b>Type of crops and operations:</b> Artichokes, Brussels sprouts, field crops, grazing</p> <p><b>Owner/operator contact information:</b> Paul Ringgold, Vice President, Land Stewardship, Phone: (650) 854-7696 pringgold@openspacetrust.org</p> <p><b>Agricultural owner/operator:</b> John Giusti, Giusti Farms, LTD. 650.726.9221.</p>	<ul style="list-style-type: none"> <li>• Stout fencing</li> <li>• Large gates to accommodate cattle and equipment passage while trail is closed</li> <li>• Gates to close trail during spraying and operations</li> <li>• Information and regulatory signs</li> <li>• Maintained by volunteer docents</li> <li>• Farmer has ability to close gates for maintenance</li> </ul> <p><b>Feedback from Involved Parties</b></p> <ul style="list-style-type: none"> <li>• Paul Ringgold: The ability to work as a team, such as on a section where bluff erosion was impacting the trail, is key to success.</li> <li>• POST recently asked whether there were any security issues that would benefit from additional gates and was told that there were none.</li> <li>• POST hasn't received any negative comments from owner John Giusti, or Giusti's agricultural tenant on the southern half of the property, Bob Marsh.</li> <li>• John Guisti reported 8/25/14 that the trail project "has not interfered with his operation at all, and he considers it a successful project." The fence is very important. There is never anybody crossing it, though sometimes there are people on the trail when it is supposed to be closed (such as for spraying - often runners. The project is a success because of the planning that took his concerns into consideration, and made it more of a partnership. The information about spraying and the allowance for closure was important.</li> </ul>

## Trail Map



----- New Cowell-Purisima Trail

— Trail

 Protected Land

 Bridge

### Cowell Ranch Beach to Purisima Creek

This portion of the trail is open weekends year round. Closed weekdays.

Source: [http://www.openspacetrust.org/images/Cowell\\_Purisima\\_Trail.pdf](http://www.openspacetrust.org/images/Cowell_Purisima_Trail.pdf)

## Project Photos



<http://www.wisdomportal.com/CowellRanchBeach/219-TrailheadToBeach.jpg>



[http://peninsulaopenspacetrust.files.wordpress.com/2014/08/cowell-purisimatrail-open\\_4562\\_cpaolovescia11.jpg](http://peninsulaopenspacetrust.files.wordpress.com/2014/08/cowell-purisimatrail-open_4562_cpaolovescia11.jpg)

## 4.4 Bob Jones Bike Trail, San Luis Obispo County, CA

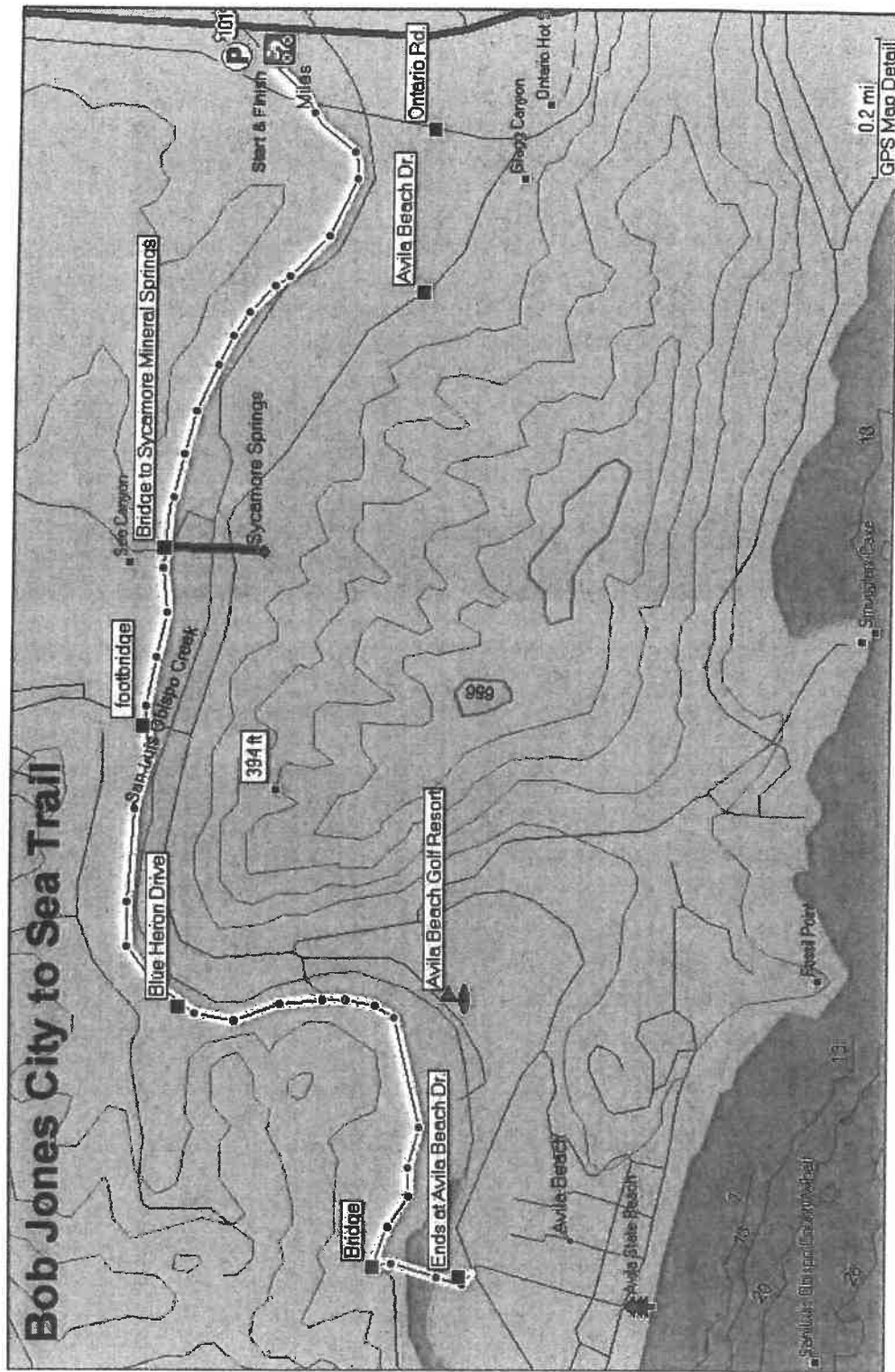
### Summary

Previously Avila Beach Trail and the Bob Jones City to Sea Bike Trail. It follows the Pacific Coast Railroad right-of-way along the San Luis Obispo Creek to Avila Beach. The trail is being built on easements that are either purchased from or donated by landowners. The existing trail is adjacent to a creek, SLO golf course, and an apple orchard.

The new segment of the trail will be adjacent to more agriculture. Draft EIR is currently underway, and a second public workshop for the EIR is expected to be conducted in late October 2014. Extensive coordination with landowners for this phase. Concerns have been raised about access and interference with farm equipment. With federal funding, negotiations on acquisitions can't begin until EIR complete.

Trail Features	Specific Design and Management Measures
<p><b>Trail operator:</b> San Luis Obispo County Parks</p> <p><b>Length:</b> 3 miles</p> <p><b>Trail width:</b> Up to 10', narrower as topography demands</p> <p><b>ROW/Corridor width:</b></p> <p><b>Trail surface:</b> Paved</p> <p><b>Trail use:</b> Bicycle and pedestrian</p> <p><b>Type of crops and operations:</b> Apple orchards</p> <p><b>Owner/operator contact information:</b> Shaun Cooper, Senior Park Planner (805) 781-4388 secooper@co.slo.ca.us</p>	<ul style="list-style-type: none"> <li>Fencing and other barriers are being considered for future portions of the trail.</li> <li>Portions of the trail have been routed around specific parcels to reduce conflict.</li> <li>In one circumstance, the route was adjusted to pass around a farm. The route originally followed farm frontage roads, but was moved to the back of properties instead.</li> </ul>
	Feedback from Involved Parties
	<ul style="list-style-type: none"> <li>Shaun Cooper: Trail generally borders agriculture on one side only, with a creek or highway on the other.</li> <li>The trail is generally on the edges of properties, where it's adjacent to either the creek or 101, so it's not interfering much with operations. The trail overall, being placed on easements, doesn't claim a great deal of property. It's taking a small overall percentage of property that it passes through.</li> </ul>

## Trail Map



Source: <http://hikesin.com/wp-content/uploads/2012/07/Bob-Jones-City-to-Sea-Trail.jpg>



## Project Photos



Source: <http://connectslocounty.org/2013/04/02/bob-jones-octagon-barn-connection-workshop-2/>

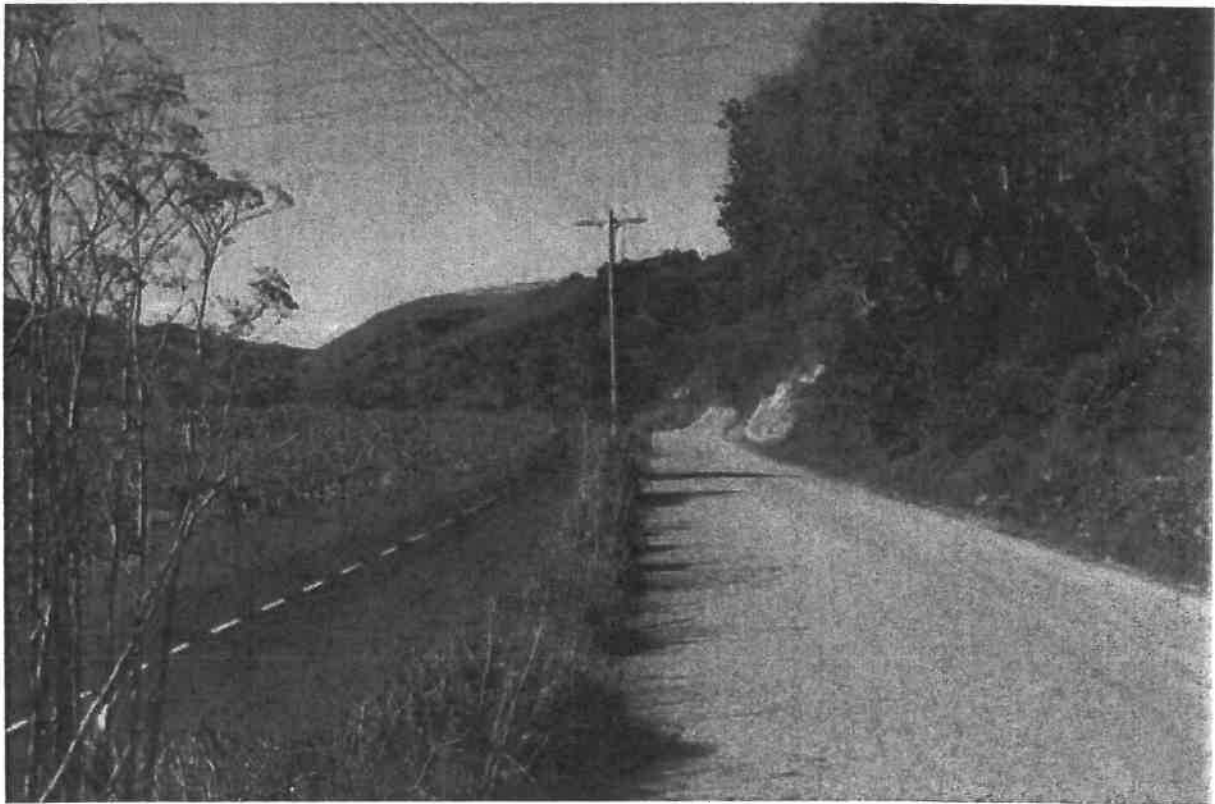


Photo simulation of proposed new segment of trail. Source: Bob Jones Pathway Draft EIR  
<http://www.slocounty.ca.gov/planning/environmental/EnvironmentalNotices/bobjonespathway.htm>



## 4.5 Obern Trail, Santa Barbara County, CA

### Summary

This trail was first proposed 1967, when housing developments and drainage creeks were being constructed in the area. George and Vie Obern lobbied for the creation of trails along these creeks, and the trail was named after them in 2004 (it was previously called the Atascadero Trail).

The surrounding agriculture includes some of the most productive in the county - it's in the flight path of the airport, not under threat from development, so there is heavy investment in irrigation, greenhouses, and equipment. The stretch from Patterson to Goleta Beach passes through high value crops.

Trail Features	Specific Design and Management Measures
<b>Trail operator:</b> Santa Barbara County <b>Length:</b> 3.5 Miles <b>Trail width:</b> 10' <b>ROW/Corridor width:</b> Varies, most often 20' <b>Trail surface:</b> Paved <b>Trail use:</b> Recreational and commuter cycling <b>Level of use:</b> High: thousands of users daily <b>Type of crops and operations:</b> Strawberries, tomatoes, nurseries, greenhouses, row crops, and orchards. <b>Owner/operator contact information:</b> Matthew Dobberteen, Alternative Transportation Manager, Santa Barbara County Department of Public Works 805-568-3576 <b>Agricultural owner/operator:</b> John Givens john.givens1@verizon.net - 805-964-4477	<ul style="list-style-type: none"><li>• Little to no physical barrier in most places. Oleander hedges and chainlink fence.</li><li>• Each farmer decides on fencing – not installed by the County.</li><li>• High levels of use create a self-policing scenario.</li><li>• The trail is lit throughout, at all hours.</li></ul>
	Feedback from Involved Parties
	<ul style="list-style-type: none"><li>• Matthew Dobberteen: In over ten years managing trails for Santa Barbara County, I have never received a complaint about the Obern Trail. Our trails that run near agriculture are never the trails we have problems with. The only issue is every few years we may get some graffiti on a retaining wall. "A bike path will make theft harder, not easier, by bringing light, attention, people, eyes to the trail." "If someone wanted to steal from a farm, they'd find a place where no one could see them, not a trail with steady use."</li><li>• John Givens: No significant impacts from the trail. Occasionally homeless pass through and there is minor vandalism, but it has not been serious enough to involve the County or other authorities. Trail users don't cut through the farm property.</li></ul>

## Trail Map

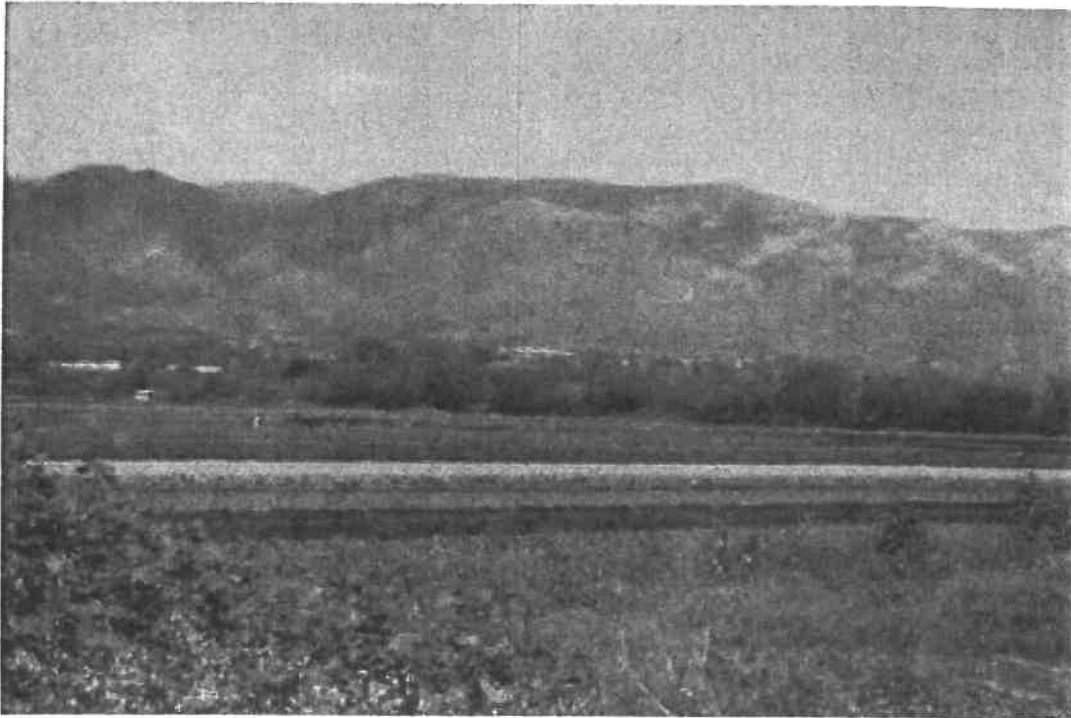


Source: <http://www.trailink.com>

## Project Photos



Source: <http://www.edhat.com/site/tidbit.cfm?nid=52049>



Source: <http://www.edhat.com/site/tidbit.cfm?nid=52049>

## 4.6 Musketawa Trail, Ottawa and Muskegon Counties, MI

### Summary

This trail was converted from an unused railroad corridor that ran between Marne and Muskegon. It links with other trails in a statewide network. Public meetings were held from 1990-1992. A trail advisory board was formed, made up of representatives from Muskegon and Ottawa Counties from different user groups and local residents.

The first mile of trail was paved in Ravenna in 1997. The following year the eastern half between Marne and Ravenna was completed. The west end will eventually connect to the Hart-Montague Trail and the east end will be extended into Grand Rapids to connect with the White Pine Trail, Kent Trails and Paul Henry-Thornapple Trail.

Trail Features	Specific Design and Management Measures
<b>Trail operator:</b> Michigan Department of Natural Resources/Friends of the Musketawa Trail  <b>Length:</b> 25 miles  <b>Trail width:</b> 12', 4-8' gravel shoulder  <b>ROW/Corridor width:</b>  <b>Trail surface:</b> Asphalt  <b>Trail use:</b> Multi-use: bicycling, equestrian, snowmobiling, pedestrian, roller/inline skating, cross-country skiing  <b>Type of crops and operations:</b> Hay, blueberries, cucumber, corn, possible fruit orchards  <b>Owner/operator contact information:</b> Wes Lomax, Michigan Department of Natural Resources, (231) 821-0553	<ul style="list-style-type: none"> <li>Chainlink or wire fencing.</li> </ul>
	<b>Feedback from Involved Parties</b>  <ul style="list-style-type: none"> <li>Wes Lomax: Conflicts with farmers during the planning phase were resolved early on; no conflicts or issues reported since.</li> </ul>



Source: [http://musketawa.mwswebsites.com/uploads/newsletters/MusketawaTrail\\_VLS-1.pdf](http://musketawa.mwswebsites.com/uploads/newsletters/MusketawaTrail_VLS-1.pdf)

## Project Photos



Source: <http://www.railstotrails.org/news/recurringfeatures/trailmonth/archives/0107.html>



Source: [http://trailsmichigan.com/trailpage.php?nr=79\\_Musketawa-Trail](http://trailsmichigan.com/trailpage.php?nr=79_Musketawa-Trail)

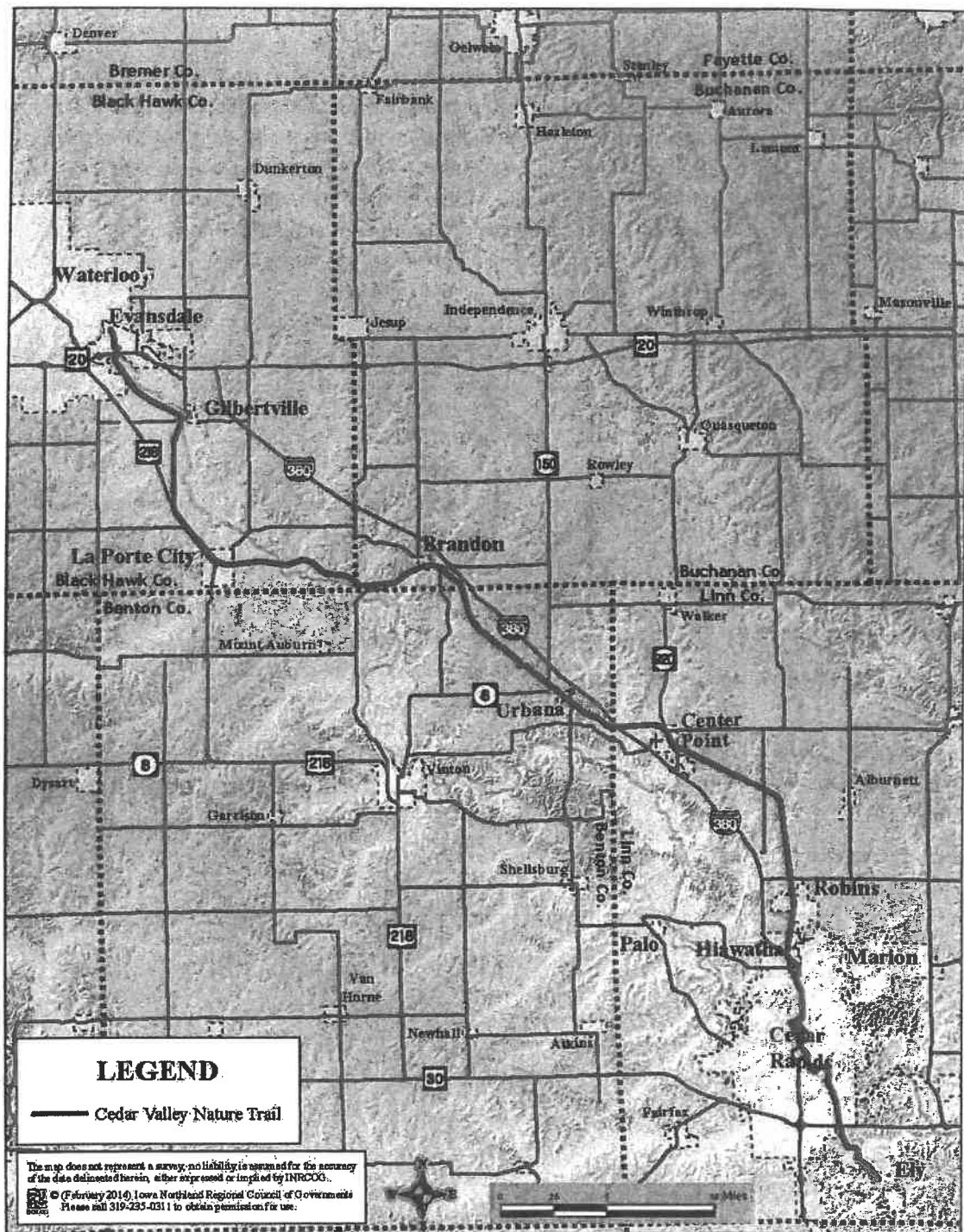
## 4.7 Cedar Valley Nature Trail, Linn County, IA

### Summary

Building this trail was a battle, with concerns ranging from trespassing and robbery to general safety for women and children. The trail bisects agricultural properties, so design measures to avoid conflicts were planned. Other issues such as trees along the trail sometimes prevented sunlight from reaching crops. Farmers have been helpful in allowing access to bridges from property. Many farmers and their families have been seen using this trail, while some are still upset due to feelings that the land should be their own. Rural towns and elected officials have become supportive of the trail, touting economic development benefits, connecting of the trail to local business. The trail abuts 3/4 miles of K&J Squires Farms, and bisects portions of their property. They have an easement allowing their equipment to cross the trail and access their fields.

Trail Features	Specific Design and Management Measures
<b>Trail operator:</b> Linn County Conservation Board <b>Length:</b> 52 miles <b>Trail width:</b> 12' <b>ROW/Corridor width:</b> 100' ROW <b>Trail surface:</b> Paved asphalt, crushed stone <b>Trail use:</b> Pedestrian and bicycles <b>Type of crops and operations:</b> Corn, dry beans, hay, wheat <b>Owner/operator contact information:</b> Dennis Goemaat, Deputy Director, Linn County Conservation Board, Iowa <b>Agricultural owner/operator:</b> Joyce Squires, K&J Squires Farms Inc.	<ul style="list-style-type: none"><li>• Fencing with gates;</li><li>• Reinforced crossing to accommodate equipment;</li><li>• Easement allowing farm equipment access;</li><li>• Signage to warn trail users of crossing farm vehicles.</li></ul>
	Feedback from Involved Parties
	<ul style="list-style-type: none"><li>• Joyce Squires: She and her husband were initially opposed to the trail, with concern about trespassing, but this has not been an issue;</li><li>• Generally the trail has been very positive, she and her family use it;</li><li>• Only problem they have is that occasionally a trail user will not pay attention to the signs and will cut in front of farm equipment on the trail.</li></ul>

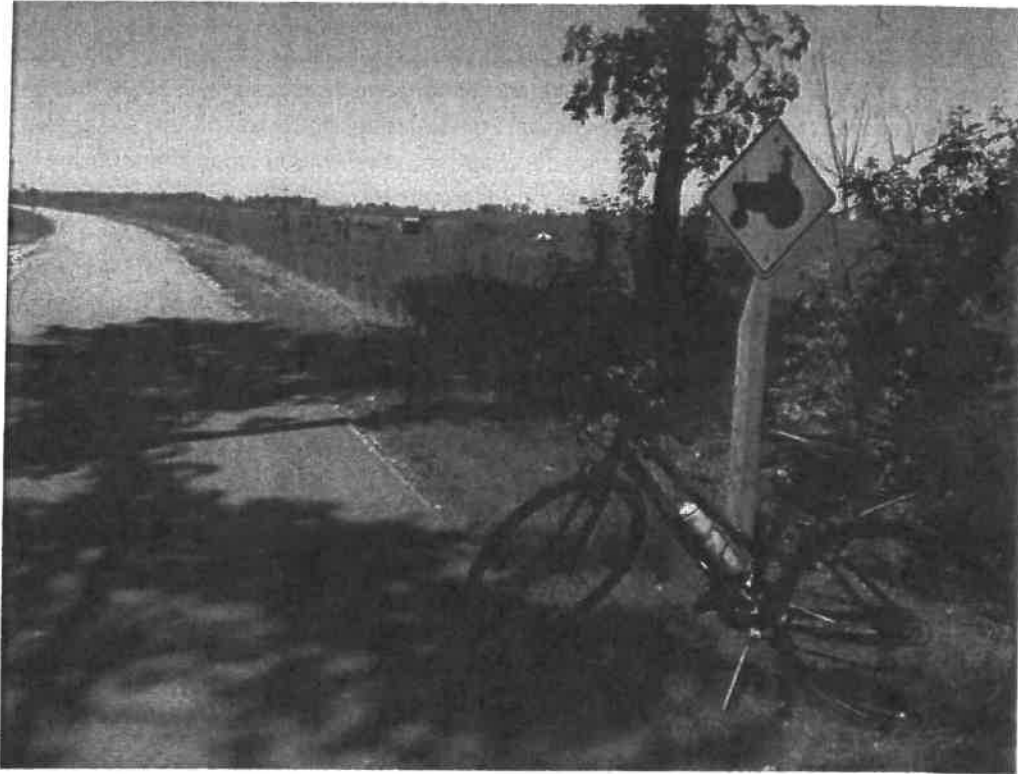




Trail Map

Source: <http://www.co.black-hawk.ia.us/conservation/Publications/maps/CVNT%20Map.pdf>

## Project Photos



*Source: Rails to Trails Conservancy*

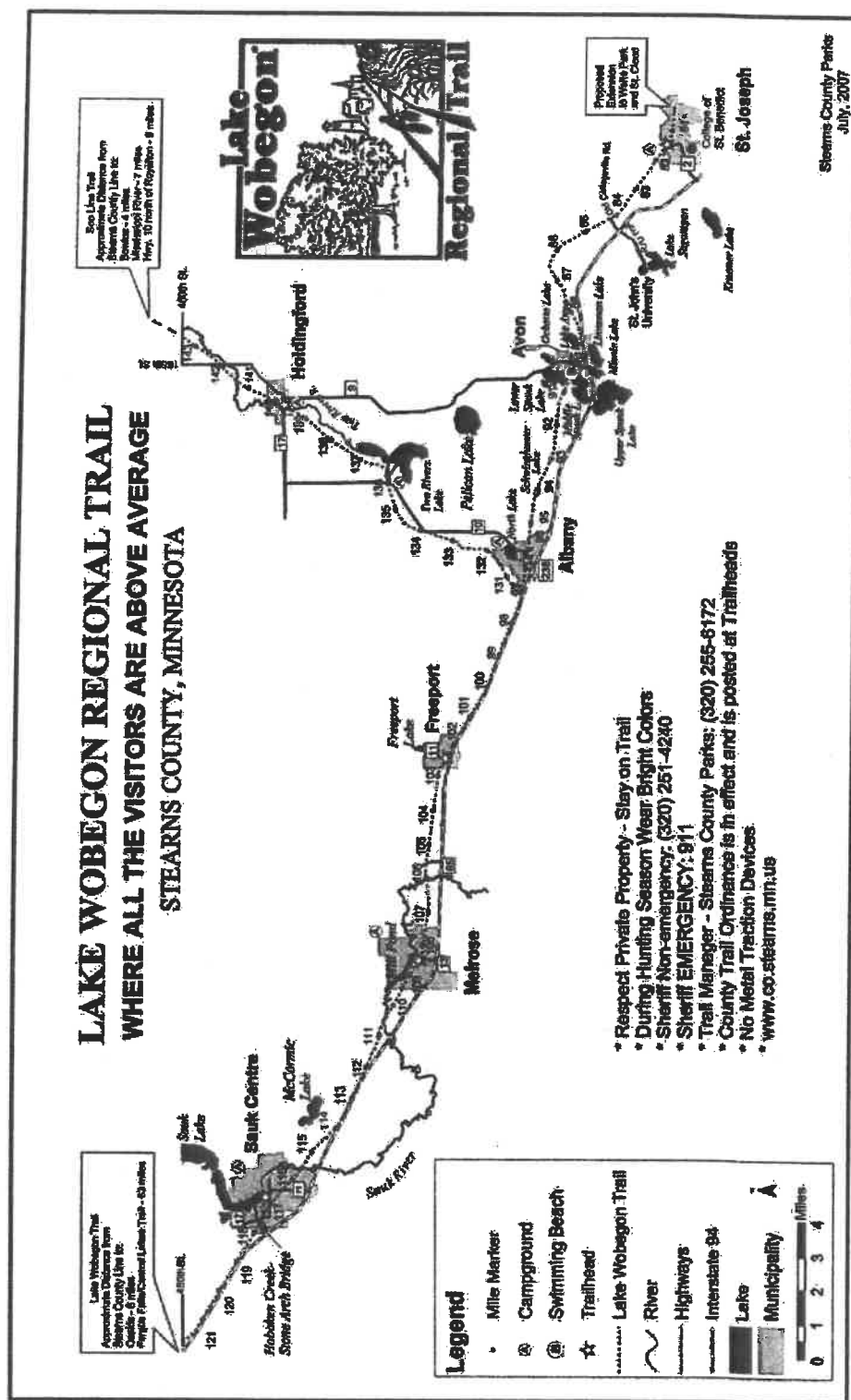
## 4.8 Lake Wobegon Trail, Stearns County, MN

### Summary

Built on a Burlington Northern Railroad corridor, this rail-trail passes the towns of Osakis, St. Joseph, and Albany in Stearns and Todd Counties. The trail opened in 1998 with significant concerns about safety. In 2000-2001 landowners convinced commissioners to build fencing. Farmland is on both sides of the trail. There are generally no gates aside from grazing areas. During the initial phase of development, there was no opposition. During the second phase issues arose when the agricultural commissioner told farmers they would get the underlying property land back for free after the railroad left. There were significant concerns about trespassing, and some portions include a fence of 3-strand barbed wire for livestock and property demarcation. Opposition is now mostly gone, there have been a few people that have expressed concern about spraying for weeds on the trail that might impact crops.

Trail Features	Specific Design and Management Measures
<p><b>Trail operator:</b> Stearns County Parks</p> <p><b>Length:</b> 62 mi</p> <p><b>Trail width:</b> 10'</p> <p><b>ROW/Corridor width:</b> 100'</p> <p><b>Trail surface:</b> Asphalt, crushed stone, gravel</p> <p><b>Trail use:</b> Bicyclists, cross-country skiers, snowmobilers, pedestrians</p> <p><b>Level of use:</b> High on weekends; 100,000-150,000 users measured from April to October 2014</p> <p><b>Type of crops and operations:</b> Corn and soybeans</p> <p><b>Owner/operator contact information:</b> Pete Theismann, Park Director Stearns County Parks, MN; 320-255-6172 parks dept. Lake Wobegon Trail</p>	<ul style="list-style-type: none"> <li>• Yield signs at crossing</li> <li>• Gates where livestock are present</li> <li>• 3 strand wire fence</li> <li>• 40' buffer through most of the corridor</li> <li>• Weekly trail maintenance</li> <li>• Local police are invited to patrol the trail</li> <li>• Trail crossings are minimized. Maintained where existing before the trail, but if new crossings are requested another must be closed</li> </ul>
	Feedback from Involved Parties
	<ul style="list-style-type: none"> <li>• Pete Theismann: Few problems have occurred, more issues are due to encroachment</li> <li>• Erosion with sand covering trails due to trees being cut down by farmers.</li> <li>• No problems reported with agricultural spraying, trespassing or littering from the public.</li> <li>• The trail is far more popular relative to the concerns that have been raised.</li> </ul>

## Trail Map

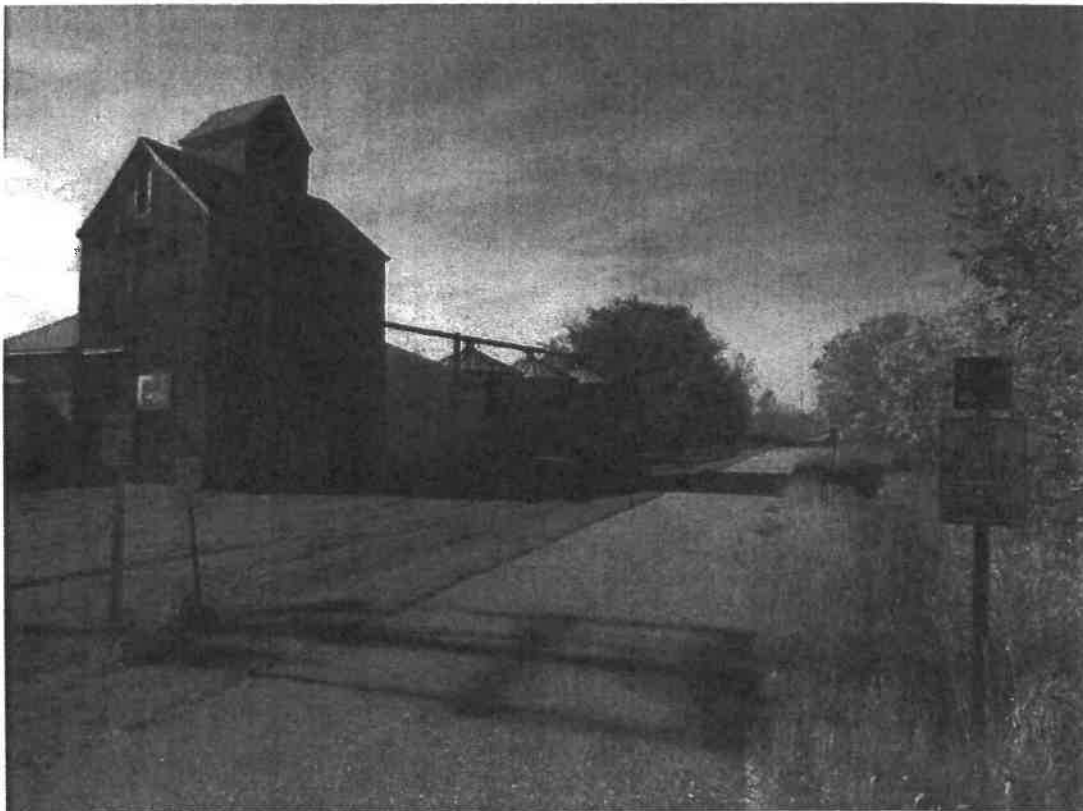


Source: <http://saukcentrechamber.com/files/507.pdf>

## Project Photos



Source: Rails to Trails Conservancy



Source: Barry Weber - <http://lwtrails.com/>

## 4.9 West County and Joe Rodota Trails, Sonoma County, CA

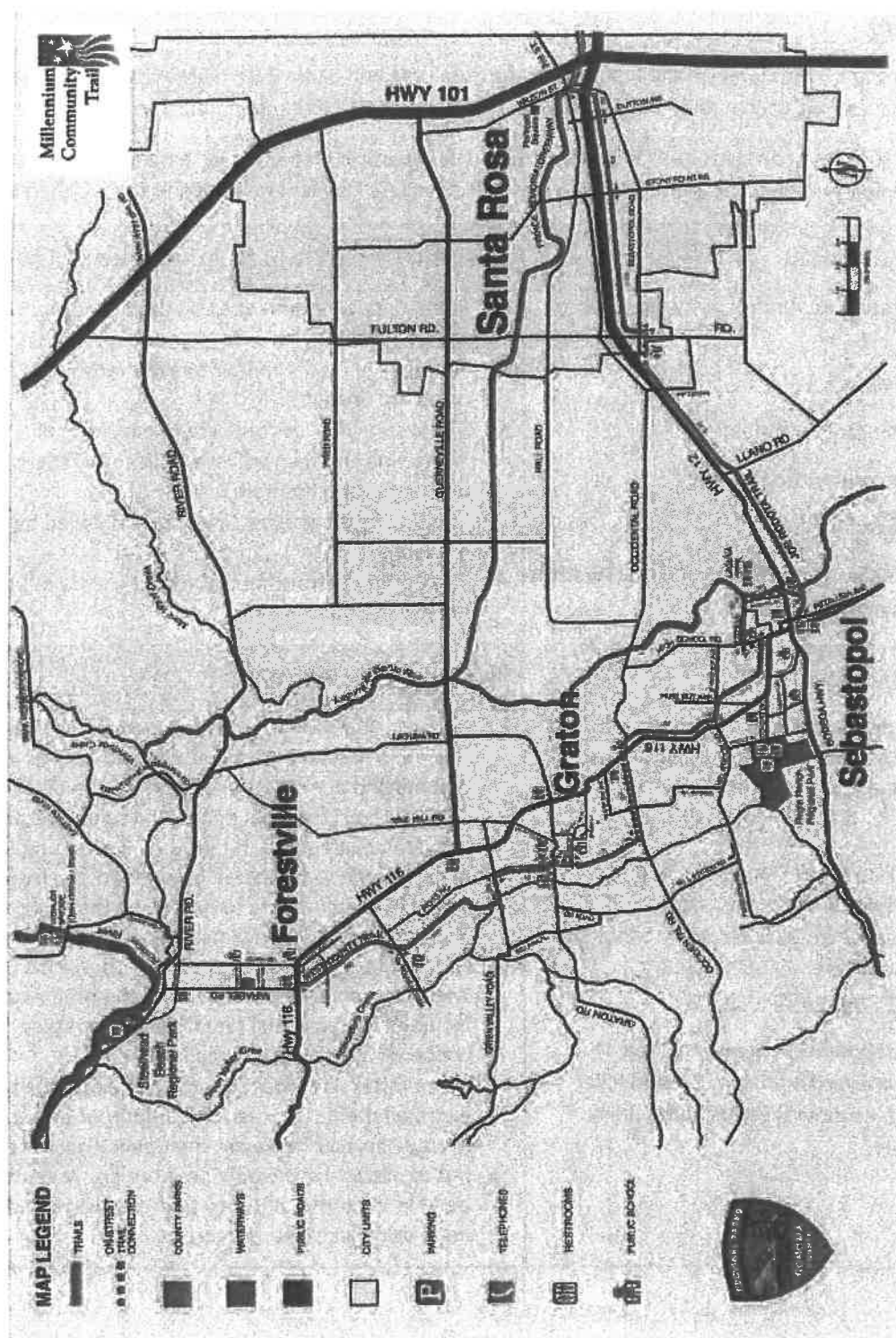
### Summary

These trails are built along land that was once the Petaluma and Santa Rosa Railway, a line that linked Petaluma and Santa Rosa with Sebastopol and Forestville. An unpaved equestrian trail runs parallel to the paved trail.

The most common concerns prior to construction included impacts to spraying activities, crop loss, dogs, and turning radius for agricultural equipment. Vineyards have less frequent maintenance needs than row crops.

Trail Features	Specific Design and Management Measures
<p><b>Trail operator:</b> Sonoma County Regional Parks</p> <p><b>Length:</b> 14 miles</p> <p><b>Trail width:</b> 8' with shoulders</p> <p><b>ROW/Corridor width:</b> 40' – 60'</p> <p><b>Trail surface:</b> Asphalt</p> <p><b>Trail use:</b> Mix of pedestrians and cyclists with limited equestrian use.</p> <p><b>Type of crops and operations:</b> Vineyards, hay, blueberries</p> <p><b>Owner/operator contact information:</b> Bert Whitaker (Maintenance and Operations Chief), Sonoma County Regional Parks. 707-565-2041</p> <p>Kenneth Tam, Park Planner II, Sonoma County Regional Parks Department, 2300 County Center Drive, Suite 120A, Santa Rosa, Ca 95403 Phone: 707-565-3348  <a href="mailto:ken.tam@sonoma-county.org">ken.tam@sonoma-county.org</a></p> <p><b>Agricultural owner/operators:</b> Kendall Jackson, vineyard manager, Russian River Vineyards. Kozlowski Farms, Jam sellers. Daryl Davis.</p>	<ul style="list-style-type: none"> <li>Farmers put A-frame signs on their property stating when spraying will occur.</li> <li>Spraying generally limited to early morning, before most trail users are present.</li> <li>Aerial spraying not conducted near the trail.</li> <li>Some vineyard owners have built connections between their properties and the trail.</li> <li>"No Trespassing" signs have been installed by some vineyard owners.</li> <li>The County patrols the trail and regularly talks with neighbors.</li> </ul>
	Feedback from Involved Parties
	<ul style="list-style-type: none"> <li>Kenneth Tam: The County conducted a record of survey and title search, then reached out individually to agricultural land owners and operators who appeared to be using the railroad ROW without the legal right to do so and requested they provide documentation that they were using the ROW legally. None were able to provide documentation. There was a blueberry farmer using the railroad ROW to access his crops. The farmer has since opened a stand along the trail to sell blueberries and blueberry ice cream. The main concerns voiced during the planning stages included the potential for crime and trespass. These concerns have not been realized.</li> <li>Bert Whitaker: Some farmers have asked for temporary encroachments (e.g., to run equipment across the trail during harvest); however, the County has taken the stance not to allow this. It would be more convenient for farmers to be able to do this, but they find they can get the access the need using just their properties.</li> </ul>

## Trail Map



Source: [http://parks.sonomacounty.ca.gov/uploadedFiles/Parks/Get\\_Outdoors/Parks/westcountyand%20joerodota\\_map\\_2012.pdf](http://parks.sonomacounty.ca.gov/uploadedFiles/Parks/Get_Outdoors/Parks/westcountyand%20joerodota_map_2012.pdf)



## Project Photos



Source: Rails to Trails Conservancy



Source: Rails to Trails Conservancy

## 5 OTHER TRAILS

The following trails were identified as potential candidates for further study, but did not meet as many criteria as the trails detailed in the previous section. The following trails all feature agricultural adjacencies, but are presented in summary form here due to inability to contact operators, inapplicable agriculture types, lower levels of use, lower levels of trail development and operations, and geographical distance from Ventura County. Lessons to be learned from these trails reinforce information gathered for the focus trails, and the following trails can provide additional guidance for trail planning and conflict mitigation, particularly regarding interactions between individual farmers and trail operators.

### JOHN WAYNE PIONEER TRAIL

King and Kittitas Counties, WA

This is a gravel trail over an old rail bed that features bicycling and equestrian activities. It is owned and operated by Washington State Parks and Lake Easton State Park and was established in 2002. Between Beverly and the Columbia National Wildlife Refuge the trail passes through Crab Creek Wildlife area. About 110 acres of WDFW land on the east end is leased for farming. The major crops in the eastern and northern Crab Creek Sub basin are cereal grains. Agriculture within the irrigation project is more diverse and crops include alfalfa, wheat, corn, potatoes, various tree fruits and many different seed crops. Vineyards and pulp farms have begun to appear recently. The trail is part of Iron Horse State Park - 240 miles total. John Wayne Pioneer Trail is one of 4 or 5 total. The majority of the trails run through agriculture. They operate a "good neighbor policy" with the farmers - crossings are established, and many (70%) were grandfathered in from when the railroad operated. There is usually no fee for farmers to cross. Most farmers own land on both sides of the trail. When the trail was established there was heavy opposition, primarily to return the land to farmers, since rail was removed. Since establishment, most requests from farmers have been able to be addressed (85%) - crossings and access. The state is developing new policy now to handle this interaction. Complaints are usually regarding dumping - people break the gates and leave car bodies or other large junk on the ROW - farmers usually call just because they don't want to look at it. No issues of break-in to farmland.

The trail is unpaved, and farming operations are mainly grains. The trail itself is within a state park.

There were extensive meetings with farmers, and crossing agreements were put in place. Requests from farmers are handled on an individual basis, and are almost always related to crossings.

**Trail operator and contact information:** Steve Hahn, Property Management Program Manager, Washington State Parks.

### MULLET HALL EQUESTRIAN TRAIL SYSTEM

Charleston County, SC

Soil trail used by pedestrians and equestrians. It is owned and operated by Charleston County Park & Rec and was established in 2005. The trails meander throughout the historic fields of the former Mullet Hall Plantation and the system boasts a swamp, active farm fields, deep forest, and meadows. The trail does go through and is adjacent to active farming of one farmer who usually grows grain. There was no conflict during the development of the trail, as the trail director and farmer had a close relationship.

No conflict between uses due to unique land ownership scenario. Soil Surface.

## **CONEWAGO RECREATIONAL TRAIL (Connects To Lebanon Valley Rail-Trail)**

Lebanon and Lancaster County, PA

A crushed rock/compacted surface trail that accommodates cyclists, equestrians, and pedestrians, the trail is owned and operated by Lancaster County Department of Parks and Recreation-Lebanon Valley Rails-to Trails, Inc. It was established in 2004. Agriculture includes horse farms specifically mentioned along trail, and possibly corn, soybean, alfalfa grown in the watershed area in proximity to the trail, but no specifics of crops interacting with the trail. The majority of the Conewago Creek watershed is in agricultural production (approximately 53%) with many of the main stem and tributary floodplains actively pastured or cultivated for crop production. There is private farmland along the trail, but contact had no information about it. Mainly pasture adjacent to the trail.

## **OAK CREEK TRAIL**

Butler and Saunders County, NE

A crushed limestone trail for bicycling, equestrian activities, walking, roller/inline skating, cross country skiing, and snowshoeing, the trail is owned and operated by Lower Platte South and was established in 2007. Resources state that "the route continues through natural prairie, open farmland and oak woodlands until the trail reaches its endpoint at the trailhead in the town of Valparaiso." Contact with the operator's office confirmed fields are corn, soybeans and other grains, but no orchards. No reports of conflicts with the farmers in the area.

## **ICE AGE TRAIL**

Statewide, WI

Ice Age Trail Alliance owns and operates this trail, which passes through farmland. Approximately 650 miles of trails, most of which is through agricultural lands. Multiple agreements are made with farmers, worked out one at a time. There are all kinds of agriculture, but more crops than grazing. Portions of the trail are rail-trail. Some issues include very narrow corridors left by farmers and also many areas are on farmers' land. The trail operators have brought landowners together to fill gaps in the trail and have worked with each farmer to ensure farms remain viable. They have also purchased easements, going well with farmers. Farmers sometimes disliked the trail and were upset at the lack of ability to drive the length of the rail line, which was not legal prior to the trail's development, but crossings were provided to alleviate the conflicts. An example was given of a farmer whose land was acquired with an easement and had to modify his practices somewhat, but it worked out. There was also a band of landowners who wanted to buy out a portion of rail line so trail couldn't go in, which went to court and the landowners lost. Ice Age has an elaborate planning process that takes many years that includes a lot of community outreach in order to slowly build support. It is, for most of the trail's distance, a narrow footpath, rather than a heavy-use paved trail.

Individual agreements were arranged with farmers and communities, formed over decades. Many stories about these agreements are available from the trail operator.

**Operator contact:** Kevin Thusius, Director of Land Conservation (800) 227-0046 - [kevin@iceagetrail.org](mailto:kevin@iceagetrail.org)

## **HANOVER TROLLEY TRAIL**

York County, PA

Owned and operated by York County Rail Trail Authority. Portions of the trail were constructed in 2008, while others were scheduled for 2013, but have not yet been constructed. There was opposition from farm operators during the feasibility study for the non-constructed portions.

**STAVICH BIKE TRAIL**

Mahoning (OH) & Lawrence (PA) Counties, PA and OH

An asphalt paved greenway and rail trail that is owned and operated by Lowellville Hillsville Charitable Foundation and Lawrence County Tourism. It was established in 2003. While there are agricultural fields in the area, they do not directly come in contact with trail itself.

**MACOMB ORCHARD TRAIL**

Macomb County, MI

This trail was built on former orchard land. Current agricultural adjacencies are minimal, and the trail is inside a park.

**LATAH TRAIL**

Moscow, ID

This trail is owned and operated by Latah Trail Foundation. It passes near, but not directly adjacent to agricultural areas, and does not conflict with them.

**ROW RIVER TRAIL**

Lane County, OR

Asphalt paved equestrian, fitness, and mountain bike trail. The trail is also considered a nature trail, rail trail, and urban trail. It is owned and operated by the Eugene Bureau of Land Management and was established in 2005. It passes through "pastoral farms" but these are historical farmhouses, not active agricultural production areas.

**MONTEREY BAY SCENIC SANCTUARY TRAIL**

Santa Cruz County, 3 miles east of Santa Cruz, CA

This is an unpaved beach path that approaches row crops. A short stretch of the trail approaches farmland. Despite this limited agricultural interaction, the trail's master plan EIR includes detailed mitigation measures for trails passing near farmland. These measures are included in Appendix B.

**HENNEPIN CANAL PARKWAY**

Bureau, Henry & Whiteside Counties, IL

This trail is partially paved and partially natural surface, and is used by cyclists, equestrians, snowmobiles, pedestrians, and cross-country skiers. It is operated by the Illinois Department of Natural Resources and established in 2004. It passes through "rolling farmland," that consists predominantly of grazing land.

**HARLEM VALLEY RAIL TRAIL**

Dutchess and Columbia Counties, NY

This trail is paved, and used by bicyclists, pedestrians, roller/inline skaters, cross-country skiers, and snowshoers. It is operated by the Harlem Valley Rail Trail Association. The trail passes dairy farms and grazing land. Adjacent agriculture is grazing and dairy production.

**ARUNDELL BARRANCA BIKE PATH**

Ventura County, Unincorporated, CA

Established prior to 1999, this trail is a paved bicycle and walking trail that passes row crops. It follows a drainage channel, and is only minimally adjacent to crops.

### **VENTURA RIVER TRAIL (Ojai Valley Trail Extension)**

Ventura County, Ventura, CA

Also called Ventura River Parkway Trail, this paved bicycle, equestrian and pedestrian trail has a short segment (1/4 mile long) that runs along row crops.

### **RUSSELL BOULEVARD BIKE PATH**

Yolo and Solano Counties, unincorporated; between Davis and Winters, CA

This paved bicycle and pedestrian trail passes row crops, horse pastures, and nut tree orchards. Directly adjacent agriculture is predominantly pasture land.

### **RACCOON RIVER VALLEY TRAIL**

Dallas County, IA

This trail is operated by the Dallas County Conservation Board, Guthrie County, and Greene County, and covers 88 miles, some of which is adjacent to farmland growing corn and soybeans. The trail surface is asphalt and concrete, with unpaved segments. Trail users include bicyclists, inline skaters, snowmobiles, pedestrians, and cross-country skiers. It receives approximately 125,000 visitors per year. Major concerns during development on this trail were trespassing and occasional snowmobile activities. This never became an issue. There has been a close working relationship between the trail and adjacent landowners, which has resulted in 99% cooperation, with the occasional encroachment on the trail by farmers. Fencing, maintained by the Conservation Board, is in place for grazing livestock. Enhanced crossings were installed for farm equipment, with signs indicating trail users to yield to farm equipment.

### **LAKELANDS TRAIL**

Ingham, Livingston, and Washtenaw Counties, MI

The only opposition to the trail was an onion farmer. Other adjacent farmers, with orchards and soybean crops, did not express concern. The Michigan Department of Natural Resources worked with Michigan State University to address concerns. The trail is 26 miles long, with a surface that varies between asphalt, ballast, and crushed stone.

### **HART-MONTAGUE TRAIL**

Muskegon and Oceana Counties, MI

An asphalt rail-trail that passes orchards and soybean crops, this trail runs for 22.7 miles, and is managed by Michigan Trails and Greenways.

### **CATSKILL SCENIC TRAIL**

Delaware County, NY

This rail-trail opened in 1997, is 26 miles long, with cinder, crushed stone, and natural surfaces. It sees heavy equestrian use. Barbed wire fencing separates the trail from adjacent cornfields. User groups include cross-country skiers, horseback riders, bicyclists, snowmobilers, pedestrians. Primary crops include feed corn and livestock. The trail is occasionally used to move livestock between fields. Dan Riordan, Executive Director of the Catskill Revitalization Corporation, the trail management agency, reports that farmers do cross trail with tractors and ride along the trail for short distances, and this has not been a problem. There have been no trespassing issues on farms.

## **NORWOTTUCK RAIL TRAIL**

Hampshire County, MA

This rail-trail is 14.9 miles long and has an asphalt surface. Corn fields are adjacent to portions of the trail. Bob Clark, of the Massachusetts Department of Conservation and Recreation, reports minimal trespassing issues. Occasionally local youth cut through farms to reach the nearby mall.

## **6 APPENDIX A – SAN DIEGUITO RIVER VALLEY JPA INDEMNIFICATION RESOLUTION**

### **RESOLUTION NO. \_\_\_\_\_**

#### **A RESOLUTION OF THE BOARD OF DIRECTORS OF THE SAN DIEGUITO RIVER VALLEY REGIONAL OPEN SPACE PARK JOINT POWERS AUTHORITY ADOPTING DESIGN INDEMNITY RE MULE HILL TRAIL**

**WHEREAS**, the San Dieguito River Valley Regional Open Space Park Joint Powers Authority ("JPA") is applying for a Site Development Permit ("Permit") from the City of San Diego for the construction of a 9.4 mile trail (the Trail) in the San Pasqual-Lake Hodges Community Planning Area which is a portion of the Trail; and

**WHEREAS**, conditions 21 through 23 of the City of San Diego ("City") Permit, impose certain design criteria for the design and construction of the Trail; and

**WHEREAS**, the JPA desires to design and construct portions of the Trail contrary to the normally applicable City requirements for setback and separation from the roadway, and such deviations from the normal design standards have been approved by the City conditioned on the JPA providing the City with design immunity; and

**WHEREAS**, the City and the affected leaseholders have requested indemnity for any expenses associated with a lawsuit brought against them by any person as a result of the design and construction of the Trail; and

**WHEREAS**, on June 16, 2000, the Board of Directors of the JPA adopted Resolution No. R00-7 agreeing to provide indemnification to the City and its agricultural leaseholders adjacent to the Trail as set forth in said Resolution, for any expenses associated with a lawsuit brought against them by a Trail user that may occur despite the broad array of statutory immunities; and

**WHEREAS**, the JPA desires to further indemnify the City and its affected leaseholders for any expenses associated with a lawsuit brought against them by any person as a result of the design and construction of the Trail as set forth in the Resolution.

**THEREFORE BE IT RESOLVED THAT**, in exchange for approval of the alternative design and construction of the Trail, the JPA shall provide the following additional indemnity and insurance coverage:

**1.1** The JPA shall defend, indemnify, protect, and hold harmless the City, its agents, officers and employees, from and against all claims, demands, causes of action, liability or loss



asserted or established for damages or injuries to any person or property arising out of the design, construction and maintenance of the Trail. Claims, demands, causes of action, liability or loss that arise from, are connected with, or are caused or claimed to be caused by the acts or omissions of the JPA, the JPA's agents, officers and employees with respect to the design, construction and maintenance of the Trail are covered. Also covered are the claims, demands, causes of action, liability or loss arising from, connected with, caused by, or claimed to be caused by the active or passive negligent acts or omissions of the City, its agents, officers, or employees which may be in combination with the negligence of the JPA, its employees, agents or officers, or any third party. The JPA's duty to defend, indemnify, protect and hold harmless shall not include any claims or liabilities arising from the established sole negligence or sole willful misconduct of the City, its agents, officers or employees.

1.2 The JPA further agrees that the indemnification agreement referred to in Section 1.1 and the duty to defend the City require the JPA to pay any costs the City incurs that are associated with enforcing the indemnification provision, and defending any claims arising from the design, construction and maintenance of the Trail. If the City chooses, as its own election, to conduct its own defense, participate in its own defense or obtain independent legal counsel in defense of any claim related to work provided under this Agreement, the JPA agrees to pay the reasonable value of attorneys' fees and all of the City's reasonable costs to the extent covered by the JPA's insurance.

2. The JPA shall maintain a policy of public liability and property damage insurance, in which the City is named as an additional insured and secured in an amount of not less than \$5 million.

3. All provisions of the indemnification agreement adopted by Resolution No. R00-7 remain in effect, except for #4, provided that the claimant/employee, agent, invitee or relative of the indemnified party was injured or damaged as a result of the alternative design, construction, or maintenance.

PASSED AND ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2001, by the following vote: \_\_\_\_\_ AYES; \_\_\_\_\_ NOES; \_\_\_\_\_ ABSENT; \_\_\_\_\_ ABSTAINED

CHAIR, SAN DIEGUITO RIVER VALLEY  
REGIONAL OPEN SPACE PARK JPA BOARD  
OF DIRECTORS

ATTEST:

CLERK, SAN DIEGUITO RIVER VALLEY  
REGIONAL OPEN SPACE PARK JPA BOARD  
OF DIRECTORS

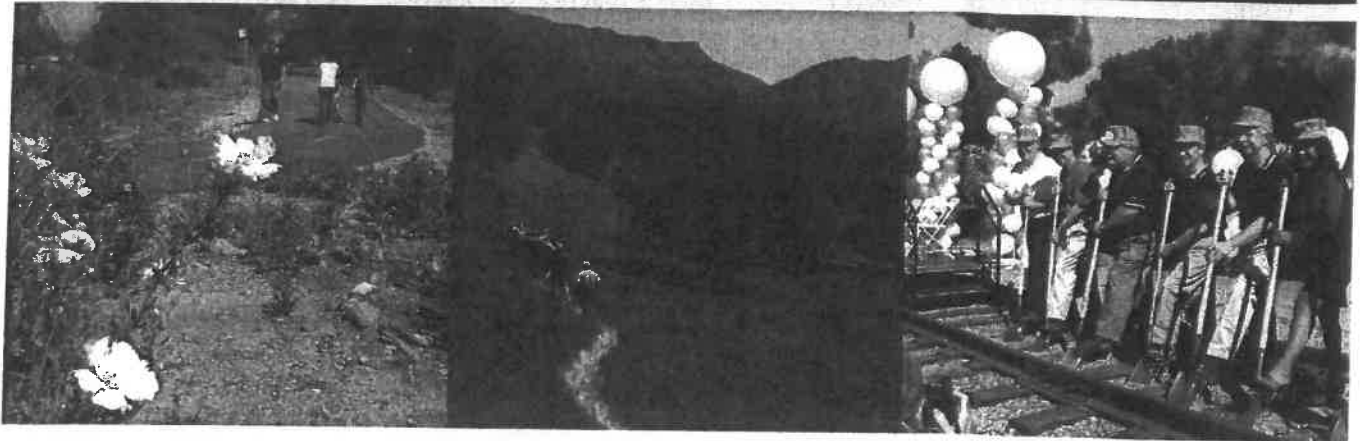
# 7 APPENDIX B – MONTEREY BAY SANCTUARY SCENIC TRAIL NETWORK MASTER PLAN EIR MITIGATION MONITORING AND REPORTING PROGRAM

Mitigation Measure/Condition of Approval	Action Required	When Monitoring to Occur	Monitoring Frequency	Responsible Agency or Party
<b>AGRICULTURAL RESOURCES</b>				
<b>AG-1(a) Placement of Fencing:</b> Placement of fencing shall be located in a manner which minimizes impacts related to accessibility to farmland and use of farming equipment (e.g., allowing turning radius area for farm equipment).	Review construction plans	During plans, specifications, and estimates for each segment	Once for each segment	Implementing Entity and/or RTC
<b>AG-3(a) Notice of Agricultural Activities.</b> The following information shall be added to the proposed notices on on-going agricultural activities: <ul style="list-style-type: none"> <li>Trail users are advised to stay on the trail and be alert to operating machinery and equipment near the trail.</li> <li>Trail users are required to use restroom facilities in consideration of food hygiene issues on adjacent agricultural lands.</li> <li>Where dogs are not prohibited, trail users are required to clean up after their dogs and prevent trespass by dogs on adjacent agricultural properties in consideration of food hygiene issues on adjacent agricultural lands.</li> <li>The legal ramifications for trespassing on adjacent properties.</li> <li>The legal ramification for trespassing or being on the trail after it is closed.</li> </ul>	Install signs along trail	Prior to trail opening	Once for each segment	Implementing Entity and/or RTC
<b>AG-3(b) Landscaping Coordination.</b> For segments adjacent to agricultural operations in the northern and Watsonville reaches, any ornamental plant material used along the trail shall be comprised of native and indigenous species. The selected plant palette shall be reviewed by the Agricultural Commissioner's office prior to approval of landscape plans. Any plant material which may host pests destructive to agriculture shall be prohibited.	Review landscaping plans	During plans, specifications, and estimates for each segment	Once for each segment	Implementing Entity and/or RTC, Agricultural Commissioner
<b>AG-3(c) Chemical Spraying Impact Reduction Options.</b> On a case-by-case basis, the RTC and/or implementing entity for segments adjacent to agricultural operations shall work with the Agricultural Commissioner's office and adjacent farmers to reduce impacts to trail users from agricultural spraying, including pesticides. Non-buffer options shall be considered, including the use of alternative methods of pest and weed control and/or an agreement that farmers notify the Agricultural Commissioner's office or Trail Manager in advance of proposed agricultural spraying within 100 feet of the trail. This would allow the Agricultural Commissioner's office, in accordance with existing requirements, to inform the RTC and/or implementing or managing entity of all spraying within 100 feet of the trail so that appropriate action can be taken (e.g., posting notices or closure of that segment of the trail).	Coordinate with Agricultural Commissioner's office and adjacent farmers to consider non-buffer spraying reduction options	As needed	As needed	Implementing Entity and/or RTC, Agricultural Commissioner

Document source: <http://www.sccrtc.org/wp-content/uploads/2013/05/MBSST-MMRP-Final.pdf>

# Benefits of Trails & Greenways

National Park Service  
U.S. Department of the Interior  
Rivers, Trails & Conservation  
Assistance Program PWR



Footpaths and trails have been an integral part of America's landscape. Native Americans hunted animals by following their trails through forests and across open prairies. Over time, these narrow paths became trade routes and established transportation corridors. Later, the western frontier was explored and expanded largely by trail. Canals and towpaths brought settlers and goods across the Appalachian Mountains. Pioneers following Indian pathways opened up the Northwest Territories. The Oregon Trail led wagon trains of pioneers to the Pacific Northwest, while El Camino Real linked Spanish Missionaries to the California coast. America's highway system and streets of towns and cities more often than not follow many of these early trails and footpaths that started as animal track through North America's forests, mountains and prairies.

Today, for millions of Americans, trails are a link to the outdoors, providing opportunities for recreation, exercise and transportation. Many are realizing the benefits from having trails and greenways close to home. The following is an overview of the range of benefits that result from having these amenities woven into the fabric of communities across America.

## Recreation

One of the most direct benefits of community trails and greenways is the close to home, accessible recreation opportunities they provide.

- Trails make communities better places to live by preserving and creating open spaces for recreation.
- Trails and greenways provide new opportunities for outdoor recreation and non-motorized transportation.
- Trails also provide a unique facility to serve a diverse population of a community that may otherwise have limited opportunities to access natural areas due to financial or transportation constraints.
- Trails provide affordable exercise and recreational opportunities within the community.

## Transportation

In addition to being corridors for recreation, trails and greenways can function as non-motorized transportation corridors for pedestrians and cyclists.

- One-fourth of all trips people make are one mile or less, but three-fourths of these short trips are made by car (Centers for Disease Control and Prevention, 2000).
- For school trips one mile or less, only 31% are made by walking; within two miles, only 2% of school trips are made by bike.
- Trails and greenways link neighborhoods with shopping and entertainment districts and provide pleasant transportation alternatives for commuting to work and school.

## Healthy Living

Many people realize exercise is important for maintaining good health in all stages of life; however many do not regularly exercise. The U.S. Surgeon General estimates that 60% of American adults are not regularly active and another 25% are not active at all. Trails and greenways provide a safe, inexpensive avenue for regular exercise for people living in rural, urban and suburban areas. There is scientific evidence that supports providing convenient access to places for physical activity, such as trails connecting to parks or other recreational facilities, increases the level of physical activity in a community.

- In California, approximately one out of every two adults is overweight or obese. One out of every three children (ages 9 to 11) and one out of four teens are at risk due to being overweight or obese.
- Obesity and physical inactivity were estimated to cost California nearly \$25 billion in health care costs and lost productivity in 2000.
- People are more likely to be physically active if they have recreational facilities close to their homes.

## Economics and Property Appreciation

Trails are becoming common in residential neighborhoods. Development plans for homes, apartments, and townhouses often include footpaths to enhance recreational opportunities and property values. Urban trails are regarded by real estate agents as an amenity that helps to attract buyers and to sell property. Trails are considered lifestyle enhancements and are usually included in the sales package for a property.

- Property adjacent to/nearby trails can be an amenity that attracts buyers and makes the property easier to sell.
- Studies in Denver, Seattle, Minnesota and other communities across the country have consistently found that that proximity to trails increases the value of homes from 1-6.5%.
- In 1990, more than 197,000 hours per day were being lost due to traffic congestion, costing California businesses more than \$2 million a day.

## Environment

Most directly, greenways protect important habitat and provide corridors for people and wildlife. However, trail corridors can also assist in providing natural buffer zones from pollution run-off and become important tools for improving water quality. They can also improve air quality by protecting the plants that naturally create oxygen and filter out air pollutants.

- Greenways function as a "buffer" between the built and natural environments.
- The value of open space to the public is enhanced by providing access.
- Greenways function as hands-on environmental classrooms for people of all ages.
- Trails provide enjoyable and safe options for transportation, which reduces air pollution.

## Safe and Livable Communities

Trails promote safe and livable communities. The recreation, health, transportation and environmental benefits collectively can contribute to an overall enhanced quality of life in communities. Another variable to consider is the impact of trails and greenways on safety. While many believe that trails promote crime, a review of four separate studies conducted between 1979 and 1997 concluded that rail-trails do not increase crime and that rates of crime on suburban rail-trails is lower than overall suburban crime (Rails-to-Trails Conservancy 1998).

- Trails and greenways can reduce crime and illegal activity through regular use and high visibility of users.
- Trails and greenways provide informal opportunities to meet and interact with neighbors.
- Integrating unique features such as trails and greenways into communities provide a sense of place and community pride.
- Neighborhoods where children can safely walk or bike to a park, school, or to a neighbor's home are generally also good places to live.

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September 9, 2020

RE: Yamhelas Westsider Trail

To Yamhill County Commissioners

The Carlton City Council and our community continue to support the development of the Yamhelas Westsider Trail. This trail will be a vital connection for the communities of Carlton, Yamhill, and Gaston.

Highway 47 is currently the only suitable connection between our communities and is not safe for biking or walking. There have been many close calls and accidents from the lack of a safe way to walk or bicycle in and between our communities. The trail will give us a much-needed connection for Carlton and Yamhill students, especially between the schools and homes in our school district.

We look forward to the development of the Yamhelas Westsider Trail and the improvements to our community that will follow.

Sincerely

Brian Rake  
Mayor  
City of Carlton

*The Mission of the City of Carlton is to safeguard and enhance the vitality and livability of the community by providing essential services with professionalism and integrity.*

## Trail studies and information available online for CPRD

Santa Paula Branch Line Study:

<https://www.americantrails.org/images/documents/VCTCSantaPaulaBranchLineStudy.pdf>

Benefits of Trails and Greenways: <https://www.railstotrails.org/resourcehandler.ashx?id=2988>

Rail Trails and Safe Communities: [https://safety.fhwa.dot.gov/ped\\_bike/docs/rt\\_safecomm.pdf](https://safety.fhwa.dot.gov/ped_bike/docs/rt_safecomm.pdf)

Oregon Outdoor Recreation Economic Impact:

<https://industry.traveloregon.com/resources/research/oregon-outdoor-recreation-economic-impact-study/>

Oregon Bicycle Tourism Economic Impact: <https://bikeportland.org/2021/01/20/bicycling-for-recreation-contributed-1-5-billion-to-oregon-economy-in-2019-324967>

Napa Valley AgRespect: [www.agrespect.org](http://www.agrespect.org)

Trail Construction Costs: <https://ssti.us/wp-content/uploads/sites/1303/2013/10/PedBikeCosts.pdf>

Maintenance costs: <https://www.railstotrails.org/resourcehandler.ashx?id=6336>

<https://www.railstotrails.org/resource-library/resources/maintenance-practices-and-costs-of-rail-trails/>

Case studies: <https://www.railstotrails.org/build-trails/trail-building-toolbox/funding/maintenance-funding>

Health cost savings from trails: [https://www.researchgate.net/publication/7883301\\_A\\_Cost-Benefit\\_Analysis\\_of\\_Physical\\_Activity\\_Using\\_BikePedestrian\\_Trails](https://www.researchgate.net/publication/7883301_A_Cost-Benefit_Analysis_of_Physical_Activity_Using_BikePedestrian_Trails)

**Subject:** Meeting Oct. 28th

**Date:** Sunday, October 3, 2021 at 11:43:35 AM Pacific Daylight Time

**From:** Tom Hammer

**To:** Kat Ricker

**CC:** Mary Starrett, Lindsay Berschauer, 'Casey Kulla'

Kat,

*DON -*

I'm Tom Hammer, involved since 2015 in the Yamhelas Trail proceedings. I would like to be on the program for the CPRD Board meeting October 28<sup>th</sup>. As one of about 30 plaintiffs that challenged Yamhill County concerning the abandoned rail line I can explain the legal actions involving the Land Use Board of Appeals and the difficulties CPRD Board and Staff would have as they ponder taking on the project. I'm not a Facebook guy, so my perceptions of the general knowledge on this topic are confined to what I have read in newspapers and observed at public hearings. There seems to be pretty widespread confusion on the YWT and that shouldn't be. I would be glad to meet with Board members ahead of time so they would know what I would say in a meeting if there is skepticism at having a plaintiff presenting the history of this project.

Tom Hammer 971-382-5133



## **CPRD Board and Management Executive Summary**

**From**

**CPRD Pickleball Steering Committee**

**October 21, 2021**

**Accomplishments, Issues and Actions**

### **Accomplishments:**

1. Pickleball Steering committee formed 10-18-20 with 6 members. Hunter Wylie elected as President and Nick Konen as Secretary
2. Assignments made for key tasks to be accomplished in the next two weeks
3. Meetings scheduled for every two weeks

### **Issues:**

1. Applying for Grants:
  - SCORP (LGGP)750K and Tourism 750K – availability and priority of matching funds in 2022 CPRD budget for any grants to support the Pickleball complex
  - Current grant slots available for this project
2. Steering Committee property recommendations for the Pickleball Complex – Identification, evaluation process, data supporting recommendations

### **Actions:**

1. Establish liaison between CPRD board and Pickleball Steering committee
2. Set timeline for resolving all time critical issues identified above “Issues” since grant windows are closing

Members of the Pickleball Steering Committee can be available at the CPRD Board meeting to address any issues if the Board or CPRD management desires.

Respectfully,

Hunter Wylie

Chairman, Pickleball Steering Committee





## CPRD Pickleball Steering Committee Minutes

October 18, 2021

Meeting Began at 7:05 at Aquatic Center

Six members attending: Linda Sandberg, Nick Konen, Sheryl Greiner, Mike Kringlen, Lance Trantham, and Hunter Wylie

1. Began individual introductions highlighting relevant background and experience
2. Held elections for Chairman - Hunter Wylie nominated by Mike Kringlen, seconded by Lance Trantham and voted in by unanimous vote
3. Held elections for Secretary – Nick Konen nominated by Sheryl Greiner, seconded by Linda Sandberg and voted in by unanimous vote
4. The committee brainstormed which local groups and individual stakeholders to contact regarding the project. Assignments were made to be conclude in the next 2 weeks

### Assignments:

Mike and Linda will attend Leadership Chehalem Valley meeting focused on tourism in the Chehalem Valley on 10/19

Hunter will reach out within two weeks to Scott Parker, Executive Director of Chamber Board

Linda will reach out and establish contact for Hunter with Karen Saunders, Lead for the committee who got the Aquatic Center Built

Nick will attend next meeting of Young Professionals of Yamhill County to network and discuss the project

---

# Pickelball

## A year round opportunity

*"Build it and they will come"*



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SEPTEMBER 27

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## **Newberg Pickleball Club**

### **Progress to date:**

- Fully organized as a 501c7 sports club
- Promoted CPRD Pickleball steering committee - 9/23 start
- Financial accounts and Fees created
- Two regional tournaments completed
- Joined The Columbia River Pickleball Club a 501c3 as a chapter
- Investigating major grant opportunities
- Working with tournament organizers to ascertain realistic schedules and capacities
- Analyzing various locations for the new complex
- Continued local growth in the sport at all levels particularly seniors

### **Strategy:**

- Make strategic alliances with large existing Pickleball organizations made up of core players -- Columbia River Pickleball Club – 971 members – 5 clubs
  - Vancouver, Camas/Washougal, West Linn, Wilsonville and Newberg
  - Lock in our potential paying users
  - Expand to other local areas – Lake Oswego, McMinnville, Salem, Beaverton, etc.
  - Drive facility utilization on a fee per court per hour basis
- Build it to handle all tournament sizes, training facilities, club and group schedules year round
- Drive the central message - We are the singular place to play Pickleball in the Northwest year-round. Why? We will be the only covered complex in PNW.
- Drive incremental new business to the community from Pickleball and Tourism
- Serve the community by generating revenues in excess of all cost to build other CPRD facilities

## **Parks Activity Report, September/October 2021**

### **Sander Estate Park**

Dundee City Council approved the Sander Estate Masterplan September 21 with conditions. We are awaiting the findings from Jim Jacks (City of Dundee Planner) to respond to the conditions.

### **Crabtree park**

We have been working with Page Knudsen, Yamhill County with a culvert repair on Knudsen lane at the entrance to the Park. We have come to find that there is no legal easement for the use of the shared driveway owned by Knudsen Vineyards. Page has had her Lawyer draft an easement for the shared access of the property and we are working on engineering and replacing the Culvert at the entrance to Knudsen Lane. I accepted the AKS Engineering Proposal as the Culvert Replacement Project engineers. AKS has completed the topographic survey and submitted an ODFW fish passage exemption request. The OFWD exemption would allow us to replace the culvert 'in-kind' rather than constructing a new fish culvert. AKS has proposed to ODF&W that we Replace the culvert as sized and mitigate the downstream section of the culvert to improve fish habitat. We met with Dave Stewart, ODF&W Biologist to go over the proposed mitigation. He seems to be on board, but this still has to be approved. If the exemption is not accepted further engineering and streambed reparations will have to occur, either way this will not be completed this year. Ongoing communication with AKS & ODF&W.

### **Aquatic and Fitness Center, Cultural Center**

We are still having HVAC issues and are trying to resolve with the Architects, contractors, engineers and representatives of ASHP's. Engineering of the solution has been finalized. Scott Edwards Architecture has given Triplett Wellman the go ahead. They are currently sourcing equipment components for the work to be performed and will advise of a date when known.

### **Development**

GreenWorks has begun the Newberg Dundee Bypass Trail Bridge Project. We have involved ODOT, City of Newberg with discussions and preliminary tasks such as Wetlands delineation, surveying, archeological assessment and talks with ODOT about future routing of Bypass and interchanges. We continue to work with Greenworks on the development of the 219 property. Don is currently updating our Park Masterplan (2010 last updated). We have hired a consultant Matt Hastie (Angelo Planning Group) to work along with John Bridges and the District to have the masterplan adopted by Yamhill County and to have zoning changed for the Heritage Trails Campground site.

RFP for the Ewing Young Trail Bridge was awarded to NV5 for the Ewing Young Bridge. They have completed the surveying required and will update on progress at the October meeting.

### **Parks**

We have been wrapping up the sports fields, working on trails-drainage and adding surface material, irrigation, pruning trees, edging pathways, maintaining and mowing our other properties, drainages, Sander Estate, Rilee property, Schaad Park. The splashpad at Rotary park is closed for the season.

### **Chehalem Glenn**

Aeration of greens tees complete and fairways nearing completion. We continue with increased play over previous years. Equipment replacement is in the works. We are experiencing many delays in the supply chain throughout the District.

### **Safety**

With the rise in Covid #'s we have Masked up in all of our indoor facilities and are requiring masks be worn indoors and outdoors per the Governor's Mandate. Still no confirmation that our District will have required vaccinations at this time. Most all full time staff are vaccinated.

## Parks Summary

Month of: September/October 2021

Sept 8 to October 20

Park Name	Hours worked
Armory	24.00
Billick/Dundee	16.00
CAFC	86.00
Cultural Center	18.00
Chehalem Valley M.S	10.00
College	2.00
Community Center	6.00
Crabtree	12.00
Crater Ballfields	28.00
Dundee Park	6.00
Dundee River Park	0.00
Elliott Road	10.00
Ewing Young	32.00
Falcon Crest Park	6.00
Fortune Park	4.00
Friends Park	20.00
Tom Gail Park	8.00
Gladys Park	16.00
Chehalem Glenn G.C.	108.00
Herbert Hoover Park	52.00
Jaquith Park	34.00
Jaquith Ball Fields	20.00
Memorial/Scout House	12.00
Mountainview	4.00
Oak Knoll Park	4.00
Oaks Park	6.00
Other District Land	6.00
Pre-School	24.00
Pride Gas	8.00
Renne Fields	16.00
Rilee Park	68.00
Rotary Park	12.00
Sander Park	6.00
Schaad Park	18.00
Scott Leavitt Park	10.00
Senior Center	60.00
Spring Meadow	8.00
Waste Mngt	8.00
vacation/holiday/sick/comp	85.00
Wilsonville Property	36.00
Youth Building	8.00
<u>Total</u>	917.00

Activity Report – Department 451  
September 2021 - COVID Continues

**Aquatic & Fitness Center**

- Facility remained open 7 days a week
- Mask mandate continues
- Sauna's still closed due to mask mandate

**Fitness Center**

- Regular fall drop-in schedule continues for the sports courts, weight room.
- We added several new programs to the fitness center for fall.
  - Fitness Studio Additions
    - We add three classes to the line-up:
      - Barre Tone & Stretch, Low Impact Aerobics, and Chair Yoga
      - Attendance has been slow to get started, we hope to see a rise in attendance as time moves onward.
  - Weight Room Addition
    - Strength Training for Bones & Balance
      - This a registration required program for seniors. We are offering three sessions this fall on Tue/Thu mornings at 8:00 a.m.
      - We had a single enrollment for the first session which unfortunately had to be cancelled.
      - We are trying again in October for session 2.

**Aquatic Center**

- September transitioned to a fall schedule (with several activity changes) on a 3 week roll out plan
- Day time public swims are done. We are only able to accommodate public swim 2 nights a week (Wed & Fri) for the fall due to lack of staff availability. Weekend public remain unchanged moving into the fall season.

**Facility Building maintenance**

- NW Aqua Tech came out September 18<sup>th</sup> to deep clean the filtration unit (Defenders). We are hoping this will reduce the amount of perlite making its way back into the pool.
- Our robot vacuums continue to be put into each pool every night to keep the perlite levels (in the water) to a minimum.
- Universal changing room #3 was reopened in September after repairs were done for the leak in the ceiling.
- A mirror was broken in the weight room on Sept. 1<sup>st</sup>. Maintenance was notified and the mirror was replaced on Sept. 28<sup>th</sup>.
- The spa was closed sept. 4<sup>th</sup> due to an electrical issue that stopped the circulation/filtration of the water. A worker came out on the 8<sup>th</sup> to fix the problem and the spa reopened on Sept. 9<sup>th</sup>.

- Front desk went without video feed on their monitors; they were unable to keep an eye on the various areas of this facility.
- Phones have been working off and on since around Friday, Sept. 17<sup>th</sup>. The telephone company was hacked and we are waiting for that to be resolved.
- Cold showers were experienced in the locker rooms; the boiler was going into alarm frequently; a worker came out Sept. 29<sup>th</sup> to take a look at the boiler.
- HVAC specialists were in the facility off and on all month working on the 3 units on the roof. It appears that progress is being made.
- Our pressure wash system in the comp pool was down for about a month. The company sent our maintenance parts to fix the hoses that were leaking causing the lack of pressure in the system; comp pool side is working again. A company tech will be coming out at a later date to review and repair the system.

### **Memberships**

- Membership sales were extremely good through September. Auto-renew assisted us greatly.
- Activenet scanners have been installed, we are still waiting to have them hooked up (software) to our system now. Currently our receptionist crew is manually logging our members into the facility. We have been without scanners since COVID-19 contact tracing was lifted.

### **Program Development & Registration**

- Fall swim lessons started the week of September 20<sup>th</sup>.
  - We are offering 3, 4-week sessions on Mon/Wed, Tue/Thu ending on Dec. 8<sup>th</sup> & 9<sup>th</sup>.
  - Our Friday program has two six-week sessions on tap finishing up on Dec. 10<sup>th</sup>.
  - Enrollment/Attendance for session 1
    - Mon/Wed Morning: 15
      - Maximum enrollment for this session = 17
    - Tue/Thu Afternoon/Evening: 82
      - Maximum enrollment for this session = 85
    - Fri Afternoon/Evening: 60
      - Maximum enrollment for this session = 67
- SUP Yoga ran three of four Sundays in September.
  - Classes were filled to capacity for all sessions.

### **Clubs/Teams**

- The last changes to the fall team/club practice schedule took place by the end of the month.
- Chehalem Swim Team's practices started up on September 7<sup>th</sup>
- NHS Polo practices continued and weekly polo games and tournaments were hosted throughout the month
- GFU swim team cap practices finished up Sept. 24<sup>th</sup> with their regular season's schedule beginning Sept. 27<sup>th</sup>
- GFU Football team continued recovery swim times in comp pool on Sunday nights
- Sept 27<sup>th</sup>, GFU soccer team booked a comp pool recovery session at moment's notice; we were very lucky to be able to accommodate their team; they will be back for more



### Aquatic & Fitness Center Staff

- Six new staffers, lifeguards and WSIs (who are attending GFU) were hired to assist with swim lessons and lifeguard shifts. We had many long time staff members leave us for new experiences at the end of the summer.
- Staff scheduling was a weekly struggle for the entire month of September due to lack of availability by our high school age staff members who are active in their school's athletic programs.

### Rentals & Special Events

- Private leisure pool parties
  - We had four weekend pool rentals in September, including several classroom rentals
    - These events are usually in celebration of a birthday
    - Back patio was closed for the season and will reopen in May
    - We receive phone calls daily inquiring about our pool party packages.
- We had an inquiry about hosting a Grad all night rental in June 2022. We are putting together a package and cost to see if we can host it.

### Management Projects

- Onboarding and getting new staff through orientation and working as soon as possible.
  - All candidates hired came with excellent references.
  - Two separate orientations were required. One each for the lifeguards and the WSIs (swim instructors).
- Staff performance and merit evaluations were prepared, conducted and submitted for approval. All of which came through as requested. We are so very grateful.
- Working hard each week to make program and staff schedules with a reduced staff availability.
- Keeping activenet, the website, social media and fliers up- to-date.

### Financial Reports

- Detailed September financial reports are attached. The synopsis below is from the original reports generated by Springbrook (as opposed to the official auditors' reports).

September	2019	2020	2021
Revenue	\$78,150.86	\$39,024.79	\$79,596.38
Personnel Services*	\$42,947.81	\$39,491.90	\$33,931.66
Material Services	\$31,353.32	\$36,711.30	\$36,039.65
Total Expenses*	\$74,301.13	\$76,203.20	\$69,971.31

\*Raw values without fringe benefits

Respectfully Submitted by,  
Wendy Roberts, Specialist and Tara Franks, Coordinator  
Chehalem Aquatic & Fitness Center

## Activity Financial Report - Sept 2021

COVID-19 - Facility Open

## Department - Aquatics 451

COVID-19 - Facility Open		Department - Aquatics 451					
EXPENDITURES	Sept '20	Sept '21	Yr to Date 20/21	Yr to Date 21/22	Year End 19/20	Year End 20/21	Est June 21/22
<b>Aquatics - 451:</b>							
<b>Personnel Services</b>							
Aquatic Supervisor							
Admin Coordinator 451.110032	\$1,470.98	\$1,629.05	\$4,535.98	\$4,743.74	\$18,161.41	\$18,087.06	\$20,363.00
Secretary I					\$503.70		
Secretary II	\$1,445.66				\$27,906.97		
Aquatic Coordinator	\$3,219.93	\$3,761.48	\$10,742.90	\$11,284.44	\$21,614.91	\$47,887.68	\$47,632.00
Aquatics Specialist	\$2,704.51	\$3,090.66	\$8,886.84	\$9,271.98	\$44,146.56	\$38,978.32	\$39,188.00
Guards	\$19,018.81	\$16,082.58	\$43,731.70	\$52,215.50	\$146,903.66	\$96,234.31	\$189,202.00
Cashiers	\$2,679.74				\$62,866.10		
Instructors	\$5,333.01	\$6,154.70	\$9,385.07	\$22,197.88	\$45,056.24	\$22,257.76	\$75,978.00
Coaches					\$725.34		\$1,540.00
Group Fitness Instructors	\$962.51	\$870.32	\$3,750.76	\$2,346.72	\$10,725.12	\$8,135.43	\$13,684.00
Personal Trainer	\$323.54	\$76.50	\$476.54	\$561.00	\$866.75	\$1,541.17	\$4,375.00
FC Monitor	\$8,515.63	\$1,410.90	\$16,324.24	\$3,251.75	\$5,211.29	\$29,656.44	\$18,952.00
Lead Guard	\$2,068.38	\$855.47	\$2,930.57	\$2,950.90	\$4,835.35	\$4,835.35	\$31,595.00
Total Personnel Services	\$39,491.90	\$33,931.66	\$100,763.60	\$108,823.91	\$420,673.38	\$267,613.52	\$442,509.00
<b>Materials &amp; Services:</b>							
Office Supplies	\$63.50	\$527.14	\$319.07	\$1,551.45	\$4,524.37	\$4,220.66	\$5,510.00
Postage Supplies	\$7.70	\$13.20	\$23.10	\$78.55	\$273.40	\$131.97	\$450.00
Program Supplies	\$1,429.69	\$1,441.86	\$4,539.82	\$2,213.35	\$12,627.25	\$16,357.60	\$15,520.00
Small Tools	\$1,417.74		\$5,354.72			\$75.86	
Chemical & Agricultural Supplies		\$4,770.97	\$1,188.23	\$8,615.45	\$26,447.01	\$33,708.33	\$29,900.00
Store Supplies		\$740.48		\$740.48	\$3,099.92	\$2,403.54	\$7,500.00
Classifieds					\$57.25		\$625.00
Brochure							
Flyers	\$59.35	\$193.39	\$178.05	\$305.49	\$1,318.30	\$676.61	\$1,450.00
Professional Dues	\$171.65	\$117.24	\$610.95	\$315.68	\$3,487.28	\$4,163.75	\$4,980.00
Conference/Workshops					\$902.50	\$179.04	\$1,450.00
Staff Mileage					\$270.28	\$82.61	\$350.00
Staff Expenses		\$95.96	\$86.82	\$145.13	\$673.69	\$1,396.79	\$1,000.00
<b>Utilities:</b>							
Electricity	\$20,379.54	\$17,879.79	\$56,461.39	\$35,519.63	\$232,937.77	\$224,768.44	\$274,500.00
Natural Gas	\$1,790.70	\$3,574.17	\$5,615.47	\$7,124.49	\$12,494.45	\$47,457.38	\$24,650.00
Water/Sewer	\$6,342.14	\$884.67	\$12,141.38	\$1,769.34	\$48,717.38	\$46,854.56	\$53,042.00
Telephone	\$330.76	\$619.98	\$992.26	\$1,859.94	\$4,089.38	\$4,376.92	\$4,200.00
Fees (activenet/bank/cc)	\$4,330.45	\$5,180.80	\$9,283.77	\$17,879.63	\$50,894.60	\$31,981.15	\$62,625.00
Internet & Communication					\$393.94	\$1,384.20	\$1,067.00
Data Storage & Backup							\$45.00
Video & Online Photography	\$19.08		\$57.24	\$38.16	\$133.57	\$267.12	\$210.00
Online Advertising				\$32.92	\$82.91		\$262.00
Ground Maint/Repairs							
Program Contracts 451.390.003	\$252.00		\$588.00	\$248.58	\$13,531.53	\$403.45	\$22,000.00
Insurance Services			\$380.36	\$380.36	\$37,819.66	\$37,674.92	\$39,175.00
Refunds	\$117.00		\$153.00	\$36.00	\$6,200.76	\$1,089.00	\$450.00
Total Materials & Services	\$36,711.30	\$36,039.65	\$97,973.63	\$78,854.63	\$460,977.20	\$459,653.90	\$555,811.00
TOTAL AQUATIC EXPENDITURES	\$76,203.20	\$69,971.31	\$198,737.23	\$187,678.54	\$881,650.58	\$727,267.42	\$998,320.00

		Department - Aquatics 451						
Activity Financial Report -Sept 2021								
COVID-19 - Facility Open								
REVENUE		Sept '20	Sept '21	Yr to Date '20/21	Yr to Date '21/22	Year End 19/20	Year End 20/21	Est June 21/22
<b>Aquatics - 451:</b>								
451.003 Youth Fitness								
451.004 Gray & Golden	\$52.00	\$100.00	\$108.00	\$272.00	\$1,379.00	\$328.00	\$1,655.00	
451.006 Group Fitness (Studio)		\$80.00	\$56.00	\$256.00	\$430.00	\$302.00	\$970.00	
451.007/291 Water Exercise	\$175.00	\$760.00	\$441.00	\$1,200.00	\$3,624.00	\$1,186.00	\$3,960.00	
451.008 Weight Training	\$55.00	\$1,280.00	\$1,615.00	\$2,282.00	\$22,848.50	\$4,106.00	\$12,385.00	
451.011 Private Swim Lessons	\$585.00	\$5,713.50	\$3,246.00	\$16,933.00	\$35,393.00	\$10,206.30	\$48,750.00	
451.012 Optum Fitness Advantage*	\$540.00		\$1,628.00	\$15,892.00	\$4,316.00	\$4,316.00	\$16,975.00	
451.013 Water Safety		\$120.00	\$50.00	\$765.00	\$5,375.35	\$7,489.65	\$6,225.00	
451.014 SilverSneakers - Tivvy*	\$318.00	\$360.50	\$394.28	\$1,022.00	\$4,291.00	\$2,070.28	\$4,250.00	
451.015 Silver&Fit - ASH*	\$2,481.00	\$2,883.00	\$6,469.50	\$9,090.00	\$59,067.02	\$26,428.12	\$62,750.00	
451.016 Water Polo		\$16,000.00	\$1,760.00	\$26,876.50	\$16,357.20	\$5,290.00	\$18,650.00	
451.017 GFU					\$39,072.25	\$18,935.00	\$35,600.00	
451.018 Newberg High School					\$812.00	\$1,784.17		
451.019 School Districts								
451.020 Locker Income								
451.021 Locker Rental								
451.023 Pool Rental	\$3,722.50	\$2,045.00	\$37,270.00	\$6,743.75	\$19,431.00	\$55,774.74	\$16,875.00	
451.024 Classroom Rental		\$300.00	\$150.00	\$875.00	\$7,501.01	\$150.00	\$8,995.00	
451.025 Sauna/Spa		\$7.50		\$100.00	\$4,757.10		\$5,490.00	
451.026 Special Events					\$100.00			
451.027 Repasses								
451.061 Vending						\$339.55		
451.200 Aquatics Misc		\$120.00			\$25.00	\$480.00		
451.280 Sales	\$348.99	\$601.00	\$835.49	\$2,471.00	\$6,024.49	\$2,475.49	\$6,500.00	
451.281 CST/CVA			\$11,707.25	\$10,855.95	\$26,678.21	\$19,500.00	\$19,500.00	
451.282 Swim Lessons	\$3,692.00	\$10,199.58	\$11,904.00	\$51,818.21	\$102,088.91	\$56,283.50	\$134,325.00	
451.283 Lap Swim	\$4,271.00	\$1,543.00	\$16,760.00	\$4,293.00	\$15,525.25	\$24,526.00	\$14,775.00	
451.284 Public Swim	\$4,474.50	\$6,887.50	\$13,935.00	\$36,575.00	\$85,411.00	\$33,598.50	\$128,875.00	
451.285 Equipment Rental								
451.286 Membership Sales	\$14,763.80	\$25,114.80	\$33,378.17	\$68,572.20	\$284,774.00	\$137,726.88	\$308,450.00	
451.287 Weight Room	\$1,134.50	\$1,424.50	\$2,840.59	\$6,059.00	\$14,545.50	\$9,818.09	\$18,320.00	
451.289 Punch Cards - General	\$2,035.00	\$3,766.00	\$5,613.00	\$10,366.00	\$32,136.51	\$23,338.50	\$38,125.00	
451.290 Gift Certificates								
451.292 Preschool Swim Lessons								
451.294 SUP Yoga		\$520.00			\$13,512.20		\$1,020.00	
451.296 Patio Rental		\$100.00		\$357.50	\$3,999.75		\$4,750.00	
451.293/425 Basketball Court	\$376.50	\$781.50	\$778.00	\$2,169.00	\$11,663.75	\$1,694.00	\$12,150.00	
451.285/426 Pickleball Court					\$1,429.05	\$26.00	\$1,540.00	
451.021/427 Volleyball Court					\$403.00		\$350.00	
451.428 SkyTrack					\$6.50		\$50.00	
451.429 Fencing		\$169.00	\$120.00	\$788.00	\$890.30	\$240.00	\$1,440.00	
451.666 CC Fees					\$555.31			
<b>TOTAL AQUATIC REVENUE</b>	\$39,024.79	\$79,596.38	\$150,719.28	\$249,884.16	\$819,066.28	\$455,590.98	\$933,700.00	





## Youth Sports

October 2021 Activity Report, Department 453

Department 453 Participation Tracking	September 2021	
Activity	Participants	Participant Hours
Football: Tackle & Flag	300	6000
Soccer	560	11200
Little Big kicks	140	2800
Mid High Volleyball	45	2000

Department 453 Financial Tracking	September 2021	
Supervisory Staff Expense	6000	
Administrative Staff Expense	2340	
Part Time Staff Expense	110	
Program/Materials Expense	13610	
Total Expense	22060	
Program Revenue	41580	
Net	19520	
Cost Per Participant	18.67	
Cost Per Participant Hour	.88	

### Department 453 – Youth Sports

CPRD has exceeded previous participation totals this 2021 Fall Sports season.

Tackle and Flag football are under way with games played each Saturday at Newberg High School.

The youth soccer league and Little big kicks programs are currently played each Saturday at Jaquith Park and Newberg High School.

The Middle School volleyball program started back up this fall with the team traveling to local schools for completion.

The winter basketball registration has opened for all levels K-8.

Sept 2021 Board Reports.

Departments

454 Recreation

455 Care

456 Senior Center

457 Community School

474 Preschool

<b>454- Recreation</b>	
Supervisory Staff Expense	\$678.78
Recreation Coordinator	\$1961.34
Part Time Staff Expense	\$2,288.54
Fringe	396.55
program Expense	610.70
Total Expense	\$5,935.91
Revenue	\$12,228.66
<b>455-Care</b>	
Total Staff Expense	\$24,150.54
Total Materials expense	\$2,909.03
Revenue	\$58,414.40
<b>456 Senior Center</b>	
Recreation Coordinator	\$339.38
Senior Center Specialist	\$3,554.82
Fringe	\$1336.04
program Expense	\$3314.37
Total Expense	\$8544.61
Program Revenue	\$5,200.00
<b>457 Community School</b>	
Supervisory Staff Expense	\$882.41
Fringe	\$4,408.91
program Expense	\$903.99
Total Expense	\$6195.31
Program Revenue	\$3,665.00
<b>474- Bonnie Benedict Preschool</b>	
Total Staff Expense	\$5,681.89
Materials Expense	\$559.81
Total Expense	\$7,972.10
Program Revenue	\$2,666.32

**Recreation**

**CPRD Before and After School Care**

We are excited to finally be able to operate our child care program in every Newberg School District Grade school. We started the school year with 112 families registered. As of today registration has grown to 164. Staffing these multiple sites continues to be difficult but we are making progress.

**Senior Center**

This building is open to the public but we are taking a slow approach to scheduling programming for the time being. Meals on wheels has been operating out of this building very consistently.

**Community School**

Programming in our local schools is going to be difficult this year. Internally we are not planning on offering many classes until after a vaccine becomes available for Grade school and Middle School students. Our hope is that school building access will be easier at that stage.

**Bonnie Benedict Preschool:**

We started the school year with 22 preschool students spread out over 3 classes. Having multiple classes allows us to socially distance appropriately and to operate the program with only one Teacher. Deanna Rice is doing a fantastic job with her kids and families.

Respectfully submitted by Matt Compton

**FYTD21 - September**

	FY2011	FY2012	FY2013	FY2014	FY2015	FY2016	FY2017	FY2018	FY2019	FY2020	FY2021	FY21v FY20	% Diff
<b>Starts by Category</b>													
Resident	2283	1622	1806	2251	1664	2440	6616	3051	3497	2979	4068	1089	36.6%
Non Resident	10015	6829	8664	7374	6052	6942	10290	3204	3085	3104	4014	910	29.3%
Group	2342	2659	2115	1563	1591	1530	1432	1130	1232	342	1223	881	257.6%
League	314	214	274	226	207	258	3660	41	13	0	0	0	0.0%
Complimentary	871	962	942	1026	856	941	1745	615	722	2338	2106	-232	-9.9%
Misc/Promotional	1372	4395	1641	1399	3847	3326	6018	5595	4875	6775	4666	-2109	-31.1%
<b>Total Starts</b>	<b>17197</b>	<b>15825</b>	<b>12286</b>	<b>13801</b>	<b>12440</b>	<b>15437</b>	<b>29761</b>	<b>13636</b>	<b>13424</b>	<b>15538</b>	<b>16077</b>	<b>539</b>	<b>3.5%</b>
<b>Revenue</b>													
Green Fees	\$ 391,052.00	\$ 406,344.00	\$ 372,523.00	\$ 309,831.00	\$ 294,539.00	\$ 308,726.00	\$ 520,873.00	\$ 278,182.00	\$ 283,180.00	\$ 349,001.00	\$ 360,954.00	\$ 11,953.00	3.4%
Driving Range	\$ 36,403.00	\$ 31,304.00	\$ 31,722.00	\$ 26,570.00	\$ 24,838.00	\$ 27,106.00	\$ 60,276.00	\$ 28,930.00	\$ 26,629.00	\$ 37,926.00	\$ 38,655.00	\$ 729.00	1.9%
Rentals	\$ 139,972.00	\$ 117,620.00	\$ 116,301.00	\$ 96,925.00	\$ 93,429.00	\$ 109,777.00	\$ 191,552.00	\$ 84,457.00	\$ 78,821.00	\$ 123,132.00	\$ 139,001.00	\$ 15,869.00	12.9%
Golf Shop	\$ 35,631.00	\$ 33,875.00	\$ 35,042.00	\$ 32,105.00	\$ 37,526.00	\$ 23,972.00	\$ 54,019.00	\$ 30,660.00	\$ 29,428.00	\$ 37,347.00	\$ 37,551.00	\$ 204.00	0.5%
Snack Bar	\$ 99,658.00	\$ 110,001.00	\$ 80,030.00	\$ 63,919.00	\$ 62,167.00	\$ 85,396.00	\$ 126,244.00	\$ 70,918.00	\$ 65,202.00	\$ 68,631.00	\$ 79,494.00	\$ 10,863.00	15.8%
Instruction	\$ 5,599.00	\$ -	\$ 4,498.00	\$ 6,328.00	\$ 1,838.00	\$ 1,144.00	\$ 5,573.00	\$ 2,445.00	\$ 2,445.00	\$ 7,975.00	\$ 11,758.00	\$ 3,783.00	47.4%
Miscellaneous	\$ 4,204.00	\$ 22,449.00	\$ 8,795.00	\$ (2,892.00)	\$ (16,700.00)	\$ (6,474.00)	\$ 17,960.00	\$ 6,331.00	\$ 4,201.00	\$ 16,892.00	\$ 23,590.00	\$ 6,698.00	39.7%
<b>Total Revenue</b>	<b>\$ 712,519.00</b>	<b>\$ 721,593.00</b>	<b>\$ 648,911.00</b>	<b>\$ 532,786.00</b>	<b>\$ 497,637.00</b>	<b>\$ 549,649.00</b>	<b>\$ 981,239.00</b>	<b>\$ 505,051.00</b>	<b>\$ 489,906.00</b>	<b>\$ 640,904.00</b>	<b>\$ 691,003.00</b>	<b>\$ 50,099.00</b>	<b>7.8%</b>
<b>\$ per Start</b>													
Green Fees \$ per Start	\$ 22.74	\$ 25.68	\$ 30.32	\$ 22.45	\$ 23.68	\$ 20.00	\$ 17.50	\$ 20.40	\$ 21.10	\$ 22.46	\$ 22.45	\$ (0.01)	0.0%
Driving Range \$ per Start	\$ 2.12	\$ 1.98	\$ 2.58	\$ 1.93	\$ 2.00	\$ 1.76	\$ 2.03	\$ 2.12	\$ 1.98	\$ 2.44	\$ 2.40	\$ (0.04)	-1.5%
Rentals \$ per Start	\$ 8.14	\$ 7.43	\$ 9.47	\$ 7.02	\$ 7.51	\$ 7.11	\$ 6.44	\$ 6.19	\$ 5.87	\$ 7.92	\$ 8.65	\$ 0.72	9.1%
Golf Revenue \$ per Start	\$ 33.00	\$ 35.09	\$ 42.37	\$ 31.40	\$ 33.18	\$ 28.87	\$ 25.96	\$ 28.72	\$ 28.95	\$ 32.83	\$ 33.50	\$ 0.68	2.1%
Golf Shop	\$ 2.07	\$ 2.14	\$ 2.85	\$ 2.33	\$ 3.02	\$ 1.55	\$ 1.82	\$ 2.25	\$ 2.19	\$ 2.40	\$ 2.34	\$ (0.07)	-2.8%
Snack Bar	\$ 5.80	\$ 6.95	\$ 6.51	\$ 4.63	\$ 5.00	\$ 5.53	\$ 4.24	\$ 5.20	\$ 4.86	\$ 4.42	\$ 4.94	\$ 0.53	11.9%
Concession Revenue	\$ 7.87	\$ 9.09	\$ 9.37	\$ 6.96	\$ 8.01	\$ 7.08	\$ 6.06	\$ 7.45	\$ 7.05	\$ 6.82	\$ 7.28	\$ 0.46	6.7%

Good weather continued through September, so the golfers kept coming. FYTD driving range has been the busiest it's been in the last 10 years.



September															% Diff
	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	21v20		
Dry Days	19	22	28	11	25	23	22								
Starts by Category															
Resident	548	402	577	579	455	631	835	589	643	658	667	1141	474	71.1%	
Non Resident	2447	1679	2936	1652	1695	1778	1659	1616	745	620	773	929	156	20.2%	
Group	825	865	536	334	366	412	389	252	125	485	43	561	518	1204.7%	
League	53	56	63	31	48	30	53	16	32	12	0	0	0	0.0%	
Complimentry	307	327	324	367	316	330	202	170	213	169	526	588	62	11.8%	
Misc/Promotional	262	1273	499	266	850	823	783	637	1375	1090	1567	1298	-269	-17.2%	
Total Starts	4442	4602	4935	3229	3732	4004	3921	3280	3133	3034	3576	4517	941	26.3%	
Revenue															
Green Fees	\$ 103,783.00	\$ 125,750.00	\$ 113,607.00	\$ 67,686.00	\$ 73,579.00	\$ 84,545.00	\$ 74,926.00	\$ 69,034.00	\$ 64,839.00	\$ 67,521.00	\$ 80,108.00	\$ 98,539.00	\$ 18,431.00	23.0%	
Driving Range	\$ 8,948.00	\$ 7,274.00	\$ 9,918.00	\$ 5,342.00	\$ 5,825.00	\$ 6,777.00	\$ 6,393.00	\$ 5,758.00	\$ 7,201.00	\$ 5,616.00	\$ 8,517.00	\$ 10,628.00	\$ 2,111.00	24.8%	
Rentals	\$ 37,516.00	\$ 30,997.00	\$ 37,765.00	\$ 22,650.00	\$ 23,412.00	\$ 29,024.00	\$ 29,879.00	\$ 25,118.00	\$ 20,796.00	\$ 18,813.00	\$ 28,814.00	\$ 39,524.00	\$ 10,710.00	37.2%	
Golf Shop	\$ 7,967.00	\$ 9,397.00	\$ 9,674.00	\$ 8,220.00	\$ 13,475.00	\$ 7,103.00	\$ 5,439.00	\$ 5,943.00	\$ 8,985.00	\$ 5,122.00	\$ 8,377.00	\$ 9,642.00	\$ 1,265.00	15.1%	
Snack Bar	\$ 24,235.00	\$ 27,584.00	\$ 24,262.00	\$ 13,262.00	\$ 15,552.00	\$ 23,431.00	\$ 21,489.00	\$ 20,713.00	\$ 16,510.00	\$ 12,908.00	\$ 14,538.00	\$ 19,223.00	\$ 4,685.00	32.2%	
Instruction	\$ 1,170.00	\$ -	\$ 165.00	\$ 675.00	\$ 250.00	\$ 140.00	\$ -	\$ -	\$ -	\$ 270.00	\$ 550.00	\$ 375.00	\$ (175.00)	-31.8%	
Miscellaneous	\$ 625.00	\$ 7,100.00	\$ 1,166.00	\$ (6,603.00)	\$ (15,932.00)	\$ (5,380.00)	\$ 8,015.00		\$ 1,626.00	\$ 960.00	\$ 4,221.00	\$ 6,727.00	\$ 2,506.00	59.4%	
Total Revenue	\$ 184,244.00	\$ 208,102.00	\$ 196,557.00	\$ 111,232.00	\$ 116,161.00	\$ 145,640.00	\$ 146,141.00	\$ 128,008.00	\$ 119,957.00	\$ 111,210.00	\$ 145,125.00	\$ 184,658.00	\$ 39,533.00	27.2%	
\$ per Start															
Green Fees \$ per Start	\$ 23.36	\$ 27.33	\$ 23.02	\$ 20.96	\$ 19.72	\$ 21.12	\$ 19.11	\$ 21.05	\$ 20.70	\$ 22.25	\$ 22.40	\$ 21.82	\$ (0.59)	-2.6%	
Driving Range \$ per Start	\$ 2.01	\$ 1.58	\$ 2.01	\$ 1.65	\$ 1.56	\$ 1.69	\$ 1.63	\$ 1.76	\$ 2.30	\$ 1.85	\$ 2.38	\$ 2.35	\$ (0.03)	-1.6%	
Rentals \$ per Start	\$ 8.45	\$ 6.74	\$ 7.65	\$ 7.01	\$ 6.27	\$ 7.25	\$ 7.62	\$ 7.66	\$ 6.64	\$ 6.20	\$ 8.06	\$ 8.75	\$ 0.69	11.2%	
Golf Revenue \$ per Start	\$ 33.82	\$ 35.64	\$ 32.68	\$ 29.63	\$ 27.55	\$ 30.06	\$ 28.36	\$ 30.46	\$ 29.63	\$ 30.31	\$ 32.84	\$ 32.92	\$ 0.08	0.3%	
Golf Shop	\$ 1.79	\$ 2.04	\$ 1.96	\$ 2.55	\$ 3.61	\$ 1.77	\$ 1.39	\$ 1.81	\$ 2.87	\$ 1.69	\$ 2.34	\$ 2.13	\$ (0.21)	-12.3%	
Snack Bar	\$ 5.46	\$ 5.99	\$ 4.92	\$ 4.11	\$ 4.17	\$ 5.85	\$ 5.48	\$ 6.31	\$ 5.27	\$ 4.25	\$ 4.07	\$ 4.26	\$ 0.19	4.5%	
Concession Revenue	\$ 7.25	\$ 8.04	\$ 6.88	\$ 6.65	\$ 7.78	\$ 7.63	\$ 6.87	\$ 8.13	\$ 8.14	\$ 5.94	\$ 6.41	\$ 6.39	\$ (0.02)	-0.3%	

Very solid September. We had 4 large tournaments, which is 1 more than usual for September.

Very sold September. We had 4 large tournaments, which is 1 more than usual for September.

**Subject:** Noise compliant  
**Date:** Tuesday, September 14, 2021 at 2:57:35 PM Pacific Daylight Time  
**From:** Robert Jones  
**To:** Don Clements, Becky Drey, Casey Creighton, Heidi Smith, Jim Bernard, Joshua Danner, Kat Ricker, Kellan Sasken  
**Attachments:** 8-2.jpg, 8-1.jpg

Hello,

I live in the Greens. My wife and I were in bed and at 6 am we woke up to the sound of leaf blowers coming from the golf course. When I inquired about what was going on, we were told it was Byran Stewart's workers using the leaf blowers. We have noise ordinances in Newberg and I don't understand why people who should know the rules were breaking them! It's not very pleasant to be woken up for no reason.

This isn't the first time I've spoken to the pro shop about Bryan and his workers. I can't tell you how many times I've been golfing and the workers in the green shirts have been using the leaf blowers in the parking lot blowing debris right into people's cars. Some of the guys in my golf group have had their cars dinged by the rocks flying around. Not only that, I've seen Bryan park his huge truck right in front of the pro shop so you can't even unload or hardly get around him. The front of the pro shop should be reserved for people who need it.

A few months ago I asked the maintenance workers about the flower beds on hole six and the tee box on hole nine. They are full of weeds and look terrible. They let Bryan know but he's never come out and cleaned them up. See the attached images. It's looked this way for months. We have an HOA and are expected to keep our yards nice for everyone but apparently he doesn't care how his stuff looks for us? But you know who does get their stuff taken care of? PCC. I've driven by and seen the workers in the green shirts working on PCC's property. As taxpayers, it's very frustrating seeing our money going to places it shouldn't and not seeing it going to places it should.

I know some of my neighbors have talked to the pro shop about some of these same problems and we're tired of nothing changing. I don't know who this email needs to go to but I'm escalating this issue and would like a response as to when these issues will be addressed and how you will resolve them.

Robert







**Subject:** Lisa Jackson status

**Date:** Tuesday, September 21, 2021 at 11:19:41 AM Pacific Daylight Time

**From:** Lisa Jackson

**To:** Kat Ricker

Hello, just before being accepted to the advisory committee, I had a total knee replacement revision. I have just learned that I have developed an infection in my new knee which is very dangerous. I am having emergency surgery in a couple days. This surgery is going to be very hard and I am looking at a very long recovery. Unfortunately, I do not think I will be able to do justice to my equestrian community and the park while I go through this very long and difficult process. I am so disappointed as the Park is very dear to me. However, I have a fellow neighbor that feels the same way as I do about the park. She is connected to a wide range of equestrian friends that are very excited to help and volunteer at the park. Would you consider letting her either take my position or fill in for me until I am back on my feet? If so, I can have her send you her information. Her name is Jenny Jeronimo. She would also be a great voice of Parrett Mountain. Thanks so much and I apologize for the inconvenience. Lisa

Sent from my iPad

**Subject:** Survey article

**Date:** Wednesday, September 22, 2021 at 12:36:19 PM Pacific Daylight Time

**From:** Wayne Wiebke

**To:** Kat Ricker

Hi Kat,  
here is the Survey article in yesterday's NR.

Wayne

By [Nicole Montesano](#) • Staff Writer • September 21, 2021  
[Tweet](#)

## Survey shows trail support

### *Decommissioning of partially built bridge to be considered*

As the Chehalis Parks and Recreation District is touting the results of a recent survey showing strong support for the Yamhelas Westsider Trail, Yamhill County commissioners are preparing to approve a contract to decommission the partially built bridge over Stag Hollow Creek. The bridge was intended to allow trail access and provide an entry point for fire trucks, should firefighting be needed along the route.

The bridge was originally intended for pedestrian access only, but after some trail opponents argued to commissioners in 2018 that field fires could prove dangerous to trail users, the county had the bridge designed to accommodate fire trucks. Since then, opponents have argued that the county was wrong to describe the bridge as necessary for firefighting access.

On Thursday, the commissioners considered a contract with Farline Bridge, Inc., to decommission the bridge, since the county has abandoned its efforts to construct the trail. They postponed making a decision, however, after learning that changes had raised the \$36,668 price tag another \$10,000.

The issue is on the agenda for this week's meeting, scheduled for 10 a.m. Thursday. It may be viewed live on YouTube, via a link from the county page, [www.co.yamhill.or.us](http://www.co.yamhill.or.us), under the meetings tab.

Commissioners are also scheduled to discuss the effects on a grant that helped to pay for the bridge.

This summer, the Chehalis Parks and Recreation District hired the Nelson Research company to conduct a survey of "the public's perception of recreational opportunities in Yamhill County and a proposal to develop the Yamhelas Westsider Trail into a multi-use trail."

The group Friends of the Yamhelas Westsider Trail contributed funds for the survey.

It found slightly more than half the participants had never heard of the trail but that, once it was explained to them, they supported it.

The report notes that "A total of 400 respondents were interviewed between July 15 and July 22, 2021. The margin of error for this survey is +/-4.9% at the 95% level of confidence.

"Slightly over half of respondents, 52%, said they had not heard, read or seen any information about the development of the Yamhelas Westsider Trail, while 46% had. Another 2% were not sure," the report's executive summary states.

Respondents were given the following information and question: "Yamhill County purchased a 92-acre parcel of railroad line to convert it into a multi-use path for pedestrians, bicyclists and horseback riders. This path, known as the Yamhelas Westsider Trail, has been planned to connect the communities of Carlton, Yamhill, Cove Orchard and Gaston. Do you personally favor or oppose developing the Yamhelas Westsider Trail into a multi-use trail?" the

**Subject:** Pickleball Advisory Committee

**Date:** Wednesday, September 22, 2021 at 7:21:46 AM Pacific Daylight Time

**From:** Laurie Rauch

**To:** Kat Ricker

Dear Kat,

I hope the decision process is going well for the Pickleball Advisory Committee. If it is decided that Hunter Wylie is going to be part of the committee please withdraw my application.

If you have any questions feel free to contact me.

Thank you,

Laurie Rauch

503-516-3111



**Subject:** Politics in Youth Sports

**Date:** Wednesday, September 29, 2021 at 11:27:05 AM Pacific Daylight Time

**From:** Donnie Haggard

**To:** CPRD Board, Julie Petersen

**CC:** Beth Haggard

Good afternoon!

I am reaching out to you to make you aware of a situation that occurred with my daughter's 3rd grade soccer coach. Our coach emailed our entire team to inform us that she was gathering signatures in support of a local recall effort that is underway for Newberg School Board member Brian Shannon and that she would be bringing sign up sheets to practices and games (see full text of email below). My wife responded immediately to let her know, politely and respectfully, that this was inappropriate and in poor taste to use her position as a coach to infuse local politics into our youth sports. I am asking you to take this opportunity to reach out to all of our coaches and let them know that our youth sports should be a neutral place where our kids can play and learn and have fun, free from any and all political activism.

Thank you for giving this matter your prompt attention. Please let me know if you have any questions.

Sincerely,

Donnie Haggard

" Hi parents,

I am collecting signatures in support of a petition to recall Brian Shannon, the member of the Newberg School Board who is the main driver behind their attempt to ban Pride and BLM memorabilia from schools. The actions of the board are believed to be illegal, and are widely not supported by administrators and educators throughout the district.

If you are interested in signing or have any questions, please let me know. I'll be bringing the signature sheets to practices / games for the next couple of weeks so let me know."

Sent from my iPhone

**Subject:** RE: Politics in Youth Sports

**Date:** Wednesday, September 29, 2021 at 1:36:45 PM Pacific Daylight Time

**From:** Jon Tromblay

**To:** Donnie Haggard, CPRD Board, Julie Petersen

**CC:** Beth Haggard

Thanks for the email Donnie. I appreciate you bringing this to our attention. I will contact the coach and make her aware that her actions in this matter are not appropriate. I apologize for any inconvenience this may have caused you and your family. Jon Tromblay

-----Original Message-----

From: Donnie Haggard [<mailto:dhaggard4@gmail.com>]

Sent: Wednesday, September 29, 2021 12:43 PM

To: CPRD Board <[board@cprdnewberg.org](mailto:board@cprdnewberg.org)>; Julie Petersen <[jpetersen@cprdnewberg.org](mailto:jpetersen@cprdnewberg.org)>; Jon Tromblay <[jtromblay@cprdnewberg.org](mailto:jtromblay@cprdnewberg.org)>

Cc: Beth Haggard <[bhaggard08@gmail.com](mailto:bhaggard08@gmail.com)>

Subject: Re: Politics in Youth Sports

Adding Jon Tromblay as I inadvertently left him off the distribution list.

Good afternoon!

I am reaching out to you to make you aware of a situation that occurred with my daughter's 3rd grade soccer coach. Our coach emailed our entire team to inform us that she was gathering signatures in support of a local recall effort that is underway for Newberg School Board member Brian Shannon and that she would be bringing sign up sheets to practices and games (see full text of email below). My wife responded immediately to let her know, politely and respectfully, that this was inappropriate and in poor taste to use her position as a coach to infuse local politics into our youth sports. I am asking you to take this opportunity to reach out to all of our coaches and let them know that our youth sports should be a neutral place where our kids can play and learn and have fun, free from any and all political activism.

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If you are interested in signing or have any questions, please let me know. I'll be bringing the signature sheets to practices / games for the next couple of weeks so let me know."

Sent from my iPhone

## Question on solicitation

**Kat Ricker** <[kricker@cprdnewberg.org](mailto:kricker@cprdnewberg.org)>

Thu 10/14/2021 10:56 AM

To: [joseph2v@gmail.com](mailto:joseph2v@gmail.com) <[joseph2v@gmail.com](mailto:joseph2v@gmail.com)>

Cc: Don Clements <[dclements@cprdnewberg.org](mailto:dclements@cprdnewberg.org)>; Casey Creighton <[ccreighton@cprdnewberg.org](mailto:ccreighton@cprdnewberg.org)>

Dear Joseph:

Thank you for contacting us with your question and concerns about political solicitation taking place on CPRD park properties. Public citizens are permitted by law to conduct political solicitation on public property. While Oregon law, and subsequently CPRD employment policy, restrict public employees from gathering signatures and related action while on paid time, it is our understanding that this activity is not being carried out by CPRD staff.

I hope that answers your question. Please contact us with any further questions. Your email message will be included for consideration at our next Board meeting.

Sincerely,

**Kat Ricker**

*Public Information Director*

*Chehalem Park & Recreation District*

*125 S. Elliott Road*

*Newberg, OR 97132*

*971.832.4222 [cprdnewberg](https://www.cprdnewberg.org) / [Instagram](#) / [Facebook](#)*

Original Message:

Don-

I am not sure if you are the right person to direct this email to but I have a question related to policies and rules at our city parks.

The past two weeks when I have taken my children to soccer on Saturday morning at Jaquith I have been greeted by 4-6 individuals asking me to sign recall petitions for various causes (recall Brian Shannon, recall Lindsey B.). Does this kind of activity require any sort of permit? Is anyone allowed to do this type of signature gathering at our parks at any time?

I have found their presence to be rather intimidating for both me and my children as we basically have to walk through their gauntlet to get to our soccer game.

As stated above, I'd like to better understand city or park policies related to this. Thank you for your assistance.

-Joe Thouvenel

--

**Kat Ricker**

**Subject:** Form submission from: Contact Us

**Date:** Friday, October 15, 2021 at 6:44:45 PM Pacific Daylight Time

**From:** DontReply

**To:** Kat Ricker

Submitted on Friday, October 15, 2021 - 6:44pm

Submitted by anonymous user: 73.11.0.118

Submitted values are:

First Name Andrew

Last Name Hughes

Email r.andrewhughes@gmail.com

Question/Comment

I wanted to thank you for all the work that you have done on the trails at Ewing Young park — adding the thick layers of gravel and wood-chips will allow these trails to be used all through the wet season (instead of becoming a muddy mess! Like they have in previous years).

I have walked several times after a heavy rain, and my shoes have left the trails clean, so good for our wet winter & spring!! So thank you!

BUT, I have noticed that some dog owners are not doing the right thing and leaving their dog poops on the trail! Its frustrating... I want to walk and enjoy the scenery, instead I'm looking at where my feet will tread, making sure I don't step in a dog turd!

Can I make a few suggestions to remedy this situation:

#1 — A sign + pooper bag dispenser at each of the two entrances to the trail — the sign explaining that its the law for dog owners to pick-up after their dogs... etc.

#2 — strategically placed trash cans around the mile loop so dog owners can pick-up the poop, but not carry it on their whole walk... perhaps place these strategically so they can be used by the Disc Golfers and the Dog Walkers

Again, thank you for the trail upgrades! They will get us outside so much more with our dog during the wet season!

The results of this submission may be viewed at:

<https://www.cprdnewberg.org/node/7/submission/4061>

**Subject:** Schaad Park Trails  
**Date:** Thursday, October 21, 2021 at 11:02:02 AM Pacific Daylight Time  
**From:** Casey Creighton  
**To:** DREECK@msn.com  
**CC:** Don Clements, Kat Ricker  
**Attachments:** Outlook-hj4ujkpa.jpg

Mr. Reeck,

We are aware of the damage that has been done to the asphalt section of the Schaad Park trail and will be addressing what we can before winter rains set in. There has been a lot of damage done from burrowing animals and lack of drainage within the pathway itself and may require reconstruction to eliminate the ongoing erosion. We have seen an increase of use on all the trails in the District and have been working on improving and updating them to accommodate the increased traffic. Over the past year we have come up with plans for updating and improving all of the trails in the District along with new trails to be constructed. Thank you for your concerns.

Best regards,

Casey Creighton  
Park Supervisor  
Chehalem Park & Recreation District



9/13/21

Dear Board Chelabem Board of Directors:

Dozens of us enjoy water exercise at the pool and are grateful it's open and has many offerings.

However while in water X we see dozens of other pool customers enjoy the hot tub. God knows we all have stress-

But for the most part the ONLY way those of us who do the one hour water exercise class can enjoy the hot tub is to leave the class early because they close the tub to those twenty plus customers in 13 classes a week promptly when the classes end!!!!

No, it doesn't make sense.

No, it isn't "fair."

Surely with 20 plus customers in 13 classes (260 people per week) the pool staff can afford a lifeguard for 15 damn minutes after each water X class!!! We're upset!!!

253

9/21/22

Will you please help. The 20 to 30

people who attend the 9:15 am aerobic water

classes? One of us spent \$100- for a

headset / speaker so we could hear the

teacher. But there's ONE woman who

1) does not wear a belt and 2) therefore

is not in no way can she do any of the  
exercises in Kathleen or Karen's classes.

What does she do? TALK, TALK, TALK

Except Thursday non-stop with a loud

former Alaskan Airline flight attendant.

So much loud non-stop talking they

stopped Kathleen three times to ask



the teacher what exercise we were  
doing.

Today, Tuesday, 9/21/22 the huge

non-stop, incredibly RUDE woman

we call "Chatty Kathy" talked and

talked ~~to~~ to a blonde/grey haired

woman from McMinnville. Kathleen  
told them to talk quietly. Here's the

Bottom line, this Chatty Kathy  
#1 Does zero of the exercises  
because she can't do any of them  
without a beet.

#2 She is beside herself if she  
can't find a recipient for her  
God damn non stop talking. When  
that happened Friday she picked up  
her toys and left. She is RUDE  
beyond belief. 60 entire  
minutes every damn 9:15 am class

Now - Stop talk, talk, talk.

Loud or quiet, it's rude -

Zero of the other participants

talk, No one talks 60 minutes

Tara will not do anything.

Several of us are going to 'just quit'

This Chatter Kathy should go to

the deep water without a beat

when there's NO class.

We are all very fed up.

PLEASE fix This!!!



six women in  
the 9:15 am  
classes, water X

10/10/21

Dear Board of Directors:

The pool Director has been told about people talking during water exercise classes, so loud you cannot hear the teachers. Little has changed except teachers had to buy, at their own expense, speakers and headsets. It is not a little problem. It is profoundly RUDE.

For example Lori and Casey non-stop talk 60 minutes in 8am class. They face each other sideways. They ignore the teacher. A few feet away is a woman with no hair due to metastasized breast cancer. She is trying to workout and focus. On her other side are Nora and her friend, talking so loud the lap swimmers can hear them. Sixty minutes.

class after class. Day after day. Tara has been told and there is no change.

The only way to end this blatant RUDENESS is announce at each class: SHUT

The rudeness of these lonely women who only come to chat is unbelievable.

Water X classes 2 or 3 very rude \*  
women ruin it  
for everyone  
especially the teacher  
on the headset  
purchased by one  
of us.

9/16/21 - <sup>(LOUD)</sup> non-stop talking by Alaska Airlines  
retired flight attendant + Chatty Kathy  
with the red VW convertible

9/21/21 - non-stop talking by lady who comes  
from Mc Minnville - blonde hair and  
Chatty Kathy w/ the red convertible

9/23/21 - Chatty Kathy<sup>her</sup> back is to the teacher  
entire 60 minutes while she talks  
non-stop to Laura. Chatty Kathy  
comes late to class - talks non-  
stop & leaves early - All this is  
attention seeking and very rude.

9/24/21 - Chatty Kathy arrives late, leaves early  
her back to the teacher all the time  
talking to Laura all the time

9/28/21 - Chatty Kathy - talking non-stop to  
Alaska Airline flight attendant  
Chatty Kathy and her "receptors"  
those who listen & also talk  
have to ask the teacher what exercise  
we are supposed to be doing

9/30/21 - Chatty Kathy - arrives late -  
leaves early - always seeking  
attention by these behaviors  
plus non-stop talking - taking  
to Gail all the class time

10/4/21 - Chatty Kathy + Laura + Gail  
never stop talking

10/14/21 - Chatty Kathy + new student  
Cathy (another one)  
entire class non-stop  
talking

10/19/21 - HORRIBLE Chatty Kathy  
+ new Cathy person  
60 minutes  
non-stop  
Laura

11/1/21 - 11/1/21 - 11/1/21  
RUE

RUE BEYOND BELIEF

**Chehalem Valley Future Focus Committee Administrators**

**8:30 a.m. Tuesday, Oct. 5, 2021**

**CPRD Administration Office**

**125 S. Elliott Road**

**Newberg, OR 97132**

**NOTES**

*Roll Call*

City of Newberg Community Engagement Specialist Lacey Dykgraaf (remote)

CPRD Public Information Director Kat Ricker

CPRD Superintendent Don Clements

CPRD Parks and Facilities Supervisor Casey Creighton

Newberg School District, Luke Neff, Director of Strategic Partnerships (remote)

TVF&R Stefan Myer

*Absent*

City of Dundee Manager Rob Daykin

City of Newberg Manager Dan Weinheimer (represented)

PCC, Lynn Montoya-Quinn

Yamhill County, Carrie Martin

- I. Don Clements called meeting to order at 8:32 a.m.
- II. Reports

**NSD**

- School has begun, it is in person, teachers and students are making it work. There are students in quarantine. We are hopeful that there are declining cases. Substitute teacher shortage; please spread the word if you know anyone who is interested. Also shortage on bus drivers. The vaccine mandate will create bussing issues, so looking at creative solutions, including asking for volunteers to drive rental vans to get students to school.
- Bond work is progressing. There are classroom-level meetings for all aspects of future construction in different locations. Discussion is underway to determine where to relocate students from schools that will be closed, including Catalyst HS.
- There is still no final decision on where Dundee Elementary School will land, but first priority is to relocate the school.
- Scott Ankeny, the District Currier, has retired, the longest-standing employee at 43 years of service.
- Board of Directors are still making national headlines with their agenda.

## **City of Newberg**

- ARPA Funds - Had the second meeting of the budget committee to discuss how the City will use \$5.4 million in ARPA funds. One thing we heard was the desire to get some money out to businesses immediately. We are planning to do some listening sessions with businesses to learn about needs and how we can best utilize funds
- Pavement and Projects - The sidewalk project around Scott Leavitt park has started and expects to be completed by the end of October
- We are putting in new ADA compliant sidewalk ramps along East 6<sup>th</sup> Street
- Crestview Drive is also moving forward.
- Additionally, we're finishing up some projects at the Wastewater Treatment Plant including our solar farm project which will help offset some energy costs for the City.
- COVID - Our library re-opens to the public today with its regular hours. We're still discussing reopening our other buildings to the public buildings.
- Additionally, we're testing out our hybrid options as far as public meetings go this month. We may try to use a hybrid system starting in November for City Council meetings
- Personnel - We are currently recruiting for an Administrative Service Director through our recruiter. This will oversee our Finance, IT, and HR departments.
- Our new Bi-Lingual Community Engagement Specialist, Ezequiel Garcia started with the city a couple of weeks ago. We're excited to get him integrated into the community.

## **TVF&R**

- Wildfire season has officially been declared ended. The local burn ban has been lifted.
- Bond will go out in November. Presentation campaign has begun.
- There are public questions about the rural fire station; a location near the airport has been discussion. This construction is not contingent upon the bond.
- Station 54 opened last week, near Charbonneau. This is a lease facility, which is unusual.
- First female captain has been named, and she will be the captain at 54.

## **CPRD**

- Don described a near-drowning incident yesterday at Chehalem Aquatic and Fitness Center, and asked clarification on dispatch process; it took 13 minutes for TVF&R to respond. Myers said that how dispatch works is Newberg ferries calls to Washington County (primary dispatch). Myers said he would be glad to look into the call and discuss further.
- City of Dundee approved the Sander Estate proposed masterplan on Sept. 21.
- Ewing Young Historical Park proposed bridge update: Casey Creighton gave an update on the recently closed RFP.
- Newberg-Dundee Bypass Trail update: CPRD meets with ODOT today to discuss plans for Phase II, which will include building an overpass (ped and bike) between golf course and hospital.



- Golf course continues to be busy.
- Pickleball and Trails Advisory Committees have been appointed; Kat gave update on them.
- Kat said Newberg City Club will resume in two weeks on its regular schedule and will meet remotely for foreseeable future.

III. **New Business:** Elected officials' meeting and survey - Don Clements urged the committee to plan a meeting, and also examine how Newberg can manage current image challenges of community that the school Board controversy have created, and support elected officials. Clements also urged a community survey be done to capture the identity and wishes of community.

Myers asked why wouldn't each agency take this on for itself. Clements said there would be a huge savings of money, and as a group, we can communicate better to the public, and also the results give the agencies a baseline with results that last a long time.

Lacey said Community Vision 2040 involved different entities, and she asked how this would fit into this. Clements responded that it was not scientific, with co-efficient factor that can be verified. Myers said scientific surveys do proactive outreach to engage public input, including "silent majority"; TVF&R does one every two years.

Meeting adjourned at 9:18 a.m.

Next meeting is scheduled for Nov. 2, 2021.

## **Save Chehalem Mountain Aquifer / Save Your Home & Business**

In January 2020, despite the objections of dozens of residents, Yamhill County Commissioners approved a highly flammable marijuana/hemp Processing/Extraction Refinery at the top of Chehalem Mountain: 18505 NE Jaquith Road, Newberg. They also approved growing more than 20 acres of hemp/marijuana requiring extensive irrigation in a designated "Limited Groundwater Area", potentially depleting our aquifer and access to water in 1-5 years, according to (2) different Hydrologist/Geologist firms. The application at Yamhill County had numerous errors and inconsistencies that were not corrected. (Google or Internet Search: Docket SDR-28-19).

Our concerns are **NOT** about the legality of marijuana or hemp or the by-products of these plants.

We **ARE** concerned about:

**SAFETY:** Time, Place and Manner are the issue: the inappropriate placement of a business using combustible liquids on top of a mountain with no fire hydrants, winding gravel roads, steep terrain and frequent high winds. Commercial operations like this should be located in an industrial area, with regular inspections for safety, ready access to fire infrastructure and with fire suppression equipment and easier/quicker access to First Responders. Having these types of combustible operations in industrial areas also greatly protects the lives of our First Responders.

**WATER:** extensive irrigation affecting all our wells (thus property values also). The Chehalem Mountain Aquifer does not have an unlimited supply of water. That is why there are State mandated restrictions on what can be irrigated in a designated "Limited Ground Water Area". Hundreds of residents and businesses have joined this fight to protect our homes through the Oregon Water Resources Department (OWRD) Google: Case #G-18843.

### **ABOUT THE CHEMICAL REFINERY:**

- Combustible and highly volatile Butane or Ethanol will be stored on the property in large tanks, to be used in the extraction process.
- The facility is approved to process 24 hours a day, 7 days a week, 365 days per year, bringing in additional cannabis from all over the state.
- The refinery process will occur in buildings that were built previously without permits and at the time of construction were not inspected/approved by the County. To date they have not been inspected by OSHA nor the Fire Department for this intended use (electrical & plumbing included).
- At least 40 documented explosions have happened in Oregon with these types of Refineries. An explosion at this refinery would be catastrophic to our lives, homes, First Responders, wildlife and the environment.
- This location was in the evacuation zone of the 2020 Chehalem Mountain Fire. Local residents called 911 as they were fleeing to warn TVF&R about the potential dangers of chemicals on that site.

### **ABOUT CHEHALEM MTN AQUIFER REGIONAL AREA & LOSS OF WATER:**

- Since 1992, OWRD has designated Chehalem Mtn and the surrounding area (see map) a "Limited Groundwater Aquifer Area", which limits irrigation to ½ acre garden per parcel. The application proposes allowing an annual volume of 21 acre feet of water use for marijuana/hemp which would allow up to 7 million gallons of water in **one** year! This irrigation could irreparably lower our groundwater resources, & many wells could go dry in 1-5 years.
- Two hydrology/geology firms concurred that the proposed irrigation at this one location could deplete our aquifer within 1-5 years. The Aquifer is larger than just Chehalem Mountain and stretches from parts of Sherwood & Tigard to Newberg, Hillsboro, Scholls, Laurel and even to Gaston. Once the water is gone, it is gone- it cannot replenish itself at a rate to provide for all our homes.

## **WHO WE ARE:**

**Save Chehalem Mountain, Inc** is a locally organized Non-Profit Organization established to ensure that the Chehalem Mountain watershed area (below) provides the quality and quantity of water, environmental stability and fire protections necessary to support the homes/businesses, fish, wildlife, recreations, biological diversity, ecological values, public health and a sound economy. We are the Residents & Neighbors of this community who are concerned that one industry can have a devastating impact on the entire aquifer & region.

We are not against the crop of cannabis/marijuana/hemp and do not advocate for the removal of it. We are concerned about the negative impacts this crop has already had on neighboring properties, the environment and on wildlife with respects to intense odor pollution, diminished quality of life on one's property, high usage of water in a "Limited Ground Water Area" and an ongoing drought as well as legitimate fire safety concerns.

## **We are in Favor of Safe & Responsible Business OPERATIONS of the Marijuana/Hemp Industry:**

- 1) **SAFE:** growing / disposal of harmful chemicals / waste water / used debris
- 2) **PROPER:** government oversight and rules with inspections on all growing/processing operations
- 3) **ADOPTION of "Marijuana Time, Place & Manner" REGULATIONS:** Yamhill County Commissioners/Planning Director & Attorney adopting these regulations. They are sensible safety guidelines designed to protect property values, quality of life, the environment/wildlife & the lives of First Responders. Almost every other County in Oregon has adopted these measures to protect their citizens & the environment. To date, Yamhill County has not done so, despite Residents actively working with them for over (2) years.

As a result of trying to protect our homes, businesses & community from this one large cannabis/hemp grow & combustible processing multi-business operation at 18505 NE Jaquith Rd, we have learned there are 14 OLCC licensed operations within our state designated "Limited Ground Water Area". \*\*\*Only 1 has an irrigation permit from the Oregon Water Resources Department. This also does not include Hemp Grows which fall under the jurisdiction of Oregon Dept of Agriculture. As reported in the news, many homeowners in Southern and Central Oregon have already experienced their wells drying up this year due to grows.

According to OWRD (Oregon Water Resources Department) the rules for irrigation using domestic wells in this area are the following: *Homeowners are not allowed to use their domestic well for outdoor or indoor nursery irrigation without a permit from OWRD. Homeowners shall not include irrigation or landscape maintenance of more than 1/2 acre.*

## **GET INVOLVED / LEARN / DONATE TO SAVE YOUR HOME or BUSINESS:**

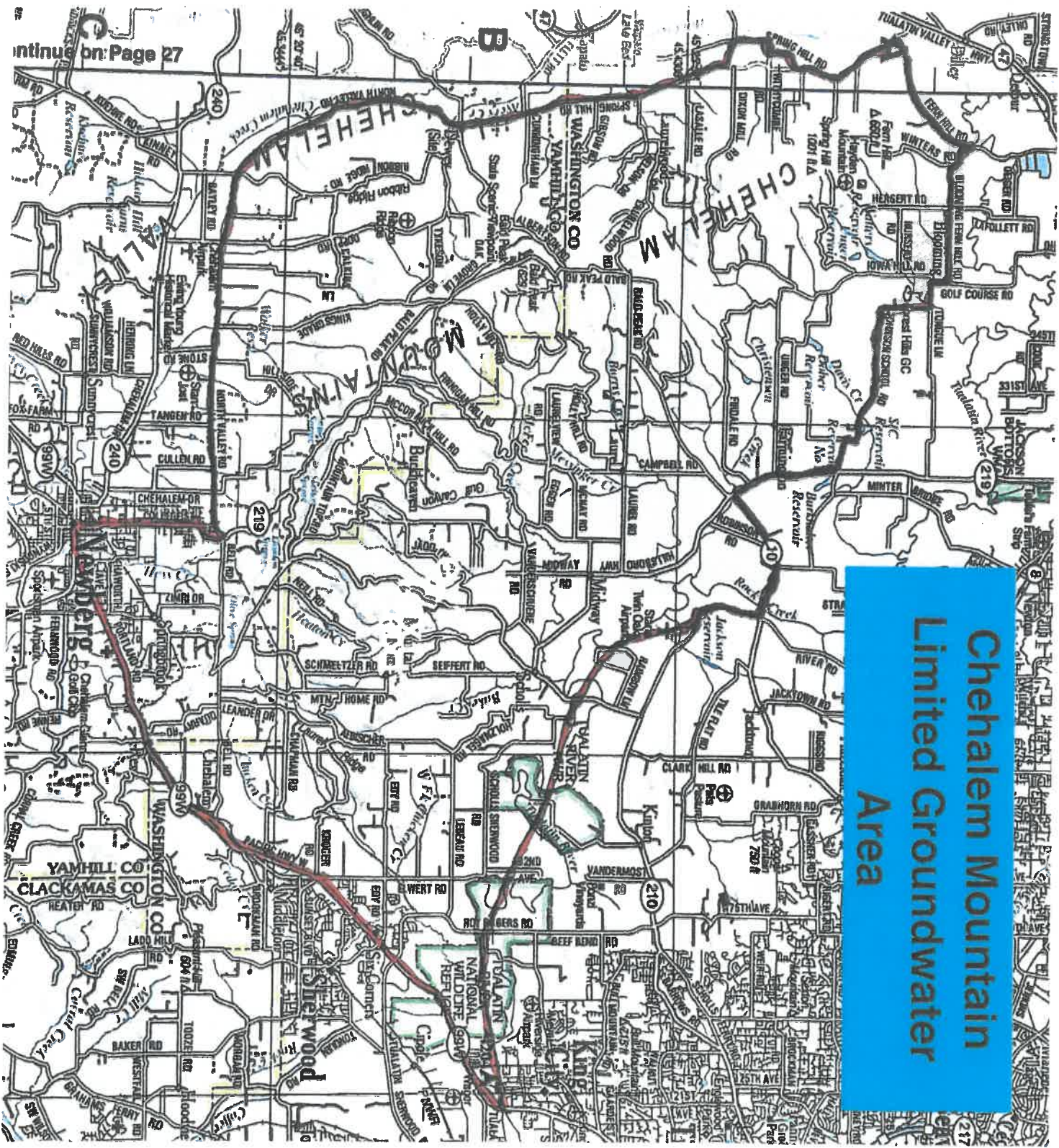
- Join our Facebook Group: Save Chehalem Mountain Aquifer
- Learn more and sign up for our newsletter on our website: [www.SaveChehalemMountain.com](http://www.SaveChehalemMountain.com)
- Donate for Attorney Fees: either on website or via check (preferred since no fees are deducted)

**Payable to: Save Chehalem Mountain**  
**15532 SW Pacific Hwy C1B#331, Tigard, OR 97224**

*Donations are not tax exempt nor refundable*

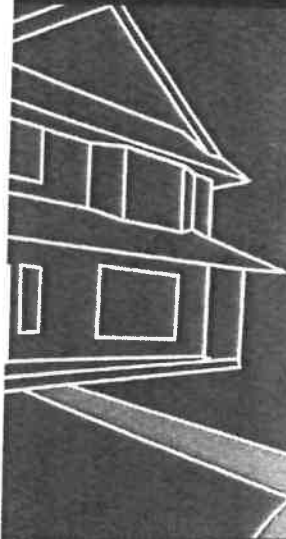
Please copy and share this flyer with neighbors or anyone that you feel will be impacted by this issue.  
Or request a copy using the group email address: [SaveChehalemMountain@gmail.com](mailto:SaveChehalemMountain@gmail.com)





Chehalis Mountain  
Limited Groundwater  
Area

Repair your sidewalk with a City of Newberg grant or loan!  
*¡Repare su acera con una subvención o préstamo de la ciudad de Newberg!*



Is the sidewalk outside your home cracked, unlevel, or missing curb ramps? Apply now for a grant or loan to repair it! | *¿La acera fuera de su casa está agrietada, desnivelada o falta una rampa? ¡Solicite ahora una subvención o préstamo para repararlo!*

**Grant Program | Programa de subvención**

Grant available to reimburse 100% repair cost not to exceed \$3,000.

*Subvención disponible para reembolsar el 100% del costo de reparación que no exceda \$3,000*

**Loan Program | Programa de préstamos**

Loans of up to \$6,000 are available for projects that will cost over \$3,000 to repair.

*Préstamos de hasta \$6,000 disponibles para reparaciones que exceden \$3,000*

Learn more! | *¡Aprenda más!*  
[www.NewbergOregon.gov/Sidewalks](http://www.NewbergOregon.gov/Sidewalks)







## Community Development Department

P.O. Box 970 • 414 E First Street • Newberg, Oregon 97132

503-537-1240 • Fax 503-537-1272 • [www.newbergoregon.gov](http://www.newbergoregon.gov)

### WE WANT YOUR COMMENTS

#### ON A PROPOSED NEW ANNEXATION IN YOUR NEIGHBORHOOD

A property owner in your neighborhood submitted an application to the City of Newberg to annex ten (10) lot(s) and adjoining public right-of-way and areas of the Newberg-Dundee Bypass. A public hearing will be held by the Planning Commission of the City of Newberg, on November 10, 2021, at 7 p.m. via Zoom <https://us06web.zoom.us/j/96560992068> Or Telephone: Dial (for higher quality, dial a number based on your current location): US: +1 253 215 8782 or +1 346 248 7799 or +1 669 900 6833 or +1 301 715 8592 or +1 312 626-6799 or +1 929 205 6099 Webinar ID: 965 6099 2068 to evaluate the proposal. You are invited to take part in the City's review of this project by sending in your written comments or by testifying before the Planning Commission. For more details about giving comments, please see the back of this sheet.

The application would expand the City limits by 187.76 acres for medium density residential (24.28 ac), high density residential (3.41 ac), commercial (4.02 ac), mixed employment (13.91 ac), and industrial (142.14 ac) zoning designations including rights-of-way and Newberg-Dundee Bypass. The area is a mix of industrial, undeveloped, natural resources areas and the Newberg-Dundee Bypass. No development is immediately planned for the annexed area.

APPLICANT: Oregon Department of Transportation  
Newberg OR LLC (Commercial  
Development Company – CDC)

TELEPHONE: 503-537-1240

PROPERTY OWNER: Oregon Department of  
Transportation (ODOT)

LOCATION: Newberg-Dundee Bypass

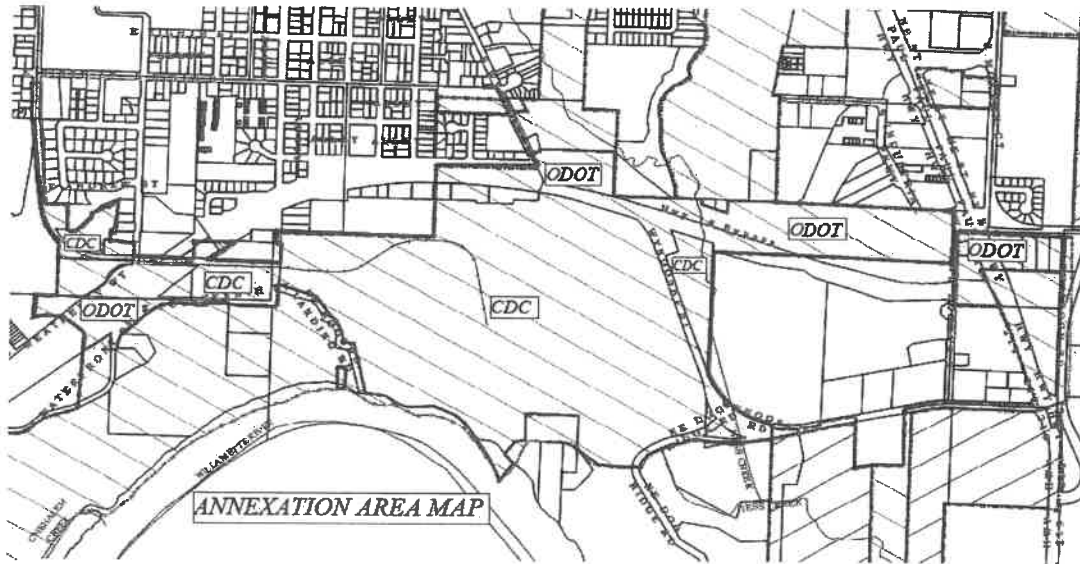
PROPERTY OWNER: Newberg OR LLC (CDC)

LOCATION: 1301 NE Wyooski Road

TAX LOT NUMBERS: Commercial Development Company: R3219-00300, R3219DD-01600, R3219-00190, R3219-00191 R3229-02600 R3229-02200 (portion within the Urban Growth Boundary), R3220CD-01900, R3220CD-02100, R3220CD-02300 R3229-00202 (portion within the Urban Growth Boundary). Oregon Department of Transportation: R3230-ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3230-00402, R3219DD ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3229-ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3220CC-ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3220CD-ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3220-ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3229-ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3221CC ROADS (Bypass portion outside city limits but within the Urban Growth Boundary), R3221-02300, R3221-02400, R3221-02500, R3221-02700, R3221-02800, R3221-02900, R3221-03000. City of Newberg Right-of-way: R3219DB-ROADS (Blaine Street portion outside city limits but within the Urban Growth Boundary) and R3219DD-ROADS (Blaine Street portion outside city limits but within the Urban Growth Boundary)

We are mailing you information about this project because you own land within 500 feet of the proposed annexation area. We invite you to participate in the land use hearing scheduled before the

Planning Commission. If you wish to participate in the hearing, you may do so in person or be represented by someone else.



If you mail your comments to the City, please put the following information on the outside of the envelope:

Written Comments: File No. ANX21-0003  
City of Newberg  
Community Development Department  
PO Box 970  
Newberg, OR 97132

You can look over all the information about this project or drop comments off at Newberg City Hall, 414 E First Street. The application can be reviewed at <https://www.newbergoregon.gov/cd/page/anx21-0003-annexation-mill-site-and-odot-newberg-dundee-bypass>. You can also buy copies of the information for a cost of 25 cents a page. A staff report relating to the proposal will be available for inspection at no cost seven days prior to the public hearing. If you have any questions about the project, you can call the Newberg Planning Division at (503) 537-1240.

All written comments must be turned in by 4:30 p.m. on October 29, 2021.

Any issue which might be raised in an appeal of this case to the Land Use Board of Appeals (LUBA) must be raised during the public hearing process. You must include enough detail to enable the decision maker an opportunity to respond. The applicable criteria used to make a decision on this application for an annexation is found in Newberg Development Code Section 15.250.

Prior to the conclusion of the initial evidentiary hearing, any participant may request an opportunity to present additional evidence, arguments or testimony regarding the application through a continuance or extension of the record. Failure of an issue to be raised in the hearing, in person or by letter, or failure to provide statements or evidence sufficient to afford the decision maker an opportunity to respond to the issue precludes appeal to the State Land Use Board of Appeals based on that issue.

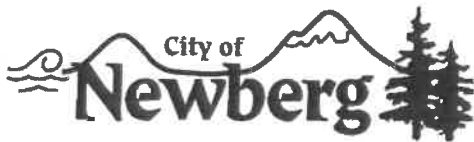


A recommendation will be made by the City Planning Commission to the City Council at the end of the initial hearing. A new evidentiary hearing will then be scheduled before the City Council. If you participate in the public hearing process, either by testifying at the public hearing, or by sending in written comments, you will be sent information about any decision made by the City relating to this project.

Date Mailed: October 19, 2021

**ACCOMODATION OF PHYSICAL IMPAIRMENTS:**

*In order to accommodate persons with physical impairments, please notify the Community Development Department Office Assistant II of any special physical or language accommodations you may need as far in advance of the meeting as possible as and no later than 48 business hours prior to the meeting. To request these arrangements, please contact the Office Assistant at (503) 537-1240. For TTY services please dial 711.*



## Community Development Department

P.O. Box 970 • 414 E First Street • Newberg, Oregon 97132  
503-537-1240. Fax 503-537-1272 [www.newbergoregon.gov](http://www.newbergoregon.gov)

### NOTICE OF PLANNING COMMISSION HEARING ON TRIPLEX, QUADPLEX, COTTAGE CLUSTER AND TOWNHOUSE REGULATIONS (HB 2001)

The City of Newberg is amending the Northwest Newberg Specific Plan to comply with HB 2001 regarding triplex, quadplex, cottage cluster and townhouse development regulations. All property within the Northwest Newberg Specific Plan that allows detached single-family dwellings will also be allowed to develop triplex, quadplex, cottage cluster and townhouse dwellings. The Newberg Planning Commission will hold a hearing on November 10, 2021 at 7pm at using Zoom:

<https://us06web.zoom.us/j/96560992068> Or Telephone: Dial (for higher quality, dial a number based on your current location):

US: +1 253 215 8782 or +1 346 248 7799 or +1 669 900 6833 or +1 301 715 8592 or +1 312 626 6799 or +1 929 205 6099

Webinar ID: 965 6099 2068, to evaluate the proposal. You are invited to take part in the City's review of this project by sending in your written comments or testifying before the Planning Commission. For more details about giving comments, please see the back of this sheet.

APPLICANT: City of Newberg  
TELEPHONE: 503-537-1240  
LOCATION: Northwest Newberg Specific Plan



We are mailing you information about this project because you own land within the Northwest Specific Plan area. We invite you to participate in the land use hearing scheduled before the Planning Commission. If you wish to participate in the hearing, you may do so in person or be represented by someone else. You also may submit written comments. Oral testimony is typically limited to five minutes per speaker.

If you mail your comments to the City, please put the following information on the outside of the envelope:

City of Newberg  
Community Development Department  
PO Box 970  
Newberg, OR 97132  
Written Comments: File No. GEN21-0011

All written comments must be received by 4:30 p.m. on October 29, 2021. Written information received after this time will be read out loud at the hearing subject to time limits for speakers and will be included in the record if there are further proceedings.

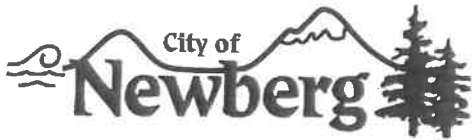
You can look over all the information about this project or drop comments off at Newberg City Hall, 414 E First Street. You can also buy copies of the information for a cost of 25 cents a page. A staff report relating to the proposal will be available for inspection at no cost seven days prior to the public hearing. If you have any questions about the project, you can call the Newberg Planning Division at 503-537-1240. The proposed material on triplex, quadplex, cottage cluster and townhouse regulations can also be accessed at: <https://www.newbergoregon.gov/cd/page/cpta21-0002-dca21-0004-gen21-0011gen21-0012gen21-0013-middle-housing-triplexes-quadplexes>

Any issue which might be raised in an appeal of this case to the Land Use Board of Appeals (LUBA) must be raised during the public hearing process. You must include enough detail to enable the decision maker an opportunity to respond. The applicable criteria used to make a decision on this application are found in Newberg Development Code Section 15.100.050, 15.100.060 and 15.346.050.

Prior to the conclusion of the initial evidentiary hearing, any participant may request an opportunity to present additional evidence, arguments or testimony regarding the application through a continuance or extension of the record. Failure of an issue to be raised in the hearing, in person or by letter, or failure to provide statements or evidence sufficient to afford the decision maker an opportunity to respond to the issue precludes appeal to the State Land Use Board of Appeals based on that issue.

The Planning Commission will make a recommendation at the end of the public hearing process. If you participate in the public hearing process, either by testifying at the public hearing, or by sending in written comments, we will send you information about any decision made by the City relating to this project.

Date Mailed: October 20, 2021



## Community Development Department

P.O. Box 970 • 414 E First Street • Newberg, Oregon 97132

503-537-1240. Fax 503-537-1272 [www.newbergoregon.gov](http://www.newbergoregon.gov)

### NOTICE OF PUBLIC HEARING

**This is to notify you that the City of Newberg has proposed a land use regulation that may affect the permissible uses of your property and other properties.**

On November 10, 2021, at 7:00 PM, via Zoom: <https://us06web.zoom.us/j/96560992068> Or Telephone: Dial (for higher quality, dial a number based on your current location): US: +1 253 215 8782 or +1 346 248 7799 or +1 669 900 6833 or +1 301 715 8592 or +1 312 626 6799 or +1 929 205 6099 Webinar ID: 965 6099 2068 the Newberg Planning Commission will hold a legislative public hearing regarding Planning Commission Resolution No. 2021-377 and City Council Ordinance No. 2021-2889.

#### PLANNING COMMISSION

RESOLUTION NO.: 2021-377

ORDINANCE NO. 2021-2889

APPLICANT: City of Newberg

REQUEST: A legislative action to implement HB 2001 triplex, quadplex, cottage clusters and townhouse regulations for all lands planned for single family residential homes [LDR (R-1, R-1/6.6 & R-P), MDR (R-2, AR & R-P), MDR/SP (R-2/SP), MDR/RD (R-2/RD), HDR (R-3) HDR/SP (R-3/SP), HDR/RD (R-3/RD), MIX/SP (R-P/SP), PQ (R-P & I), SD (LDR & MRR)] by amending the Comprehensive Plan, Development Code, Northwest Newberg Specific Plan, Springbrook Oaks Specific Plan, and Springbrook Master Plan.

LOCATION: City of Newberg and Newberg Urban Growth Boundary (see map). An interactive map can for Comprehensive Plan and Zoning designations can be accessed at:  
<https://newberg.maps.arcgis.com/home/webmap/viewer.html?webmap=1de60af01cb64885af90c5eb94d565b4>

TAX LOT: Various tax lots within the Newberg Urban Growth Boundary (see map). The interactive map link is listed above.

FILE NO: CPTA21-0002, DCA21-0004 GEN21-0011/GEN21-0012/GEN21-0013

CRITERIA: Newberg Comprehensive Plan; Newberg Development Code 15.100.050 and 15.100.060 and Sections 15.05.030, 15.100.020, 15.205.050, 15.205.060, 15.220.050, 15.220.060, 15.240.020, 15.302.032, 15.302.040, 15.302.200,, 15.305.020, 15.336.020, 15.340.020, 15.346.070, 15.346.070, 15.352.050, 15.405.010,

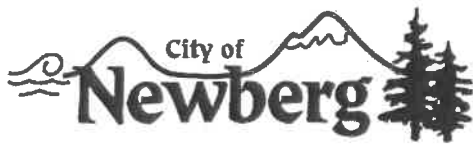
*Working Together For A Better Community-Serious About Service"*

ORS 227.186 requires the City to print the following sentence: "The City of Newberg has determined that adoption of this resolution and subsequent ordinance by the City Council may affect the permissible uses of your property, and other properties in the affected zones, and may change the value of your property." Actually, no determination has been or is expected to be made as to the effect of the proposal on the value of your property by the City.

The City Council is expected to review the Planning Commission recommendation on the proposal on December 6, 2021, at 7 p.m. via Zoom.

By: Doug Rux, Community Development Director





## Community Development Department

P.O. Box 970 • 414 E First Street • Newberg, Oregon 97132  
503-537-1240. Fax 503-537-1272 [www.newbergoregon.gov](http://www.newbergoregon.gov)

### NOTICE OF PLANNING COMMISSION HEARING ON TRIPLEX, QUADPLEX, COTTAGE CLUSTER AND TOWNHOUSE REGULATIONS (HB 2001)

The City of Newberg is amending the Springbrook Oaks Specific Plan to comply with HB 2001 regarding triplex, quadplex, cottage cluster and townhouse development regulations. All property within the Springbrook Oaks Specific Plan that allows detached single-family dwellings will also be allowed to develop triplex, quadplex, cottage cluster and townhouse dwellings. The Newberg Planning Commission will hold a hearing on November 10, 2021 at 7pm at using Zoom:

<https://us06web.zoom.us/j/96560992068> Or Telephone: Dial (for higher quality, dial a number based on your current location):

US: +1 253 215 8782 or +1 346 248 7799 or +1 669 900 6833 or +1 301 715 8592 or +1 312 626 6799 or +1 929 205 6099

Webinar ID: 965 6099 2068, to evaluate the proposal. You are invited to take part in the City's review of this project by sending in your written comments or testifying before the Planning Commission. For more details about giving comments, please see the back of this sheet.

APPLICANT: City of Newberg  
TELEPHONE: 503-537-1240  
LOCATION: Springbrook Oaks Specific Plan



We are mailing you information about this project because you own land within the Springbrook Oaks Specific Plan area. We invite

you to participate in the land use hearing scheduled before the Planning Commission. If you wish to participate in the hearing, you may do so in person or be represented by someone else. You also may submit written comments. Oral testimony is typically limited to five minutes per speaker.

If you mail your comments to the City, please put the following information on the outside of the envelope:

City of Newberg  
Community Development Department  
PO Box 970  
Newberg, OR 97132  
Written Comments: File No. GEN21-0012

All written comments must be received by 4:30 p.m. on October 29, 2021. Written information received after this time will be read out loud at the hearing subject to time limits for speakers and will be included in the record if there are further proceedings.

You can look over all the information about this project or drop comments off at Newberg City Hall, 414 E. First Street. You can also buy copies of the information for a cost of 25 cents a page. A staff report relating to the proposal will be available for inspection at no cost seven days prior to the public hearing. If you have any questions about the project, you can call the Newberg Planning Division at 503-537-1240. The proposed material on triplex, quadplex, cottage cluster and townhouse regulations can also be accessed at: <https://www.newbergoregon.gov/cd/page/cpta21-0002-dca21-0004-gen21-0011gen21-0012gen21-0013-middle-housing-triplexes-quadplexes>

Any issue which might be raised in an appeal of this case to the Land Use Board of Appeals (LUBA) must be raised during the public hearing process. You must include enough detail to enable the decision maker an opportunity to respond. The applicable criteria used to make a decision on this application are found in Newberg Development Code Section 15.100.050, 15.100.060 and 15.346.050.

Prior to the conclusion of the initial evidentiary hearing, any participant may request an opportunity to present additional evidence, arguments or testimony regarding the application through a continuance or extension of the record. Failure of an issue to be raised in the hearing, in person or by letter, or failure to provide statements or evidence sufficient to afford the decision maker an opportunity to respond to the issue precludes appeal to the State Land Use Board of Appeals based on that issue.

The Planning Commission will make a recommendation at the end of the public hearing process. If you participate in the public hearing process, either by testifying at the public hearing, or by sending in written comments, we will send you information about any decision made by the City relating to this project.

Date Mailed: October 20, 2021



# Memorandum

**To:** Don Clements  
**From:** Heidi Smith  
**Date:** October 13, 2021  
**RE:** Update requests for the Employee Handbook

Upon review of the District handbook, below are listed proposals for additions/changes:

**Page 10:** add the word "the" in the second paragraph, last line, before the word "supervisor".

**Page 18:** Under Dispute Resolution. Should the additional level of the "personnel committee" be added after the Superintendent?

**Page 18:** under Employment: re: internally posting for a position – is there a procedure for how we advertise this? This is not necessarily a change, just a question.

**Page 37:** add the following to cover the growing costs to the District (Cascade Employers has advised this is allowable provided it is in our handbook):

"Reissuance of pay checks:

Paper checks:

In the event an employee needs a check re-issued due to damage or failure to deposit the check prior to the check becoming "stale", (checks become stale 6 months from the date of the check) a re-issuance fee of no less than \$30.00 PER CHECK will be due and payable by the employee to cover employer banking costs.

If a check is lost/stolen, a stop payment fee of no less than \$35.00 PER CHECK will be due and payable by the employee to cover banking cost. A lost/stolen check affidavit will need to be completed by the employee before the check can be stopped.

Direct Deposits:

In the event the employee errors in their account information for direct deposit and after the funds have been verified of their return to Chehalem Park and Recreation District, there will be no cost to issue a paper check for the first and second re-issuance. Upon the third and subsequent re-issuance due to a direct deposit error not being corrected by the employee, there will be a fee of \$25 PER CHECK re-issuance, which will be due and payable to CPRD. This fee may be waived if it is determined not to be the employee's error."

**Page 47:** Under Sick Leave: Update to Oregon law: "Employees may carry over up to 40 hours of unused sick time from one leave year to the next. The year begins each January 1<sup>st</sup>. Full-time exempt employees may carry up to 100 days/800 hours of sick leave from one leave year to the next." (under the current handbook – that portion was removed and if it is not changed, I have to remove accrued sick time for all employees who have over 40 hours.

**Page 47:** Update the holidays for office closure to align with the Federal holidays in addition to maintaining the current authorized observed holidays already in our handbook. This would mean adding Juneteeth (June 19<sup>th</sup> annually) and Columbus day.

**Page 50:** add a sentence, "Leave options may not be inclusive, and the District will follow the most updated Leave of Absence Policies to be in compliance with Federal, State, and local employment regulations. Please contact your supervisor to request the most updated information."

**Page 64:** Update the second paragraph to read, "To foster a safe workplace, all unauthorized weapons are prohibited on District premises. Additionally, employees are prohibited from carrying unauthorized weapons of any kind while away from the premises while conducting District business. Premises includes, but is not necessarily limited to buildings and lots, offices, desks, lockers, and vehicles (privately owned and District owned)."

**Pages 66-67:** Have the Safety Committee review for updates.

